Pavement, Sidewalk, and Guardrail Management Study

SUDBURY, MASSACHUSETTS

PREPARED FOR

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Introduction

The Town of Sudbury hired Vanasse Hangen Brustlin, Inc (VHB) in 2017 to perform a pavement, sidewalk, and guardrail management study. The comprehensive study was undertaken to evaluate pavement, sidewalk, and guardrail conditions in Sudbury and to allow for prioritization of maintenance and rehabilitation activities and the analysis of various funding scenarios.

Under the scope of this project, VHB performed distress ratings on 139.6 miles of Town-maintained roadway as well as evaluated the condition of the sidewalk and guardrail network. The pavement ratings were completed using the RoadManager system rating formula that is detailed in section 2 of this report.

The following report describes the relevant concepts of pavement management practice, the steps undergone by VHB throughout the course of the project, a summary of the conditions found in the Town, and the budget analysis performed by VHB.

Theory of Pavement Management

Pavement management is the practice of planning for pavement repairs and maintenance with the goal of maximizing the value and life of a pavement network.

To accomplish this, a community needs to have several repair techniques in its arsenal and the knowledge of when to apply them. This is where pavement management comes into play. With a comprehensive database of road conditions, the pavement management software can model when to perform which repairs on a road network. Of course, engineering judgment is required to finalize any list of street repairs, as no computer model

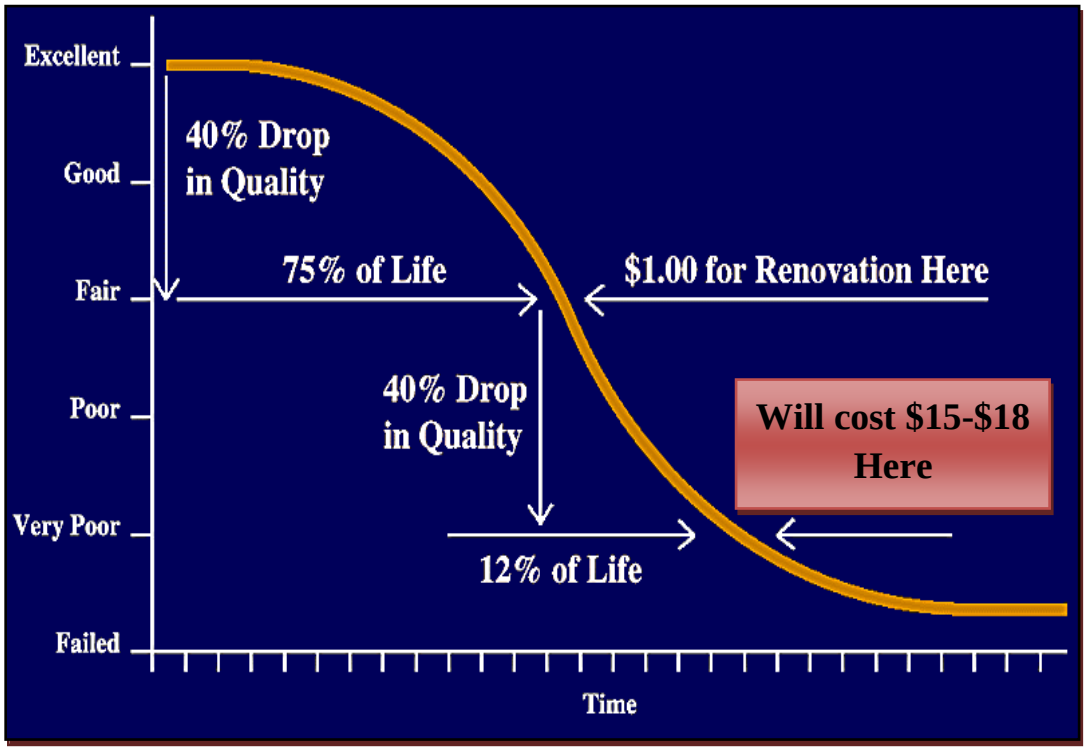
can take every variable analyzed in making a repair decision into account. The computer system is a great springboard to help a community start its repair program for each year and is an excellent method of storing the repair data.

Pavement Management puts a focus on **Pavement Preservation**. Maintaining the roads in fair condition has been proven to be the most cost effective approach to sustaining a road network.

The Pavement Deterioration Curve

Below is a model of how a street's pavement deteriorates over time. Interpreting the curve, a street starts out in excellent condition when it is newly constructed. Midway through its life, a low-cost repair such as crack seal and full depth patch will cost approximately a dollar a square yard. It takes only a few years for the window of opportunity to perform this low-cost maintenance to pass after which the road would need an overlay costing $15-$18 per square yard. By performing timely maintenance, road conditions can be improved today thereby extending the life of the road.

Figure 1 Pavement Deterioration Curve



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Pavement Methodology

VHB performed a detailed condition evaluation on Sudbury's 139.6 miles of town maintained roadways to develop the pavement management system. The evaluation was performed using the Town's existing route system, thus the first steps of developing the pavement inventory and defining pavement management segments must be completed prior to assessing surface distresses. VHB carefully categorized, measured, and recorded the individual pavement distresses within each pavement management section

Network Identification

Network Identification builds an inventory of streets that describe the municipality's complete roadway network. The direction of travel, street length, width, ownership, classification, zone and pavement type are among the items identified at this initial phase in the pavement management process. This integral step ensures the streets surveyed are the definitive set to be analyzed.

Pavement Management Section Identification

Once the Network Identification is complete, the field work begins. Each street contains one or more pavement management sections. A pavement management section defines the limits of previous construction or maintenance activities within each street. Sections are defined by having the same width, typical distresses, functional class, etc. The goal is to set up homogenous areas of pavement to aid in assigning the appropriate repair. A street may

be one section, or it may be comprised of several pavement management sections, depending on its construction history.

Surface Distress Assessment

For each pavement management section, the severity and extent of nine major pavement distresses are recorded, and then entered into a weighted formula to arrive at a Pavement Condition Index (PCI). The distresses are categorized as base related or surface related distresses. Base related distresses indicate that the pavement structure is inadequate for the existing traffic load and soil conditions. Streets that show significant base related distresses may need to have the pavement structure strengthened with either thicker or stronger base or pavement materials. Surface related distresses are caused by age and weathering of the pavement. Streets that have predominantly surface related distresses are excellent candidates for maintenance sealing to inhibit further pavement oxidization (the main effect of aging). Streets with more of the base related distresses will most likely need some full depth patching, structural overlays or reclamation/reconstruction.

The four base related distresses are:

- potholing or non-utility patching
- alligator cracking
- distortion
- rutting

The five surface related distresses are:

- block cracking
- transverse or longitudinal cracking
- bleeding or polished aggregate
- surface wear or raveling
- shoving, slippage or corrugation

The RoadManager pavement distress rating system, described above, and PCI formula shown below, was developed by Vanasse Hangen Brustlin, Inc. specifically to allow the efficient rating of the distresses and conditions found on New England roadways, and is used by over 100 municipalities throughout the region.

PCI Defined

A PCI was generated for each inventoried pavement management section in Sudbury using the surface distress data collected by VHB. PCI is measured on a scale of zero to one hundred, with one hundred representing a pavement in perfect condition and zero describing a road in impassable condition. Each type of observed pavement distress is assigned a deduct value based on the type, severity and extent of the distress. A weighted sum of the deduct points is subtracted from the perfect "one hundred" road in order to generate a PCI for each pavement management section. In general, base related (pavement foundation) distresses are weighted more heavily than surface related distresses. For example, if 15% of a road section had medium severity "Alligator Cracking" it would receive a deduct of 40 points. Whereas the same area of "Block Cracking" would only receive a deduct of 15 points. The actual PCI calculation follows:

PCI = 100 – (Highest Deduct Value) – (25% of remaining base related deduct values) – (10% of remaining surface related deduct values)

The Five Treatment Bands

The pavement management system uses broad ranges to group the individual repair types into five major treatment bands. Treatment bands are a useful tool to summarize data on a Town-wide basis. An individual road segment will fall into a particular category based on the strategy table's output of repair types and will vary due to functional classification. The goal is to gain a broad understanding of the existing conditions in simple yet meaningful terms.

Table 1 Treatment Band Descriptions

Treatment Band	PCI[1]	Description
Do Nothing	93-100	Excellent condition - in need of no maintenance.
Routine Maintenance	86-92	Good condition – may be in need of crack sealing or minor localized repair.
Preventive Maintenance	73-85	Fair condition – pavement surface may be in need of surface sealing, full depth patch and/or crack sealing.
Structural Improvement	61-72	Deficient condition – pavement surface structure in need of added strength for existing traffic. Typical repairs are overlay with or without milling.
Base Rehabilitation	0-60	Poor condition – in need of base improvement. Typical repairs are reclamation or full depth reconstruction.

Note: The Treatment bands are defined below.

1 These are only general PCI ranges for reference purposes, and represent only one pavement type. There are several fields considered by the strategy table when assigning repair types to each individual street.

Do Nothing

The Do Nothing category exhibits roads which are in need of no maintenance. These roads are in excellent condition and existing distresses generally do not need to be addressed.

Routine Maintenance

Routine maintenance activities are those which are taken to correct a specific pavement distress. Routine maintenance usually addresses localized pavement defects and includes activities such as:

- Full depth patching;
- Skin patching;
- Crack sealing.

Preventive Maintenance

Preventive maintenance activities are those which are performed at planned intervals to protect and seal the pavement. Seals are designed to provide one or more of the following benefits:

- Prevent the intrusion of air and moisture;
- Fill small cracks and voids;
- Rejuvenate an oxidized binder;

› Provide a new wearing surface.

Structural Improvement

Structural improvement includes the work necessary to restore the pavement to a condition that will allow it to perform satisfactorily for several years. Generally, a structural improvement will consist of milling the existing pavement down and applying a new Hot Mix Asphalt Overlay allowing existing grades to be maintained.

When the existing grade can be increased a new Hot Mix Asphalt course can simply be placed upon the existing surface.

Structural improvements also include the work necessary to prepare the pavement for an overlay, either with or without milling. The major activities involved in the preparation process are:

› Partial depth patching;
› Full depth patching;
› Joint and crack sealing.
› Grinding and milling
› Hot Mix Asphalt Leveling Courses.

Base Rehabilitation

Base rehabilitation utilizes one of two methods:

› Reclamation;
› Reconstruction.

Reclamation is the process of rehabilitating existing deteriorated pavements. The existing pavement and base, subbase, and possibly subgrade are pulverized and blended to create a homogenous pavement base. This reclaimed pavement base is then paved with a new Hot Mix Asphalt surface.

Reconstruction is the complete removal and replacement of a failed pavement, and might also involve widening, realignment, traffic control devices, safety hardware, and major base and drainage work.

Customizing Repair Strategies

VHB met with the Town Engineering staff to review VHB's typical repair strategies, and to learn how to customize these strategies to meet the Town's specific needs. VHB also refined repair unit costs. The list of repair alternatives (Appendix A) was developed based on the types of projects the town would like to implement. The unit costs for each type of projects were developed using a combination of line-item unit costs from previous Sudbury paving contracts and from bid averages from MassDOT. VHB's goal was to understand Sudbury's decision-making process and simulate that process in the budget analysis software based on the pavement condition and other criteria of each pavement section.

Often the repair strategy differs between municipalities due to specific aspects of the maintenance approaches previously used. In Sudbury's case, there has been a history of repeated overlays on a number of roadways. Due to this, the current strategy and associated cost, need to account for the removal of asphalt layers and/or adjustment of road profile in conjunction with the proposed rehabilitation activies.

Preparing Budget Scenarios

Once the roadway conditions are inventoried and analyzed, and the repair strategies are defined, the impact of various spending programs on the roadway network is assessed. These studies can range from 1 to 20 years; however, for the purpose of this report 5-year studies are used. The purpose of the budget planning process is to determine the impact of various spending levels to find a funding level that will best meet Sudbury's needs. The budget analysis software uses pavement deterioration curves, unit costs, and the strategy tables developed in the repair strategy definition phase to assign each street a repair type and associated cost for each year of the study. The software also assigns each street a benefit value that is used to prioritize which streets the software will select for repair each year. It is important to understand that a pavement management system is a network-wide planning tool, and is not intended to give definitive street-by-street repair data. Field verification and testing are recommended to confirm any street repair list generated.

Deterioration Curves

In order to properly plan for future repairs, the budget analysis feature of the pavement management system uses deterioration curves. The deterioration curves estimate the rate at which the pavement condition decreases over time. These pavement deterioration curves depict two major categories of functional classification - arterials and collectors in one curve and local roads in the other as well as a differentiation for pavement type. The following deterioration curve is for a local HMA roadway.

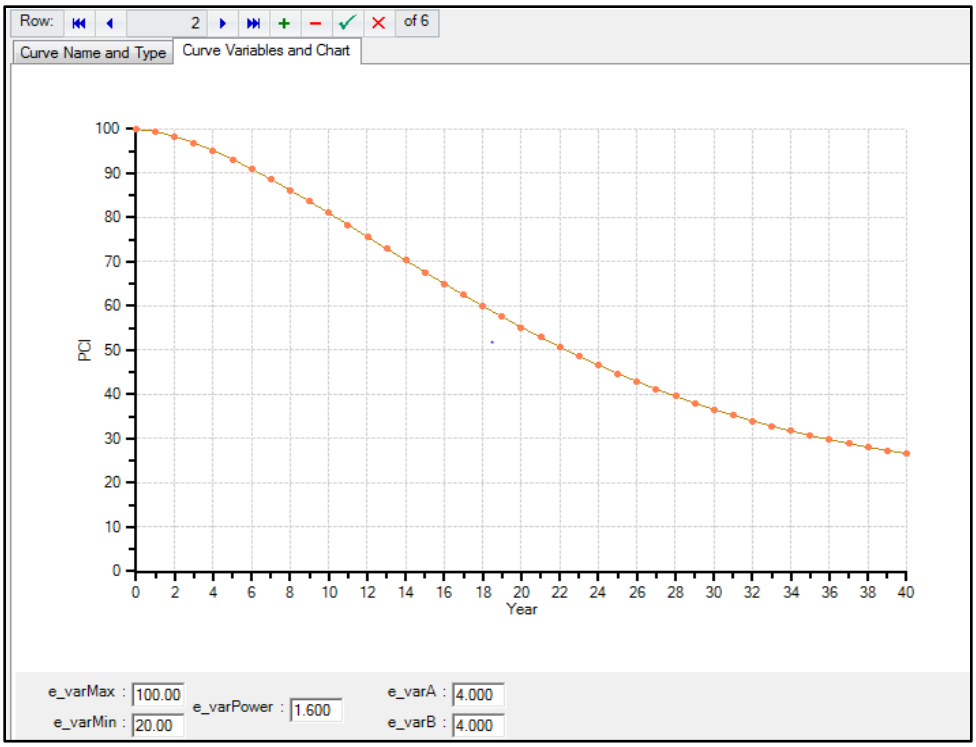
Strategy Table (Decision Tree)

The pavement management system uses a table of repair strategies to assign specific road repair types to individual roadway segments. The repair strategy table incorporates PCI ranges as well as functional class and pavement type to simulate decisions consistent with Sudbury's repair practices and procedures.

Project Prioritization

The budget analysis software prioritizes needed system repairs based on the estimated "Benefit Value". The Benefit Value formula is calculated using variables representing traffic volume, repair service life, PCI, and unit repair costs for each pavement management section. The calculation for the Benefit Value is shown below. For each plan year, the software prepares a future roadway condition projection, exhausts the assigned budget, and then produces an annual list of roads included in the repair program. The system also allows the user to enter an inflation rate to account for estimated increases in future year construction costs. A 4% inflation rate was used for Sudbury. The project prioritization puts a focus on pavement preservation.

Figure 2 Sample Curve from Sudbury Pavement Management System



The Benefit Value prioritization process generally favors cost effective maintenance alternatives. Repair actions are typically delayed on those sections that require reconstruction or major rehabilitation because the benefits for dollars spent are generally lower than maintenance candidates. After the relatively good roads are "saved", improvements are directed towards the poorer arterial and collector roads, and then to the local roads in need of major rehabilitation.

The calculation of Benefit Value is as follows:

$$Benefit\ Value = \frac{ADT * Life\ of\ Repair}{PCI * Unit\ Cost\ of\ Repair}$$

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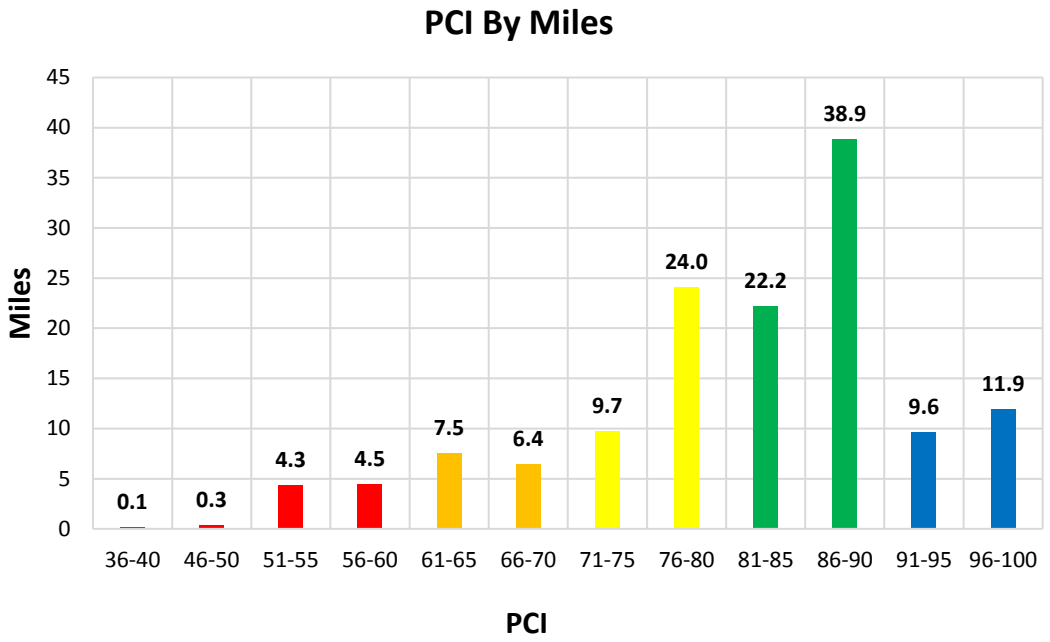
Existing Pavement Conditions

Town Roads Pavement Conditions

VHB conducted the field evaluation of pavement conditions in the summer of 2017. The average PCI for Sudbury's **Public** road network was found to be an **81**. A PCI of 81 represents a road in fair to good condition. Throughout 2017, VHB, in concert with Town officials, performed a thorough data validation, reviewed pavement maintenance and rehabilitation treatment options, configured the pavement management system, and began prioritizing projects.

The following chart shows the distribution of conditions in small PCI increments which provides a more detailed picture of the state of Sudbury's roads.

Figure 3 PCI Distribution- All Public HMA Roads



While there is a wide distribution of pavement conditions in Sudbury, a majority of the roads are between a 60 and 90 PCI indicating a variety of pavement maintenance and rehabilitation options will be appropriate. The sections of roadway that are between 61 and 65 should be addressed before they fall below 60 and need to be reclaimed.

Backlog of Work

Applying the five treatment bands shown in Table 1 and unit costs referenced in Appendix A to Sudbury's road network, a summary of outstanding work was developed. The following table gives the miles and dollars associated with each treatment band for the conditions at the time of the evaluation.

Table 2 Summary of Miles and Dollars of Outstanding Work

Treatment Bands	Miles	Cost
Base Rehabilitation	9.3	$3,359,000
Structural Improvement	19.4	$3,883,000
Preventive Maintenance	50.5	$3,687,000
Routine Maintenance	39.3	$276,000
Do Nothing	21.2	$0.00
Totals	**139.6**	**$11,205,000**

Figure 4 Current Backlog Mileage by Treatment Band

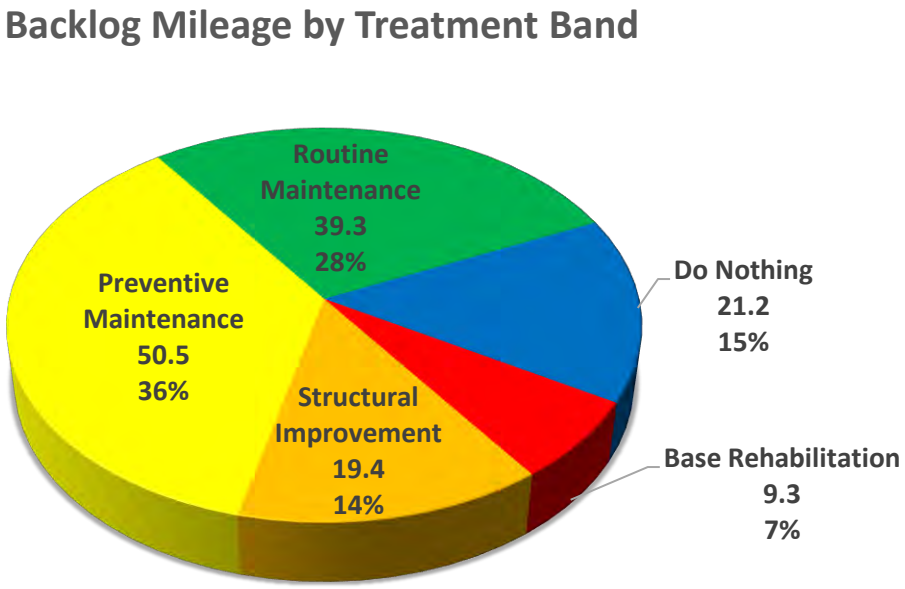
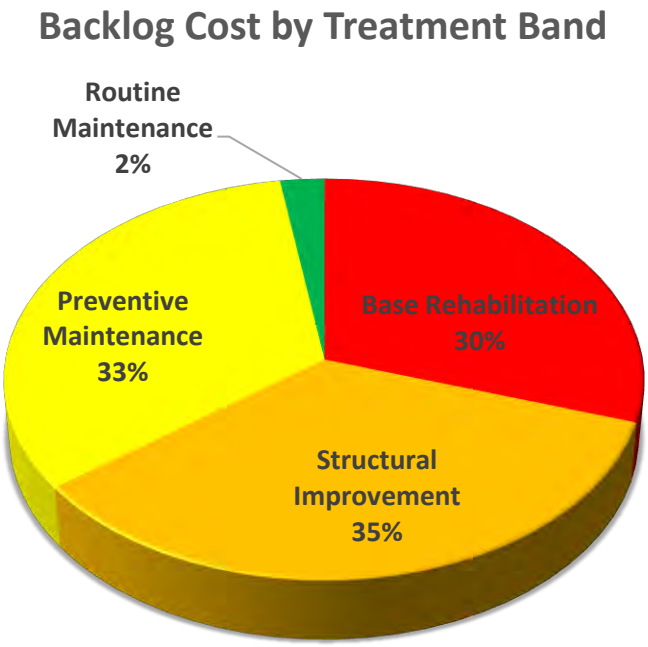


Figure 5 Current Backlog Cost by Treatment Band



Figures 4 and 5 show that approximately 21% of the Town's roadway mileage needing structural improvement or base rehabilitation add up to 66% of the backlog cost, and consequently that the 63% of roads requiring only maintenance makes up 33% of the backlog. To improve the road network an increase in funding will be needed to address the roads in poorer shape, but it will be very important to perform timely maintenance on those roads in better condition to prevent them from deteriorating and adding to the backlog.

Thematic GIS Mapping

By having the asset management data integrated with GIS, thematic maps can be developed displaying paving conditions, road program, repair history, or any other information stored in the system. The inventories are mapped in the Appendices.

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Budget Analysis

Sudbury has a major investment in its 134.6 miles of town accepted roads. It is easy to forget that roadways are a community's single largest investment. The final phase of the pavement management process that VHB undertook for this report was to project the results of anticipated funding levels.

Budget Scenarios

VHB analyzed the effects of 2 different funding scenarios on the projected future overall pavement condition in the Town for 5 years as well as the future backlog of needs in dollars.

The funding scenarios analyzed were:

Chapter 90

- **$750,000** per year for the next five years.
- **$1,250,00** to maintain PCI

The following tables show the year by year budget, predicted average PCI, and predicted outstanding total dollar backlog of work for the five-year analysis period.

Current Funding

The first scenario projects the effects of a $750,000 per year allotment.

Table 3 Chapter 90

Plan Date	Budget	PCI	Backlog
Surveyed		81	$11,205,000
2018	$750,000	81	$13,407,000
2019	$750,000	80	$15,366,000
2020	$750,000	79	$17,090,000
2021	$750,000	79	$18,933,000
2022	$750,000	78	$20,409,000

Maintain PCI

This scenario shows the effects of a $1,250,000 per year allotment.

Table 4 Chapter 90 Funding

Plan Date	Budget	PCI	Backlog
Surveyed		81	$11,205,000
2018	$1,250,000	82	$13,407,000
2019	$1,250,000	82	$14,561,000
2020	$1,250,000	82	$15,404,000
2021	$1,250,000	82	$16,481,000
2022	$1,250,000	81	$16,981,000

Budget Analysis Summary

The following two charts display the summary results for the two scenarios together for easy comparison.

Figure 6 Budget Projection PCI Summary

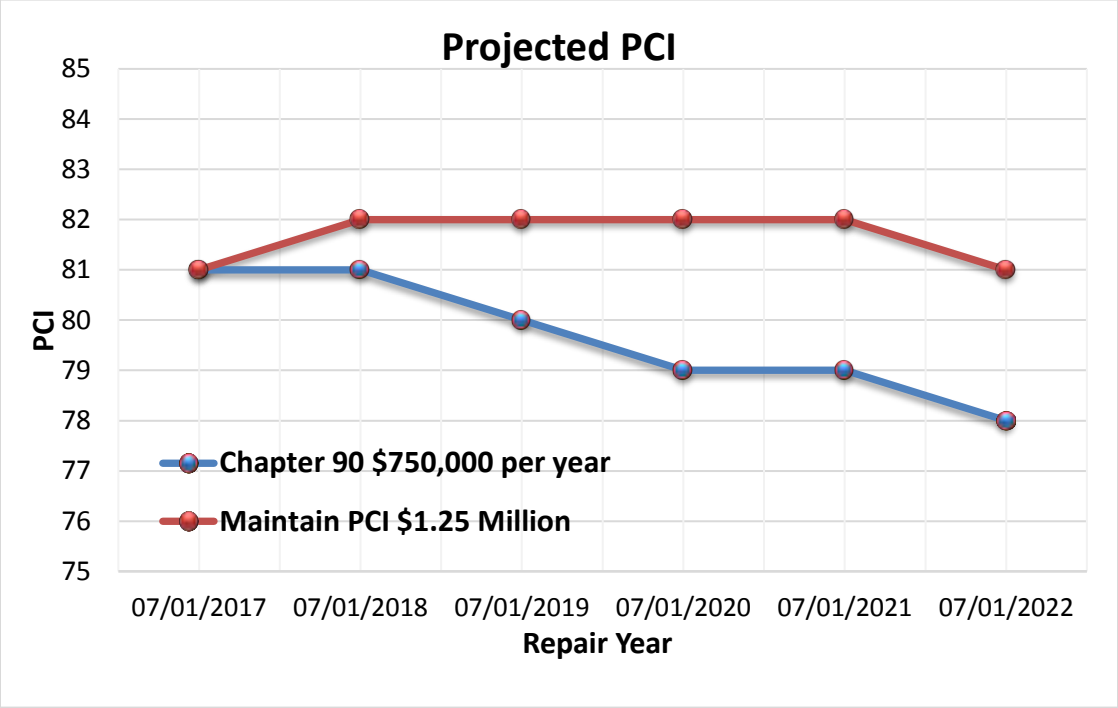
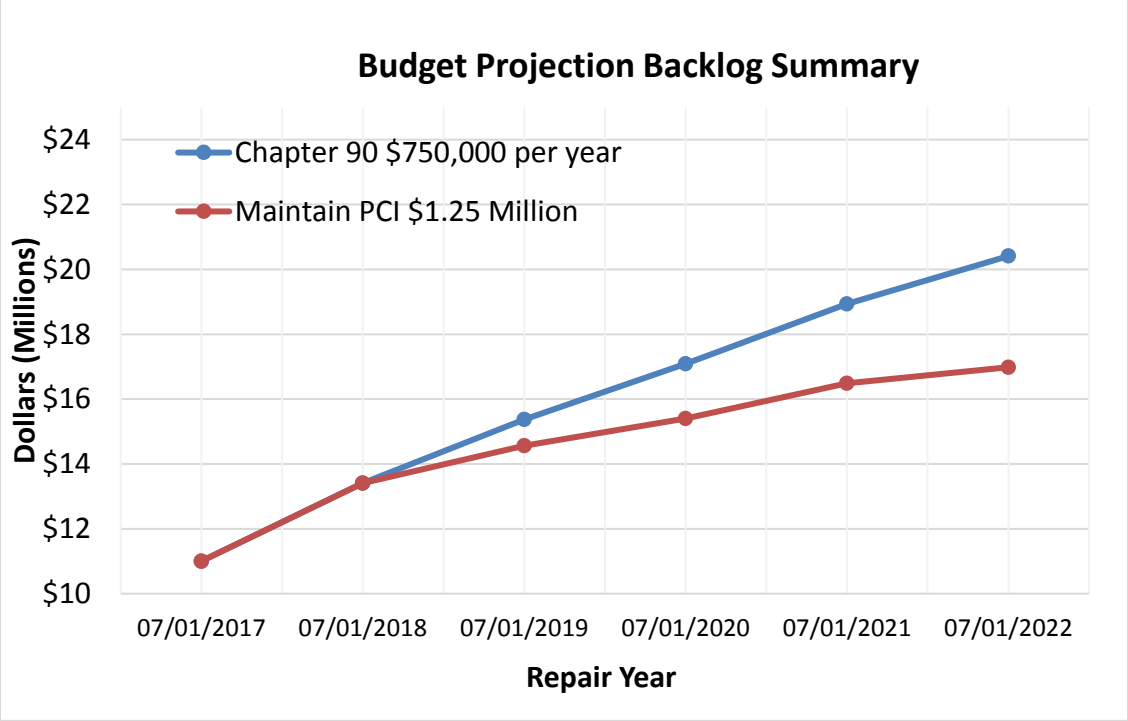


Figure 7 Budget Projection Backlog Summary



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Sidewalk Inventory

Americans with Disabilities Act (ADA) and Public Right of Way Accessibility Guidelines (PROWAG)

The Town of Sudbury Public Works department (SPW), as the responsible party for ensuring that PAR (accessible corridors for pedestrian use within the pedestrian zone of the public right-of-way) within the town's rights of way are non-discriminatory to individuals with disabilities as mandated by the federal government to conform to the specifications of the Americans with Disabilities Act. In addition to those responsibilities prescribed by law, HPW has also taken a proactive approach by evaluating PAR as they relate to the PROWAG to ensure that updates to the Transition Plan do not become obsolete when the PROWAG is adopted into law, as expected. Comprehensive detail of these requirements can be found at ADA.gov and https://www.access-board.gov/guidelines-and-standards/streets-sidewalks/public-rights-of-way/proposed-rights-of-way-guidelines. For pretext to this report, a summary of the key compliance attributes are summarized below.

Sidewalks:

Sidewalk should maintain a 48" minimum width, free from obstructions and abrupt level changes.

- Ground obstructions should not interrupt the 48" minimum width

- Above ground obstructions should not extend more than 4" into the travel path between the heights of 27" and 80"
- Cross Slope should not exceed 2%
- There should be no abrupt level changes greater than ¼"

Sidewalk Ramp Compliance

Title II of the ADA requires state and local governments to make pedestrian crossings accessible to people with disabilities by providing curb ramps. To allow people with disabilities to cross streets safely, state and local governments must provide curb ramps at pedestrian crossings and at public transportation stops where walkways intersect a curb. To comply with ADA requirements, the curb ramps provided must meet specific standards for width, slope, cross slope, placement, and other features. The Town of Sudbury has 367 existing ramps and an additional 275 ramps missing within their sidewalk network.

The following are the key characteristics of an accessible curb ramp according to the ADA Standards:

- The ramp run has the least running slope possible. (On a curb ramp, the running slope is the slope in the direction of pedestrian travel on the ramp run.)
- For curb ramps constructed after January 26, 1992 (post-ADA), the slope must be 8.33 percent (1:12) or less.
- For curb ramps constructed before January 26, 1992 (pre-ADA), including those that have since been altered, the running slope must generally be 8.33 percent (1:12) or less. However, ramp runs with greater slopes are allowed for pre-ADA curb ramps in the two following instances where space limitations prohibit the use of a slope of 8.33 percent (1:12) or less:
- The ramp run may have a running slope of up to 10 percent (1:10) if the rise is no more than six inches.
- The ramp run may have a running slope of up to 12.5 percent (1:8) if the rise is no more than three inches.
- The cross slope of the ramp run itself may not exceed 2 percent (1:50). (On a curb ramp, the cross slope is the slope perpendicular to [across] the direction of pedestrian travel on the ramp run.)
- The ramp, or ramp run, must be at least 36 inches wide, not including the flared sides.
- The ramp run must have detectable warnings – i.e., dome-shaped bumps – that extend the full width and depth of the ramp.
- Transitions from the ramp to the walkway, gutter, and street must be flush (level) or have a less than ¼" lip and free of abrupt level changes.
- The gutter must have a slope of no more than 5 percent (1:20) toward the ramp.

Ramp Replacement "Alteration" Policy

Municipalities are required to make pedestrian facilities ADA compliant any time alterations are done to the facilities themselves or the adjacent roadways. The U.S. Department of

Justice and U.S. Department of Transportation have defined roadway alternations to include roadway reconstruction, pavement overlays (with or without milling), open graded surface course, microsurfacing, cape seals, in place asphalt recycling, and thin lift overlays. Crack sealing, slurry seals, chip seals and fog seals, have been deemed maintenance, and not an alteration requiring ADA improvements.

Sidewalk Inventory Methodology

VHB created a geodatabase to store the location, inventory, and attribute data for all features required in this self-evaluation. Feature classes were set up for Sidewalks and Curb Ramps. This database was posted to ArcGIS Online so that it would be cloud accessible.

Then, using data enabled tablet computers with Global Positioning System (GPS) receivers and ArcGIS Collector application, VHB walked the PAR's to record the inventory data.

Sidewalk Evaluation Criteria:

A sidewalk segment is defined as a continuous length of sidewalk with similar characteristics. Each sidewalk segment was characterized with the following information

- Length
- Average Width
- Material
- General Condition (Excellent, Good, Fair, or Poor)
- Estimated Percent in need of Repair
- Beginning Term Type
- Ending Term Type
- Curb Material

Existing Sidewalk Inventory

Sidewalk Length

Length of the sidewalk segment measured to the nearest foot

Sidewalk Width

Average width of the sidewalk segment measured to the nearest half foot.

Sidewalk Material

Material consisted of two main types:

- Bituminous Concrete
- Portland Cement Concrete

Sidewalk Condition

A sidewalk condition value was established to quickly categorize sidewalk conditions into a repair strategy schema. Sidewalks were graded by the following:

- Excellent (like new condition)
- Good (light wear)
- Fair (considerable wear, cracking, or distortion)
- Poor (wear, cracking or distortion significantly impacts serviceability)

Estimated Percent Repair

The percent of sidewalk area in need of repairs was estimated for each segment. This was determined by carefully evaluating the entire segment and determining areas that are deteriorating or cause a safety hazards for pedestrians. Common distresses that were found in the sidewalk network were tree root issues and transverse cracking.

Beginning/End Term Type

The beginning and end type of the sidewalk. This field included the following types:

- BC Non-Compliant Ramp
    - Bituminous Concrete ramp that does not appear to be ADA compliant
- BC Compliant Ramp
    - Bituminous Concrete ramp that appears to be ADA Compliant
- Continuous
    - Used when sidewalk changes in width or condition to connect two segments that are continuous
- Intersection – No Ramp
    - Sidewalk at an intersection that does not have a ramp
- No Access
    - Used in areas that the sidewalk begins or ends without access to the road
- PCC Compliant Ramp
    - Portland Cement Concrete ramp that appears to be ADA compliant
- PCC Non-Compliant Ramp
    - Portland Cement Concrete ramp that does not appear to be ADA compliant

Curb Type

VHB recorded the presence and type of curbing along all sidewalks. This field included the following types of curbing:

- Bituminous Concrete
- Bituminous Concrete Berm
- Granite

› None
› Sloped Granite

Sidewalk Inventory Summaries

Sidewalk Material Summary in Miles

The following table summarizes the mileage of Sudbury sidewalk by material type.

Values represent the actual length of sidewalk, not roadway.

Example: A roadway 1 mile long with bituminous concrete sidewalks fully along both sides of the road would be represented in the totals below as 2 miles of bituminous concrete sidewalk.

Table 5 Sidewalk Material in Miles

Sidewalk Material	Miles
Bituminous Concrete	45.45
Portland Cement Concrete	0.0081
Other	0.0246
Total	**45.49**

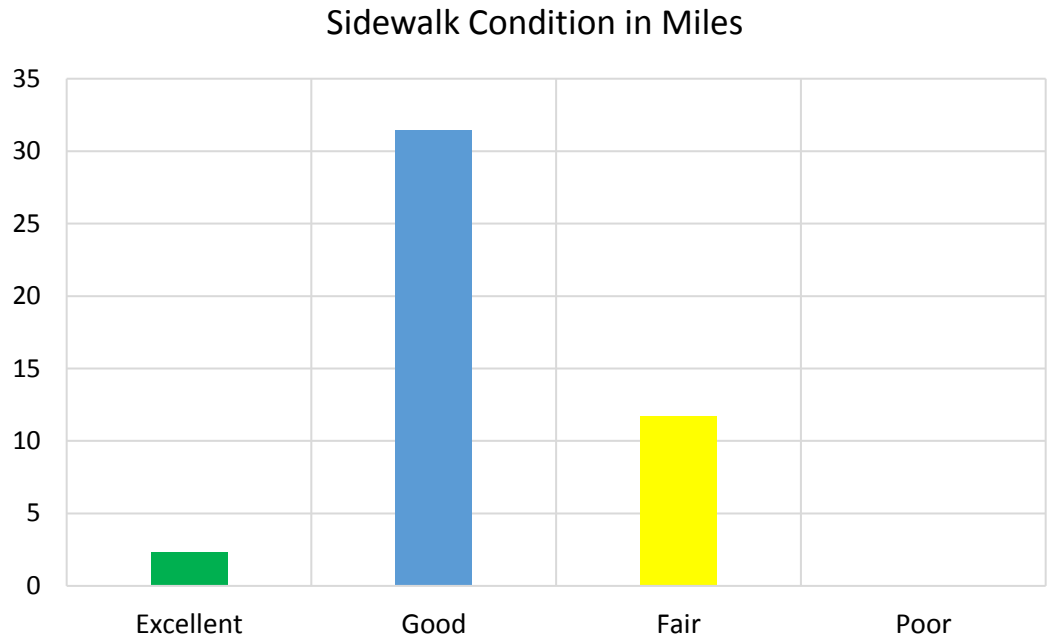
Sidewalk Condition Summary in Miles

The following table and chart summarize the mileage of Sudbury sidewalk by condition.

Table 6 Sidewalk Condition in Miles

Sidewalk Condition	Miles
Excellent	2.34
Good	31.45
Fair	11.70
Poor	0.0
Total	**45.49**

Figure 8 Sidewalk Condition in Miles



Sidewalk Backlog

Cost to Repair Existing Sidewalks

To estimate the cost of repairing sidewalk, VHB made the following assumptions.

- Full repair of all Poor Sidewalks
- 50% repair of all Fair Sidewalks
- Sidewalk width will be 5 feet
- All repairs are completed using bituminous concrete

Repair Type	Quantity	Unit Cost	Cost
50% Fair Sidewalk Repair	17,157 SY	$37/SY	$634,817
Poor Sidewalk Repair	0 SY	$37/SY	$0
Total:			**$634,817**

It is estimated that it would cost the town $634,817 to bring the current sidewalk network to an overall condition of excellent or good.

Sidewalk Prioritization

In order to prioritize the backlog of sidewalk repairs, VHB developed the following prioritization plan.

The prioritization is based on 3 factors.

1. Sidewalk Condition Priority Factor (Condition_Factor) (40%)
2. Location Priority Factor (Location_Factor) (40%)
3. Width Priority Factor (Width_Factor (20%)

The Sidewalk Condition Priority Factor is based directly on the General Condition Rating

- Excellent = 0
- Good = 33
- Fair = 66
- Poor/Reconstruct = 100

The Location Priority Factor give priority to sidewalks near high pedestrian traffic generators such as schools, public buildings and commercial zones. The town classified sidewalks into two tiers.

- Tier 1 = 100
- Tier 2 = 50

The Sidewalk Width Priority Factor (CompPF) takes into account the width of the sidewalk, with narrow sections having a higher priority factor

The equation for Sidewalk Priority Index (SPI) is thus

SPI = 0.4 (Condition_Factor) + 0.4 (Location_Factor) + 0.2 (Width_Factor)

A full report of sidewalk segments listed with SPI is in the appendices.

Sidewalk Examples

Figure 9 Excellent Condition

Powder Mill Road

Figure 10 Good Condition

Hunter's Run

Figure 11 Fair Condition

Phillips Road

6

Guardrail Inventory

Data Collection

VHB created a geodatabase to store the location, inventory, and attribute data for all features required in this self-evaluation. Feature classes were set up for the guardrail inventory. This database was posted to ArcGIS Online so that it would be cloud accessible.

Then, using data enabled tablet computers with Global Positioning System (GPS) receivers and ArcGIS Collector application, VHB walked the PAR's to record the inventory data. Each guardrail segment was evaluated for type, start treatment, end treatment, post type, post spacing, condition, and accident notification.

Guardrail Evaluation Criteria:

- Guardrail Type
- Start Treatment
- End Treatment
- Post Type
- Post Spacing
- Condition
- Accident Notification
- Accident Extent

Post type was determined by the material of the post component of the guardrail system. The materials most commonly found include wood, galvanized steel, combination (material varied throughout), concrete, and bridge rail.

Post spacing was recorded by measuring the average distance (in feet) between posts along the guardrail section.

The guardrail sections were flagged if an accident has occurred and compromised any portion of the guardrail system, requiring immediate replacement. The extent of damage was recorded as limited, moderate, or extensive.

The remaining guardrail parameters are further described in the tables below.

Table 7 Guardrail Type

Guardrail Type	Description
Bridge Rail	Any rail that can be found on bridges.
Cable Barrier (High and Low Tension)	Any cable system. Usually found in medians.
Thrie-Beam Steel	Wider beam with 3 bumps. Used in heavier traffic areas and around bridge underpass usually.
W-Beam Steel	W shaped metal beam. Most frequently occurring guardrail system.
Weathering Steel	W-beam that is rusted, or rusts to be a brown color intentionally.
Wood Timber	Usually all wood, post and rail. Usually used for aesthetic purposes. Typically found in parking lots, private drives, or low speed boulevards.

Table 8 Start/End Treatment

Start/End Treatment	Description
Blunt End	Classic flared end. Usually found on end of guardrail not facing oncoming traffic.
Bridge Attachment	Any guardrail type that is physically attached to the start/end of a bridge section
Roll Over	The guardrail beam is buried into the ground or rolled over. Also called a buried end.
Terminal End	Rounded steel. Resembles a boxing glove.

Table 9 Condition Assessment

Condition Assessment	Description
Excellent	New guardrail and posts. No damage or wear.
Good	Minimal weathering to guardrail and/or posts. No damage to rail, posts, or end treatments.
Fair	Normal weathering and minor dents to rail. No structural issues visible. Routine maintenance may be necessary for posts, etc. If accident has occurred, it is isolated while the rest of the guardrail is functional.
Poor	Significant weathering and dents to rail, covering less than 50% of span. Potential and visible structural issues (guardrail curving, slanted because posts are compromised). If damage to end treatment has occurred and maintenance is necessary to correct.
Very Poor	Significant weathering and damage to rail system, covering over 50% of span. Total loss of structural integrity. This may be due to an accident or from age. Rail, start/end treatment, and/or posts need replacing.

Guardrail Inventory Summaries

Guardrail Type Summary

The following table summarizes the length of Sudbury guardrail by type.

Table 10 Guardrail Type by Length

Guardrail Type	Length (ft)
Bridge Rail	377
Other	278
W-Beam Steel	24,785
Wood Timber	9,282
Total	**34,722**

Condition	Length (ft)
Excellent	443
Good	29,717
Fair	4,293
Poor	269
Very Poor	0
Total	**34,722**

Guardrail Backlog

Cost to Repair Existing Guardrail

The guardrail repair cost estimates are developed using an price of of $37/lf for W-Beam and $125/lf for Steel Backed Timber. Guardrail section that were given a condition rating of excellent or good were assumed to not need any repairs at this time, yielding an estimate repair cost of $0. However, sections that were rated Fair, Poor, or Very Poor, were estimated to be replaced in full. All Timber guardrails were assumed to be replaced with Steel Backed Timber.

The following table details the total cost grouped by the condition rating.

Table 11 Guardrail Conditional Repair Cost Summary

Type	Quantity (ft)	Unit Cost	Cost
W-Beam Steel	3,787	$37/lf	$140,119
Wood Timber	483	$125/lf	$60,375
Total:			**$200,494**

It is estimated that it would cost the town approximately $200,494 to bring the current guardrail network to an overall condition of excellent or good.

Guardrail Prioritization

In order to prioritize the backlog of sidewalk repairs, VHB developed the following prioritization plan.

The prioritization is based on 2 factors.

- Guardrail Condition Priority Factor (Condition_Factor) (80%)
- Functional Class Priority Factor (Functional_Factor) (20%)

The Guardrail Condition Priority Factor is based directly on the General Condition Rating

- Excellent = 0
- Good = 25
- Fair = 50
- Poor = 75
- Very Poor = 100

The Functional Class Priority Factor give priority to guardrail segments based on the functional class of the roadway it is servicing. The priority factors associated with each functional class are detailed below.

- Local – Dead End = 0
- Local – Thru = 33

› Collector = 66
› Arterials = 100

Finally, a weighted equation was used to calculate a "Guardrail Priority Index" (GPI) for each guardrail section. The condition factor contributed to 80% of the GPI while the functional class contributed the remaining 20%. This weighted equation allows for the segments in poor or very poor condition to have a higher priority when selecting potential repair locations. All guardrail segments with a condition rating of "Excellent" were given a priority index of 0, as these sections would not need any repairs therefore should not have a high priority to be repaired. The following equation was used to develop the GPI.

GPI = 0.8 (Condition_Factor) + 0.2 (Functional_Factor)

A full report of guardrail segments sorted by GPI is in the appendices.

Guardrail Examples

Figure 12 Excellent Condition

Old Lancaster Road

Figure 13 Good Condition

North Road

Figure 14 Fair Condition

North Road

Figure 15 Poor Condition

Raymond Road

Feature Relationships

After the data was collected in the field, a comprehensive process was undertaken to create data relationships/links between features so that they could be analyzed and/or reported together.

Street/Route Reference

The Town of Sudbury has long utilized a pavement management system upon which the pavement segment inventory was depicted and referenced to a Roadway Route System. Each Roadway Route is a continuous length of roadway with the same name. It has a beginning and an end identified, and has a linear referencing system (stations).

In order to locate features when GIS mapping is not available, every recorded feature was attributed with its corresponding "RouteID" and station (distance in feet from the beginning of the road) Line features have a beginning and end station, such as crosswalks and sidewalks. Point features such as ramps and pinch points have a single station. Each feature also has an approximate offset (distance) from the road centerline.

In addition, all ramps, obstructions, cross slopes, pinch points, and trip hazards were attributed with their corresponding sidewalk segment ID, so that they could be summarized for each sidewalk. Crosswalks were attributed with the ID of both ramps that they serve.

7

Closing Remarks

Based on descriptive information recorded in 2017. The Sudbury pavement and sidewalk management system gives Town decision-makers a picture of existing roadway infrastructure conditions and a dollar estimate to improve streets in poor condition while protecting those pavements already in good condition. This also allows Town staff to logically and systematically integrate results of this plan with other critical master plans in Town.

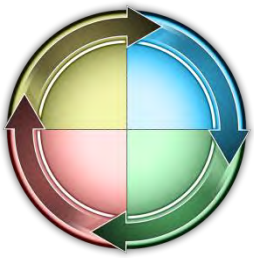
The Pavement Management System being implemented by the Town is a planning tool, with primary functions of determining the funding levels required to achieve Town wide condition goals, and to identify candidate road projects to achieve those goals. Any project list generated by the system needs to be reviewed by the Town Public Works staff and adjusted based on numerous factors, including coordination with utility work, and geographic issues.

Documents, Project Planning, and Next Steps

This report presents the results of a "network level study", in that it uses a customized but common approach to recommending repairs to all features (pavement and guardrails) and a systematic approach to prioritizing projects using a limited range of information. This allows us to summarize the results of the study, and project the effects of various budgeting scenarios. The next step in pavement management moves to the "project level" phase, in which actual projects are selected and specific repairs types are determined. For this purpose, VHB has provided the data inventory to the town in alternative formats, including

larger scale maps, and Microsoft Excel spreadsheets. This allows the town to customize repair recommendations, modify costs, and prioritize the projects on a case by case basis, and incorporating local knowledge.

Recommendations – *Pavement Management*



- Budget adequate funds to achieve pavement condition goals
- Make timely maintenance repairs
- Repair localized base problems before applying an overlay
- Address major rehabilitation needs as funding allows
- **Develop multi-year road programs**
- Coordinate with local utilities to perform upgrades and repairs in advance of projected construction projects
- **Perform project level testing prior to major rehabilitation projects to ensure proper life of new pavement**
- **Provide for construction inspection at the plant and in the field to ensure quality material is provided and quality work is being performed**
- Update pavement management system to reflect work that is done (maintains accuracy of system)
- Update pavement conditions at a minimum of every 4 years or 25%per year
- Track specific and overall conditions periodically
- **Evaluate funding levels periodically**

Appendices

Appendix A: Repair Alternatives

Unit Costs

In order to estimate pavement maintenance and rehabilitation budget needs, a short list of typical pavement treatments is developed and applied to roads based on condition and other factors. Each representative repair alternative is assigned a unit cost. This cost was developed based on contract bids received by the town, and state bid averages from the MassDOT. These costs are used for system wide analysis and summary.

Name	Unit Cost $/SY
Reclaim Arterial w/Drainage	62.6
Reclaim Collector w/Drainage	48.1
Reclaim Local w/Drainage	33.7
Reclaim Arterial	45.9
Reclaim Collector	35.3
Reclaim Local	24.7
Mill/Overlay A/C	15.9
Mill/Overlay Local	15
Prev. Maint w/Patch	7
Prev. Maint	4.7
Crackseal/Patch	1.8
Crackseal	.40

Appendix B: Pavement Inventory Report (alpha)

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
AARON ROAD	FIRECUT LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	622	100
ABBOTTSWOOD	CAMPERDOWN LANE	GATE AT COUNTRY VILLAGE LAN	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,190	95
ADAMS ROAD	DUDLEY ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	834	78
ALLAN AVENUE	ALLENE AVENUE	REEVES STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	565	57
ALLEN PLACE	MASSASOIT AVENUE	INDIAN RIDGE ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	507	90
ALLENE AVENUE	HUDSON ROAD	ALLAN AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	647	47
ALTA ROAD	WOODSIDE ROAD	CLIFFORD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	914	80
AMANDA ROAD	DUTTON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,461	95
AMES ROAD	LANDHAM ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,798	95
ANSELM WAY	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,201	77
ARBORETUM WAY	MAYNARD ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,127	100
ARBORWOOD ROAD	BIRCHWOOD AVENUE	ELMWOOD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	225	93
ARROWHEAD ROAD	ENGLISH ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	437	86
ATKINSON LANE	DUTTON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,582	81
AUGUST ROAD	HUDSON ROAD	PRIVATE PORTION	Hot Mix Asphalt	Local- Thru Street	HMA	Public	400	79
AUSTIN DRIVE	HILLTOP ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	463	55
AUSTIN ROAD	TANBARK ROAD	100' S OF HOBART ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,234	55
AUSTIN ROAD	100' S OF HOBART ROAD	PEAKHAM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,243	86
AUTUMN STREET	DUTTON ROAD	PINE STREET	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	909	100
AXDELL ROAD	AUSTIN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	630	85
BABE RUTH DRIVE	HUDSON ROAD	ATKINSON LANE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,352	83
BALCOM ROAD	RUDDOCK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,370	83
BALDWIN DRIVE	MAYNARD ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	247	95
BARBARA ROAD	RONALD ROAD	AARON ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	985	100
BARNET ROAD	POWERS ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	724	90
BARTON DRIVE	DUTTON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	4,079	89
BASSWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,911	89
BECKWITH STREET	PELHAM ISLAND ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	976	85
BEECHWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,867	89
BELCHER DRIVE	WILLIS ROAD	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	4,311	88
BENT BROOK ROAD	BARTON DRIVE	MILLPOND ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	802	60
BENT ROAD	PEAKHAM ROAD	ROBBINS ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,842	78
BENT ROAD	ROBBINS ROAD	OLD MEADOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	568	59
BIGELOW DRIVE	BOSTON POST ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	3,010	80
BIRCHWOOD AVENUE	WILLIS LAKE DRIVE	GREAT LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,016	89
BISHOP LANE	THOMPSON DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,094	78

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
BLACKMER ROAD	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,854	95
BLACKSMITH DRIVE	DAKIN ROAD	WILLARD GRANT ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,655	99
BLANDFORD DRIVE	WILLIS ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	395	100
BLUEBERRY HILL LANE	PEAKHAM ROAD	PEAKHAM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,767	90
BOWDITCH ROAD	BOSTON POST ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	1,625	86
BOWEN CIRCLE	PLYMPTON ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	665	86
BOWKER DRIVE	FORD ROAD	ELSBETH ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,225	90
BRADLEY PLACE	MASSASOIT AVENUE	INDIAN RIDGE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	535	99
BRENTWOOD ROAD	BRIAR PATCH LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	565	99
BREWSTER ROAD	GOODMANS HILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,642	90
BRIANT DRIVE	WILLIS ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,514	79
BRIAR PATCH LANE	PRATTS MILL ROAD	HOLLOW OAK DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	860	95
BRIDLE PATH	TALL PINE DRIVE	TALL PINE DRIVE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,547	89
BRIMSTONE LANE	BOSTON POST ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Local- Thru Street	None	Public	4,323	74
BROOKDALE LANE	BROOKDALE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	323	95
BROOKDALE ROAD	LANDHAM ROAD	STOCK FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,348	89
BROOKS ROAD	LANDHAM ROAD	MURRAY DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	920	78
BROOKSIDE FARM LANE	LANDHAM ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	641	93
BROWNSTONE LANE	OLD LANCASTER ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	559	89
BUCKMASTER DRIVE	BELCHER DRIVE	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	832	100
BULKLEY ROAD	AUSTIN ROAD	560' W OF AUSTIN ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	560	95
BULKLEY ROAD EXT	560' W OF AUSTIN ROAD	AMANDA ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	369	95
BUTLER ROAD	FAIRBANK ROAD	THORNBERRY LANE	Hot Mix Asphalt	Local- Dead End	None	Public	732	75
BUTLER ROAD	THORNBERRY LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,708	93
CAIL FARM ROAD	CONCORD ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	221	95
CAKEBREAD DRIVE	WITHERELL DRIVE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	563	100
CAMPERDOWN LANE	FAIRBANK ROAD	1000' E OF FAIRBANK ROAD	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,000	100
CAMPERDOWN LANE EXT	1000' E OF FAIRBANK ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,504	95
CANDLEWOOD CIRCLE	PEAKHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	293	69
CANDY HILL ROAD	CONCORD ROAD	PLYMPTON ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,472	87
CANTERBURY DRIVE	HAMPSHIRE STREET	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,631	78
CARDING MILL ROAD	BOSTON POST ROAD	BIGELOW DRIVE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,311	80
CARRIAGE WAY	FRENCH ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,266	89
CEDAR CREEK ROAD	WILLARD GRANT ROAD	POWERS ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,246	63
CENTER STREET	PRATTS MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	970	88
CHANTICLEER ROAD	FORD ROAD	LONGFELLOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,817	100

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
CHECKERBERRY CIRCLE	POSSUM LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	571	89
CHRISTOPHER LANE	STOCK FARM ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	790	46
CHURCH STREET	CONCORD ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	549	86
CHURCHILL STREET	SAXONY DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,750	95
CIDER MILL ROAD	RAYMOND ROAD	RAMBLING ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,327	92
CLARK LANE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,414	90
CLARK ROAD	PLYMPTON ROAD	NEW BRIDGE ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,392	89
CLIFFORD ROAD	ALTA ROAD	WARREN ROAD	Hot Mix Asphalt	Local- Dead End	HMA	Public	383	82
CLIFFORD ROAD	WARREN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	714	88
COBBLESTONE PLACE	GREYSTONE LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	500	88
CODJER LANE	CONCORD ROAD	UNION AVENUE	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,568	79
CODJER LANE	UNION AVENUE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	2,629	88
CODMAN DRIVE	MORSE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,090	99
COLBURN CIRCLE	ROBERT BEST ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	559	79
COLONIAL ROAD	OLD LANCASTER ROAD	200' W OF HOMESTEAD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	876	81
COLONIAL ROAD	200' W OF HOMESTEAD AVENUE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,145	57
CONCORD ROAD	BOSTON POST ROAD	CODJER LANE	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	County	2,523	88
CONCORD ROAD	CODJER LANE	UNION AVENUE	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	County	1,968	88
CONCORD ROAD	UNION AVENUE	GOODMANS HILL ROAD	Hot Mix Asphalt	Major Collector	Cape Cod Berm	County	2,379	78
CONCORD ROAD	GOODMANS HILL ROAD	100' N OF CONCORD ROAD (Y INT)	Hot Mix Asphalt	Major Collector	Sloped Granite	County	1,310	100
CONCORD ROAD	100' N OF CONCORD ROAD (Y INT)	NEW BRIDGE ROAD	Hot Mix Asphalt	Minor Collector	HMA	County	4,531	83
CONCORD ROAD	NEW BRIDGE ROAD	400' N OF LINCOLN ROAD	Hot Mix Asphalt	Minor Collector	HMA	County	1,405	76
CONCORD ROAD	400' N OF LINCOLN ROAD	PANTRY ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	County	3,735	90
CONCORD ROAD	PANTRY ROAD	TWIN POND LANE	Hot Mix Asphalt	Minor Collector	None	County	1,968	72
CONCORD ROAD	TWIN POND LANE	4700' S OF TOWN LINE	Hot Mix Asphalt	Minor Collector	None	County	3,770	72
CONCORD ROAD	4700' S OF TOWN LINE	CONCORD TOWN LINE	Hot Mix Asphalt	Minor Collector	None	County	4,701	80
CONCORD ROAD (Y INT)	OLD SUDBURY ROAD	CONCORD ROAD	Hot Mix Asphalt	Local- Thru Street	Sloped Granite	County	276	100
CONFIDENCE WAY	CONCORD ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	266	83
CORTLAND LANE	FAIRBANK ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Sloped Granite	Public	559	100
COUNTRY VILLAGE LANE	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,379	99
CRAIG LANE	MARLBORO ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	520	89
CRANBERRY CIRCLE	POWDER MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,711	88
CRESCENT LANE	MAYNARD ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,523	80
CRESTVIEW DRIVE	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	286	74
CROWN POINT ROAD	WILLIS ROAD	PENDLETON ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	984	89
CRYSTAL LAKE DRIVE	HUDSON ROAD	BASSWOOD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,207	93

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
CUDWORTH LANE	BRIANT DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,138	87
CURRY LANE	GREENWOOD ROAD	ELAINE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	567	100
CURTISS CIRCLE	ROBERT BEST ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	682	71
CUTLER FARM ROAD	WOODSIDE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,418	90
CUTTING LANE	ARBORETUM WAY	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	628	87
DAKIN ROAD	NORTH ROAD	CONCORD TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	3,743	89
DARVELL DRIVE	WITHERELL DRIVE	BELCHER DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	690	77
DAWSON DRIVE	JOAN AVENUE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,403	99
DEACON LANE	PANTRY ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	852	88
DEER POND ROAD	MAYNARD FARM ROAD	MAYNARD FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,195	80
DEMARCO ROAD	OLD LANCASTER ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	839	56
DOROTHY ROAD	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	843	58
DOUGLAS DRIVE	STONE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	679	100
DRUM LANE	COLONIAL ROAD	DEMARCO ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,347	62
DUDLEY ROAD	BOSTON POST ROAD	NOBSCOT ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	2,978	67
DUNSTER ROAD	HAYNES ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,117	68
DUTTON ROAD	HUDSON ROAD	FRENCH ROAD	Hot Mix Asphalt	Minor Collector	None	Public	3,802	63
DUTTON ROAD	FRENCH ROAD	963' S OF OLD GARRISON ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,446	68
DUTTON ROAD	963' S OF OLD GARRISON ROAD	OLD GARRISON ROAD	Hot Mix Asphalt	Minor Collector	None	Public	964	87
DUTTON ROAD	OLD GARRISON ROAD	TANBARK ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	2,724	86
DUTTON ROAD	TANBARK ROAD	PRATTS MILL ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	2,450	86
DUTTON ROAD	PRATTS MILL ROAD	MOORE ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	1,972	85
DUTTON ROAD	MOORE ROAD	HUDSON ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	957	86
EASY STREET	CUL DE SAC	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,297	89
EDDY STREET	LANDHAM ROAD	BROOKDALE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,699	90
ELAINE ROAD	FORD ROAD	LONGFELLOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,313	99
ELDERBERRY CIRCLE	FAIRBANK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	273	90
ELLIOT ROAD	OLD MEADOW ROAD	HORSE POND ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	437	89
ELMWOOD AVENUE	LAKE SHORE DRIVE	ARBORWOOD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	384	88
ELSBETH ROAD	FORD ROAD	MAYNARD TOWN LINE	Hot Mix Asphalt	Local- Thru Street	None	Public	771	99
EMERSON WAY	MORSE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	849	95
ENGLISH ROAD	CANTERBURY DRIVE	ARROWHEAD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	399	77
EVERGREEN ROAD	HORSE POND ROAD	STONEBROOK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,267	99
FAIRBANK ROAD	HUDSON ROAD	MAYNARD ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	4,358	72
FAIRBANK ROAD (Y INT)	HUDSON ROAD	FAIRBANK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	88	86
FAIRHAVEN CIRCLE	HUDSON ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	858	78

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
FARM LANE	MOSSMAN ROAD	OLD COACH ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	449	90
FIELD ROAD	DAKIN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,038	76
FIELDSTONE FARM ROAD	RICE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,186	71
FIRECUT LANE	STEARNS LANE	AARON ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,157	99
FIRECUT LANE EXT	AARON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	375	100
FLINTLOCK LANE	WOODMERE DRIVE	STARVIEW DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	963	99
FORD ROAD	GREAT ROAD	BELCHER DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	2,536	84
FORD ROAD	BELCHER DRIVE	WILLIS ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,624	94
FOREST STREET	PEAKHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,465	83
FOX HILL DRIVE	MARY CATHERINE LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	965	80
FOX RUN	PEAKHAM ROAD	SADDLE RIDGE ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	3,015	79
FRANKLIN PLACE	MASSASOIT AVENUE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	281	90
FRENCH ROAD	PEAKHAM ROAD	GARRISON ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	105	77
FRENCH ROAD	OLD GARRISON ROAD	DUTTON ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,914	100
FROST LANE	CONCORD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	660	85
GERRY DRIVE	OLD LANCASTER ROAD	WASH BROOK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	937	99
GLEN LANE	DUTTON ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	263	89
GOODMANS HILL ROAD	CONCORD ROAD	GOODMANS HILL ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	3,929	78
GOODMANS HILL ROAD	GOODMANS HILL ROAD	BOSTON POST ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	4,360	76
GOODNOW ROAD	HUDSON ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,898	82
GREAT LAKE DRIVE	HUDSON ROAD	BIRCHWOOD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,179	89
GREAT ROAD	NORTH ROAD	MAYNARD TOWN LINE	Hot Mix Asphalt	Major Collector	HMA	County	1,647	86
GREEN HILL ROAD	BOSTON POST ROAD	GOODMANS HILL ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,377	88
GREENWOOD ROAD	FORD ROAD	LONGFELLOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,217	100
GREYSTONE LANE	CUL DE SAC	JULIANS WAY	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	850	76
GREYSTONE LANE	JULIANS WAY	DEAD END	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	3,277	55
GRIFFIN LANE	CROWN POINT ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	518	89
GRINDSTONE LANE	COUNTRY VILLAGE LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	977	99
GRISCOM ROAD	PELHAM ISLAND ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,322	86
GUZZLEBROOK DRIVE	BECKWITH STREET	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	905	85
HADLEY ROAD	HAYNES ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	999	88
HAMBLIN LANE	MARLBORO ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	207	86
HAMMOND CIRCLE	BENT ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	619	59
HAMPSHIRE STREET	WILLIS ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Sloped Granite	Public	983	79
HARNESS LANE	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,726	88
HARNESS LANE EXT	HARNESS LANE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	252	88

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
HARVARD DRIVE	FORD ROAD	LONGFELLOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,174	68
HAWES ROAD	DAKIN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,012	80
HAWTHORNE DRIVE	PELHAM ISLAND ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	556	83
HAYDEN CIRCLE	BENT ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	619	51
HAYNES ROAD	PANTRY ROAD	MARLBORO ROAD	Hot Mix Asphalt	Minor Collector	None	Public	1,609	94
HAYNES ROAD	MARLBORO ROAD	PUFFER LANE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,659	77
HAYNES ROAD	PUFFER LANE	NORTH ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,704	77
HEMLOCK ROAD	CUL DE SAC	300' S OF TANBARK ROAD	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,644	64
HEMLOCK ROAD	300' S OF TANBARK ROAD	PARKING LOT	Hot Mix Asphalt	Local- Dead End	None	Public	443	60
HENRYS MILL LANE	FRENCH ROAD	CARRIAGE WAY	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,539	90
HERMITAGE STREET	HAMPSHIRE STREET	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	705	83
HICKORY ROAD	ROBBINS ROAD	STONE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,094	100
HICKORY ROAD	STONE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	735	100
HILLTOP ROAD	MORSE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	920	89
HOBART ROAD	AUSTIN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	592	56
HOLLOW OAK DRIVE	BRENTWOOD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	866	95
HOMESTEAD STREET	PEAKHAM ROAD	COLONIAL ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	531	75
HOP BROOK LANE	OAK HILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	717	89
HOPESTILL BROWN ROAD	WOODSIDE ROAD	WOODSIDE ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,345	81
HORSE POND ROAD	BOSTON POST ROAD	TALL PINE DRIVE	Hot Mix Asphalt	Minor Collector	None	Public	4,009	83
HORSE POND ROAD	TALL PINE DRIVE	PEAKHAM ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,460	83
HOWELL ROAD	STONE ROAD	ROBBINS ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	823	59
HUDSON ROAD	CONCORD ROAD	100' E OF MAYNARD ROAD	Hot Mix Asphalt	Minor Arterial	Sloped Granite	Public	1,814	100
HUDSON ROAD	100' E OF MAYNARD ROAD	OLD LANCASTER ROAD	Hot Mix Asphalt	Major Collector	None	Public	1,638	83
HUDSON ROAD	OLD LANCASTER ROAD	TEAKETTLE LANE	Hot Mix Asphalt	Major Collector	None	Public	2,429	83
HUDSON ROAD	TEAKETTLE LANE	100' W OF AUGUST ROAD	Hot Mix Asphalt	Major Collector	None	Public	2,170	83
HUDSON ROAD	100' W OF AUGUST ROAD	100' W OF INTERVALE ROAD	Hot Mix Asphalt	Major Collector	HMA	Public	3,618	82
HUDSON ROAD	100' W OF INTERVALE ROAD	CRYSTAL LAKE DRIVE	Hot Mix Asphalt	Major Collector	None	Public	2,261	72
HUDSON ROAD	CRYSTAL LAKE DRIVE	STOW TOWN LINE	Hot Mix Asphalt	Major Collector	None	Public	3,208	95
HUNT ROAD	RIDGE HILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,575	80
HUNTERS RUN	MEACHEN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	462	99
INDIAN RIDGE ROAD	KING PHILIP ROAD	OLD LANCASTER ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,892	89
INTERVALE ROAD	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,270	89
IRONWORKS ROAD	TAINTOR DRIVE	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,356	87
JACK PINE DRIVE	PRIDES CROSSING ROAD	RED OAK DRIVE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,825	90
JARMAN ROAD	HORSE POND ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	2,153	71

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
JASON DRIVE	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	634	84
JULIANS WAY	HAYNES ROAD	GREYSTONE LANE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,497	85
JULY ROAD	HUDSON ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,115	57
JUNIPER ROAD	WAKE ROBIN ROAD	WAKE ROBIN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,096	88
KATO DRIVE	GOODMANS HILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,191	80
KATO SUMMIT	KATO DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	264	84
KAY STREET	BROOKDALE ROAD	STOCK FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,544	89
KENDALL ROAD	HOBART ROAD	TANBARK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,151	81
KENDRA LANE	WILLIS ROAD	CUDWORTH LANE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	928	78
KING PHILIP ROAD	BOSTON POST ROAD	BOSTON POST ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,773	87
LAFAYETTE DRIVE	BOSTON POST ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,639	90
LAKE SHORE DRIVE	LAKEWOOD DRIVE	GREAT LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	353	99
LAKEWOOD DRIVE	HUDSON ROAD	BASSWOOD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,039	99
LAKEWOOD DRIVE	BASSWOOD AVENUE	LAKE SHORE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	509	89
LANDHAM ROAD	BOSTON POST ROAD	1666' S OF BOSTON POST ROAD	Hot Mix Asphalt	Major Collector	Granite	County	1,666	80
LANDHAM ROAD	1666' S OF BOSTON POST ROAD	WOODSIDE ROAD	Hot Mix Asphalt	Major Collector	HMA	County	2,420	85
LANDHAM ROAD	WOODSIDE ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Major Collector	HMA	County	3,144	85
LANDHAM ROAD (NB Y INT)	LANDHAM ROAD (NB LEG)	BOSTON POST ROAD	Hot Mix Asphalt	Major Collector	Granite	County	140	99
LANDHAM ROAD (NB LEG)	LANDHAM ROAD	BOSTON POST ROAD	Hot Mix Asphalt	Major Collector	Granite	County	176	98
LANDHAM ROAD (SB Y INT)	BOSTON POST ROAD	LANDHAM ROAD	Hot Mix Asphalt	Major Collector	Granite	County	119	90
LANDS END LANE	WARREN ROAD	ROBERT FROST ROAD	Hot Mix Asphalt	Local- Dead End	None	Public	1,475	59
LAUREL CIRCLE	WHITE OAK LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	445	83
LEDGE ROAD	MOSSMAN ROAD	SYLVAN WAY	Hot Mix Asphalt	Local- Thru Street	None	Public	996	89
LEE ANNE CIRCLE	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	169	76
LETTERY CIRCLE	WOODSIDE ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,101	87
LILLIAN AVENUE	HUDSON ROAD	REEVES STREET	Hot Mix Asphalt	Local- Thru Street	HMA	Public	755	56
LINCOLN LANE	LINCOLN ROAD	LINCOLN LANE PRIVATE SECTION	Hot Mix Asphalt	Local- Thru Street	None	Public	2,250	56
LINCOLN ROAD	CONCORD ROAD	SAWMILL LANE	Hot Mix Asphalt	Minor Collector	None	Public	681	63
LINCOLN ROAD	SAWMILL LANE	WATER ROW	Hot Mix Asphalt	Minor Collector	None	Public	3,870	63
LINCOLN ROAD	WATER ROW	WEIR HILL ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,533	73
LINCOLN ROAD	WEIR HILL ROAD	WAYLAND TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	3,479	73
LINCOLN ROAD (EB Y INT)	LINCOLN ROAD	WAYLAND TOWN LINE	Hot Mix Asphalt	Local- Thru Street	Sloped Granite	Public	180	89
LINCOLN ROAD (WB Y INT)	CONCORD ROAD	WAYLAND TOWN LINE	Hot Mix Asphalt	Local- Thru Street	Sloped Granite	Public	292	89
LINDEN ROAD	POPLAR STREET	MAGNOLIA ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	758	79
LOKER ROAD	WILLIS ROAD	BALCOM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	399	89
LOMBARD LANE	RAMBLING ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	628	67

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
LONGFELLOW ROAD	NORTH ROAD	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	4,169	89
LYNNE ROAD	LANDHAM ROAD	BROOKDALE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,060	99
MAGNOLIA ROAD	DUTTON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	936	83
MAPLE AVENUE	BOSTON POST ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,404	94
MAPLEWOOD AVENUE	LAKEWOOD DRIVE	DEAD END	Hot Mix Asphalt	Local- Thru Street	None	Public	1,033	89
MARK LANE	MAYNARD ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	661	88
MARKED TREE ROAD	PEAKHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	832	67
MARLBORO ROAD	MAYNARD ROAD	WILLIS ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,277	86
MARLBORO ROAD	WILLIS ROAD	MORSE ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,917	79
MARLBORO ROAD	MORSE ROAD	HAYNES ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,607	76
MARTIN DRIVE	MAYNARD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	966	55
MARY CATHERINE LANE	NORTH ROAD (W)	100' N OF NORTH ROAD (E)	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	848	77
MARY CATHERINE LANE	100' N OF NORTH ROAD (E)	NORTH ROAD (E)	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,017	85
MASSASOIT AVENUE	BOSTON POST ROAD	FRANKLIN PLACE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,522	94
MAYBURY ROAD	GREAT ROAD	GREAT ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	691	74
MAYNARD FARM CIRCLE	MAYNARD FARM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	653	74
MAYNARD FARM ROAD	POWERS ROAD	POWERS ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	4,380	78
MAYNARD ROAD	HUDSON ROAD	400' N OF HUDSON ROAD	Hot Mix Asphalt	Minor Arterial	HMA	County	400	84
MAYNARD ROAD	400' N OF HUDSON ROAD	WILLIS ROAD	Hot Mix Asphalt	Minor Arterial	HMA	County	3,442	84
MAYNARD ROAD	WILLIS ROAD	MARTIN DRIVE	Hot Mix Asphalt	Minor Arterial	HMA	County	1,285	68
MAYNARD ROAD	MARTIN DRIVE	MARLBORO ROAD	Hot Mix Asphalt	Minor Arterial	HMA	County	1,514	87
MAYNARD ROAD	MARLBORO ROAD	MAYNARD TOWN LINE	Hot Mix Asphalt	Minor Arterial	None	County	4,279	69
MCLEAN DRIVE	PRATTS MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	996	89
MEACHEN ROAD	MARLBORO ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,334	95
MEADOW DRIVE	OLD LANCASTER ROAD	PHEASANT AVENUE	Hot Mix Asphalt	Local- Dead End	None	Public	383	100
MEADOW DRIVE	PHEASANT AVENUE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	754	89
MEADOWBROOK CIRCLE	PEAKHAM ROAD	MEADOWBROOK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,164	90
MEADOWBROOK ROAD	WILLOW ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	837	69
METACOMET WAY	WINSOR ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	540	89
MICHAEL LANE	WIDOW RITES LANE	CUDWORTH LANE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	297	89
MIDDLE ROAD	FIRECUT LANE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	991	90
MILLPOND ROAD	BARTON DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,215	87
MINEBROOK ROAD	MAYNARD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,039	84
MINUTEMAN LANE	MARLBORO ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	690	99
MOORE ROAD	DUTTON ROAD	FIRECUT LANE	Hot Mix Asphalt	Local- Dead End	None	Public	2,613	72
MOORE ROAD	FIRECUT LANE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	2,260	72

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
MORAN CIRCLE	RAYMOND ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,061	77
MORSE ROAD	CONCORD ROAD	WAKE ROBIN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,807	84
MORSE ROAD	WAKE ROBIN ROAD	MARLBORO ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	3,224	84
MOSSMAN ROAD	MARLBORO ROAD	LEDGE ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	3,492	80
MOSSMAN ROAD	LEDGE ROAD	WILLIS ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,257	80
MOSSMAN ROAD	WILLIS ROAD	NORTH ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	3,016	67
MUNNINGS DRIVE	BELCHER DRIVE	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	985	100
MURRAY DRIVE	AMES ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	644	83
MUSKET LANE	STARVIEW DRIVE	WOODMERE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	787	95
NASHOBA ROAD	GOODMANS HILL ROAD	PURITAN LANE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	872	90
NEW BRIDGE ROAD	CONCORD ROAD	CLARK ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,443	76
NEW BRIDGE ROAD	CLARK ROAD	WATER ROW	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,452	81
NEWTON ROAD	WHISPERING PINE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	614	95
NOBSCOT ROAD	BOSTON POST ROAD	DUDLEY ROAD	Hot Mix Asphalt	Major Collector	Cape Cod Berm	County	3,333	66
NOBSCOT ROAD	DUDLEY ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Major Collector	None	County	2,914	88
NORMANDY DRIVE	SAXONY DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,685	95
NORTH ROAD	MAYNARD TOWN LINE	GREAT ROAD	Hot Mix Asphalt	Major Collector	Sloped Granite	Public	1,401	87
NORTH ROAD	GREAT ROAD	MOSSMAN ROAD	Hot Mix Asphalt	Major Collector	Sloped Granite	County	1,809	86
NORTH ROAD	MOSSMANN ROAD	WILLARD GRANT ROAD	Hot Mix Asphalt	Major Collector	HMA	County	2,960	87
NORTH ROAD	WILLARD GRANT ROAD	PANTRY ROAD	Hot Mix Asphalt	Major Collector	None	County	2,225	87
NORTH ROAD	PANTRY ROAD	WINDMILL DRIVE	Hot Mix Asphalt	Major Collector	HMA	County	1,205	80
NORTH ROAD	WINDMILL DRIVE	NORTHWOOD DRIVE	Hot Mix Asphalt	Major Collector	None	County	3,140	87
NORTH ROAD	NORTHWOOD DRIVE	CONCORD TOWN LINE	Hot Mix Asphalt	Major Collector	None	County	2,313	88
NOYES LANE	HORSE POND ROAD	MARKED TREE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	636	78
OAK HILL ROAD	PRATTS MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,024	99
OAKRIDGE ROAD	LINCOLN ROAD	SAWMILL LANE	Hot Mix Asphalt	Local- Thru Street	None	Public	757	57
OAKWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,881	89
OLD COACH ROAD	LEDGE ROAD	FARM LANE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,058	89
OLD COUNTY ROAD	BOSTON POST ROAD	500' E OF BOSTON POST ROAD	Hot Mix Asphalt	Minor Collector	Sloped Granite	Public	500	88
OLD COUNTY ROAD	500' E OF BOSTON POST ROAD	1300' E OF BOSTON POST ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	800	93
OLD COUNTY ROAD	1300' E OF BOSTON POST ROAD	WAYLAND TOWN LINE	Hot Mix Asphalt	Minor Collector	HMA	Public	1,265	69
OLD FORGE LANE	COUNTRY VILLAGE LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	893	90
OLD FRAMINGHAM ROAD	NOBSCOT ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Local- Thru Street	None	Public	2,325	76
OLD GARRISON ROAD	FRENCH ROAD	DUTTON ROAD	Hot Mix Asphalt	Minor Collector	None	Public	2,966	65
OLD GARRISON ROAD (YOUTH)	OLD GARRISON ROAD	PEAKHAM ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	116	73
OLD LANCASTER ROAD	HUDSON ROAD	PEAKHAM ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	2,977	98

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
OLD LANCASTER ROAD	PEAKHAM ROAD	CONCORD ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	3,205	88
OLD LANCASTER ROAD	CONCORD ROAD	GREEN HILL ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	4,101	88
OLD MEADOW ROAD	DEAD END	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,137	88
OLD ORCHARD ROAD	BROOKDALE ROAD	STOCK FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,062	99
OLD SUDBURY ROAD	CONCORD ROAD	900' E OF CONCORD ROAD	Hot Mix Asphalt	Local- Thru Street	Sloped Granite	County	900	100
OLD SUDBURY ROAD	900' E OF CONCORD ROAD	RICE ROAD €	Hot Mix Asphalt	Minor Arterial	None	Public	3,283	56
OLD SUDBURY ROAD	RICE ROAD €	WAYLAND TOWN LINE	Hot Mix Asphalt	Minor Arterial	None	County	3,393	85
OLD SUDBURY ROAD (LOOP)	OLD SUDBURY ROAD	OLD SUDBURY ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	493	88
PADDOCK WAY	DAKIN ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	1,001	75
PANTRY ROAD	NORTH ROAD	CONCORD ROAD	Hot Mix Asphalt	Minor Collector	None	Public	4,166	80
PARMENTER ROAD	FAIRBANK ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	504	56
PARTRIDGE LANE	SURREY LANE	PONDVIEW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	770	74
PATRICIA ROAD	LANDHAM ROAD	DAWSON DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,609	95
PEAKHAM CIRCLE	PEAKHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	489	69
PEAKHAM ROAD	BOSTON POST ROAD	FRENCH ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	2,366	78
PEAKHAM ROAD	FRENCH ROAD	ROBERT BEST ROAD (N)	Hot Mix Asphalt	Minor Collector	None	Public	2,927	63
PEAKHAM ROAD	ROBERT BEST ROAD (N)	MARKED TREE ROAD	Hot Mix Asphalt	Minor Collector	None	Public	3,061	73
PEAKHAM ROAD	MARKED TREE ROAD	HORSE POND ROAD	Hot Mix Asphalt	Minor Collector	None	Public	1,986	77
PEAKHAM ROAD	HORSE POND ROAD	OLD LANCASTER ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	2,201	83
PEAKHAM ROAD	OLD LANCASTER ROAD	HUDSON ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	3,181	83
PELHAM ISLAND ROAD	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	3,390	89
PENDLETON ROAD	MARLBORO ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	777	88
PENNY MEADOW ROAD	CONCORD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,589	68
PERRY CIRCLE	ATKINSON LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	453	75
PETERSEN CIRCLE	ATKINSON LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	306	72
PHEASANT AVENUE	UNION AVENUE	MEADOW DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	650	98
PHILEMON WHALE LANE	DAKIN ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	982	89
PHILLIPS ROAD	FAIRBANK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,067	90
PILGRIMS PATH	PURITAN LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	916	82
PINE RIDGE ROAD	GERRY DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	579	98
PINE STREET	BARTON DRIVE	DEAD END	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,003	100
PINEWOOD AVENUE	CRYSTAL LAKE DRIVE	WILLIS LAKE DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	2,031	89
PIPSISSEWA CIRCLE	WHITE OAK LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	472	82
PLANTATION CIRCLE	OLD LANCASTER ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	781	100
PLYMPTON ROAD	CONCORD ROAD	CLARK ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,795	80
PLYMPTON ROAD	CLARK ROAD	WATER ROW	Hot Mix Asphalt	Local- Thru Street	None	Public	3,488	80

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
PLYMPTON ROAD (Y INT)	PLYMPTON ROAD	CONCORD ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	226	98
POKONOKET AVENUE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,753	87
PONDVIEW ROAD	SURREY LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,282	80
POPLAR STREET	HEMLOCK ROAD	PRIVATE PORTION	Hot Mix Asphalt	Local- Thru Street	None	Public	878	81
POSSUM LANE	NORTH ROAD	MOSSMAN ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,336	89
POWDER MILL ROAD	NORTH ROAD	TAVERN CIRCLE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	3,633	63
POWDER MILL ROAD	TAVERN CIRCLE	MAYNARD TOWN LINE	Hot Mix Asphalt	Local- Thru Street	None	Public	853	87
POWERS ROAD	NORTH ROAD	CEDAR CREEK ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,987	78
POWERS ROAD	CEDAR CREEK ROAD	CONCORD TOWN LINE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,737	78
PRATTS MILL ROAD	PEAKHAM ROAD	WILLOW ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,750	85
PRATTS MILL ROAD	WILLOW ROAD	DUTTON ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,760	60
PRIDES CROSSING ROAD	WAYSIDE INN ROAD	JACK LINE DR	Hot Mix Asphalt	Local- Thru Street	None	Public	3,396	88
PRIDES CROSSING ROAD	JACK PINE DRIVE	MARLBORO TOWN LINE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,418	88
PUFFER LANE	NORTH ROAD	HAYNES ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,685	66
PURITAN LANE	CUL DE SAC	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	1,402	79
RAMBLING ROAD	WOODSIDE ROAD	CIDER MILL ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	285	94
RAMBLING ROAD	CIDER MILL ROAD	WARREN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,584	54
RAYMOND ROAD	BOSTON POST ROAD	WARREN ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	3,997	84
RAYMOND ROAD	WARREN ROAD	FRAMINGHAM TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	2,368	72
RAYNOR ROAD	MOORE ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,207	88
READ ROAD	CUTLER FARM ROAD	CUTLER FARM ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	761	90
RED OAK DRIVE	CUL DE SAC	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,104	88
REEVES STREET	ALAN AVENUE	RICHARD AVENUE	Hot Mix Asphalt	Local- Thru Street	None	Public	477	58
REVERE STREET	LAFAYETTE DRIVE	WASHINGTON DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	519	85
REVOLUTIONARY ROAD	WASHINGTON DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	693	88
RICE ROAD	OLD SUDBURY ROAD	OLD SUDBURY ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,348	58
RICHARD AVENUE	HUDSON ROAD	REEVES STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	757	58
RICHARD AVENUE	REEVES STREET	SEXTON STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	327	48
RIDGE HILL ROAD	MORSE ROAD	HUNT ROAD	Hot Mix Asphalt	Local- Dead End	None	Public	1,684	82
ROBBINS ROAD	BENT ROAD	HOWELL ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,676	67
ROBERT BEST ROAD	PEAKHAM ROAD	PEAKHAM ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,499	73
ROBERT FROST ROAD	CIDER MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	944	52
ROLLING LANE	JARMAN ROAD	JARMAN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,456	81
RONALD ROAD	HUDSON ROAD	FIRECUT LANE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,002	100
RUDDOCK ROAD	THUNDER ROAD	WILLIS ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,096	55
RUN BROOK CIRCLE	FAIRBANK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	646	95

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
RUSSET LANE	LANDHAM ROAD	EDDY STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	782	95
SADDLE RIDGE ROAD	FOX RUN	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,083	81
SAUNDERS ROAD	BOWKER DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	499	89
SAWMILL LANE	LINCOLN ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,012	66
SAXONY DRIVE	HUDSON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	2,229	95
SCOTTS WOOD DRIVE	PRATTS MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	490	80
SEXTON STREET	RICHARD AVENUE	HOWE STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	335	55
SHADOW OAK DRIVE	STOCK FARM ROAD	STOCK FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,637	86
SILVER HILL ROAD	MOSSMAN ROAD	THUNDER ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	2,407	98
SINGING HILL CIRCLE	POWDER MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	221	100
SINGLETARY LANE	KING PHILIP ROAD	GREEN HILL ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,412	89
SKYVIEW LANE	DAKIN ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,246	88
SOUTH MEADOW DRIVE	NOBSCOT ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	631	100
SOUTHWEST CIRCLE	PEAKHAM ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	424	99
SPILLER CIRCLE	MCLEAN DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	240	85
SPRING STREET	DUTTON ROAD	WINTER STREET (SOUTHERLY)	Hot Mix Asphalt	Local- Thru Street	None	Public	766	100
SPRUCE LANE	HUDSON ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	569	88
STAGECOACH DRIVE	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Sloped Granite	Public	283	65
STARVIEW DRIVE	MORSE ROAD	MUSKET LANE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	775	95
STATION ROAD	BOSTON POST ROAD	UNION AVENUE	Hot Mix Asphalt	Local- Thru Street	None	County	1,241	65
STEARNS LANE	MOORE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,562	87
STOCK FARM ROAD	VICTORIA ROAD	LANDHAM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,595	62
STONE ROAD	BOSTON POST ROAD	DOUGLAS DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,092	87
STONE ROAD	DOUGLAS DRIVE	HICKORY ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,081	94
STONE ROAD	HICKORY ROAD	BENT ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	473	94
STONE ROOT LANE	MOSSMAN ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	935	90
STONEBROOK ROAD	HORSE POND ROAD	EVERGREEN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,076	99
STUBTOE LANE	COUNTRY VILLAGE LANE	TEAKETTLE LANE	Hot Mix Asphalt	Local- Thru Street	None	Public	1,306	95
SUFFOLK ROAD	BELCHER DRIVE	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	740	99
SUMMER STREET	DUTTON ROAD	PINE STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	1,159	100
SUNSET PATH	THUNDER ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,140	95
SURREY LANE	MOORE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	2,078	83
SYCAMORE ROAD	POPLAR STREET	MAGNOLIA ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	885	82
SYLVAN WAY	OLD COACH ROAD	LEDGE ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	628	99
SYLVAN WAY	LEDGE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	165	60
TAINTOR DRIVE	WITHERELL DRIVE	400' S OF WITHERELL DRIVE	Hot Mix Asphalt	Local- Dead End	None	Public	407	95

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
TAINTOR DRIVE	400' S OF WITHERELL DRIVE	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	763	78
TALL PINE DRIVE	HORSE POND ROAD	BRIDLE PATH	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,023	100
TANBARK ROAD	DUTTON ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,933	62
TANTAMOUSE TRAIL	HAWES ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,038	82
TAVERN CIRCLE	POWDER MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,100	82
TAYLOR ROAD	UNION AVENUE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	605	100
TEAKETTLE LANE	HUDSON ROAD	STUBTOE LANE	Hot Mix Asphalt	Local- Thru Street	None	Public	465	100
THOMPSON DRIVE	CONCORD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,663	88
THOREAU WAY	BLACKSMITH DRIVE	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	666	53
THORNBERRY LANE	BUTLER ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	None	Public	1,377	95
THUNDER ROAD	BALCOM ROAD	SILVER HILL ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,023	89
TIPPLING ROCK ROAD	DUDLEY ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	585	69
TRAILSIDE CIRCLE	BRIDLE PATH	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	465	81
TREVOR WAY	HORSE POND ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	584	88
TUDOR ROAD	NORMANDY DRIVE	CHURCHILL STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	593	99
TWILLINGATE LANE	HUDSON ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	588	74
TWIN MEADOW LANE	FAIRBANK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	367	100
TWIN POND LANE	CONCORD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	830	83
UNION AVENUE	BOSTON POST ROAD	STATION ROAD	Hot Mix Asphalt	Major Collector	Granite	Public	986	87
UNION AVENUE	STATION ROAD	PHEASANT AVENUE	Hot Mix Asphalt	Major Collector	None	County	3,777	88
UNION AVENUE	PHEASANT AVENUE	CONCORD ROAD	Hot Mix Asphalt	Major Collector	Sloped Granite	County	375	65
UPLOOK DRIVE	BOSTON POST ROAD	EASY STREET	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	347	89
VICTORIA ROAD	STOCK FARM ROAD	STOCK FARM ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,202	89
VILLAGE ROAD	PUFFER LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,091	89
VIRGINIA RIDGE ROAD	POWDER MILL ROAD	CUL DE SAC LOOP	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,744	71
WADSWORTH ROAD	HAYNES ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	851	66
WAGONWHEEL ROAD	LANDHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,772	88
WAKE ROBIN ROAD	MORSE ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	HMA	Public	2,009	88
WALKER FARM ROAD	GOODMANS HILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	962	80
WALKUP ROAD	CONCORD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	418	80
WARD ROAD	MAYNARD ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	845	88
WARREN ROAD	RAYMOND ROAD	WOODSIDE ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	3,039	86
WASH BROOK ROAD	CUL DE SAC	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	698	98
WASHINGTON DRIVE	CUL DE SAC	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	2,037	83
WATER ROW	OLD SUDBURY ROAD	PLYMPTON ROAD	Hot Mix Asphalt	Local- Thru Street	None	County	2,979	79
WATER ROW	PLYMPTON ROAD	3050' S OF LINCOLN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	4,032	79

Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
WATER ROW	3050' S OF LINCOLN ROAD	LINCOLN ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	3,051	54
WAYSIDE INN ROAD	MARLBOROUGH TOWN LINE	350' E OF DUTTON ROAD	Hot Mix Asphalt	Minor Collector	None	Public	3,500	78
WAYSIDE INN ROAD	350' E OF DUTTON ROAD	1500' W OF MARLBOROUGH TOWN LINE	Hot Mix Asphalt	Minor Collector	HMA	Public	759	77
WAYSIDE INN ROAD	1500' W OF MARLBOROUGH TOWN LINE	MARLBORO TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	1,502	86
WEBSTER CIRCLE	PHILLIPS ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	996	88
WEIR HILL ROAD	LINCOLN ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	2,128	57
WEST STREET	PRATTS MILL ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	505	70
WHISPERING PINE ROAD	PEAKHAM ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,124	88
WHITE OAK LANE	MOORE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,163	80
WHITETAIL LANE	SAWMILL LANE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	499	86
WIDOW RITES LANE	WYMAN DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	1,798	84
WILDWOOD LANE	OLD LANCASTER ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	596	80
WILLARD GRANT ROAD	NORTH ROAD	BLACKSMITH DRIVE	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,396	61
WILLARD GRANT ROAD	BLACKSMITH DRIVE	PLYMPTON ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,814	56
WILLIS LAKE DRIVE	HUDSON ROAD	ARBORWOOD ROAD	Hot Mix Asphalt	Local- Thru Street	None	Public	1,886	89
WILLIS ROAD	MAYNARD ROAD	MARLBORO ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,525	84
WILLIS ROAD	MARLBORO ROAD	BRYANT ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,721	78
WILLIS ROAD	BRYANT ROAD	FORD ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	2,502	65
WILLIS ROAD	FORD ROAD	MOSSMAN ROAD	Hot Mix Asphalt	Local- Thru Street	HMA	Public	1,991	65
WILLOW ROAD	PRATTS MILL ROAD	HEMLOCK ROAD	Hot Mix Asphalt	Local- Dead End	None	Public	1,275	70
WILLOW ROAD	HEMLOCK ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	942	80
WILSHIRE STREET	DEAD END	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	HMA	Public	713	40
WILSON ROAD	PRATTS MILL ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	468	87
WINDMILL DRIVE	NORTH ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,308	88
WINSOR ROAD	SINGLETARY LANE	OLD LANCASTER ROAD	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	2,331	88
WINTER STREET	BARTON DRIVE	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	466	90
WINTER STREET (SOUTHERN)	SPRING STREET	SUMMER STREET	Hot Mix Asphalt	Local- Thru Street	None	Public	447	90
WITHERELL DRIVE	WILLIS ROAD	BELCHER DRIVE	Hot Mix Asphalt	Local- Thru Street	None	Public	2,449	87
WOLBACH ROAD	OLD SUDBURY ROAD	DEAD END	Hot Mix Asphalt	Local- Dead End	None	Public	1,121	54
WOODBERRY ROAD	FOREST STREET	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	357	83
WOODLAND ROAD	DUDLEY ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	776	95
WOODMERE DRIVE	MORSE ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	Cape Cod Berm	Public	2,488	95
WOODSIDE ROAD	HOPESTILL BROWN ROAD (S)	TOWN LINE	Hot Mix Asphalt	Minor Collector	None	Public	662	55
WOODSIDE ROAD	WARREN ROAD	HOPESTILL BROWN ROAD (N)	Hot Mix Asphalt	Minor Collector	None	Public	1,278	73
WOODSIDE ROAD	CUTLER FARM ROAD	WARREN ROAD	Hot Mix Asphalt	Minor Collector	HMA	Public	1,616	54
WOODSIDE ROAD	LANDHAM ROAD	CUTLER FARM ROAD	Hot Mix Asphalt	Minor Collector	Cape Cod Berm	Public	1,295	77

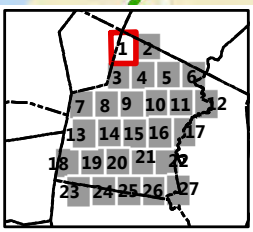
Pavement Inventory Report (Alphabetical)

Street Name	From Segment	To Segment	Pavement Type	Pavement Class	CurbMaterial	Owner	Length (ft)	PCI
WRIGHT ROAD	ALTA ROAD	CUL DE SAC	Hot Mix Asphalt	Local- Dead End	None	Public	1,005	80
WYMAN DRIVE	MAYNARD ROAD	CUDWORTH LANE	Hot Mix Asphalt	Local- Thru Street	Cape Cod Berm	Public	1,333	85

Appendix C: Pavement Inventory Maps

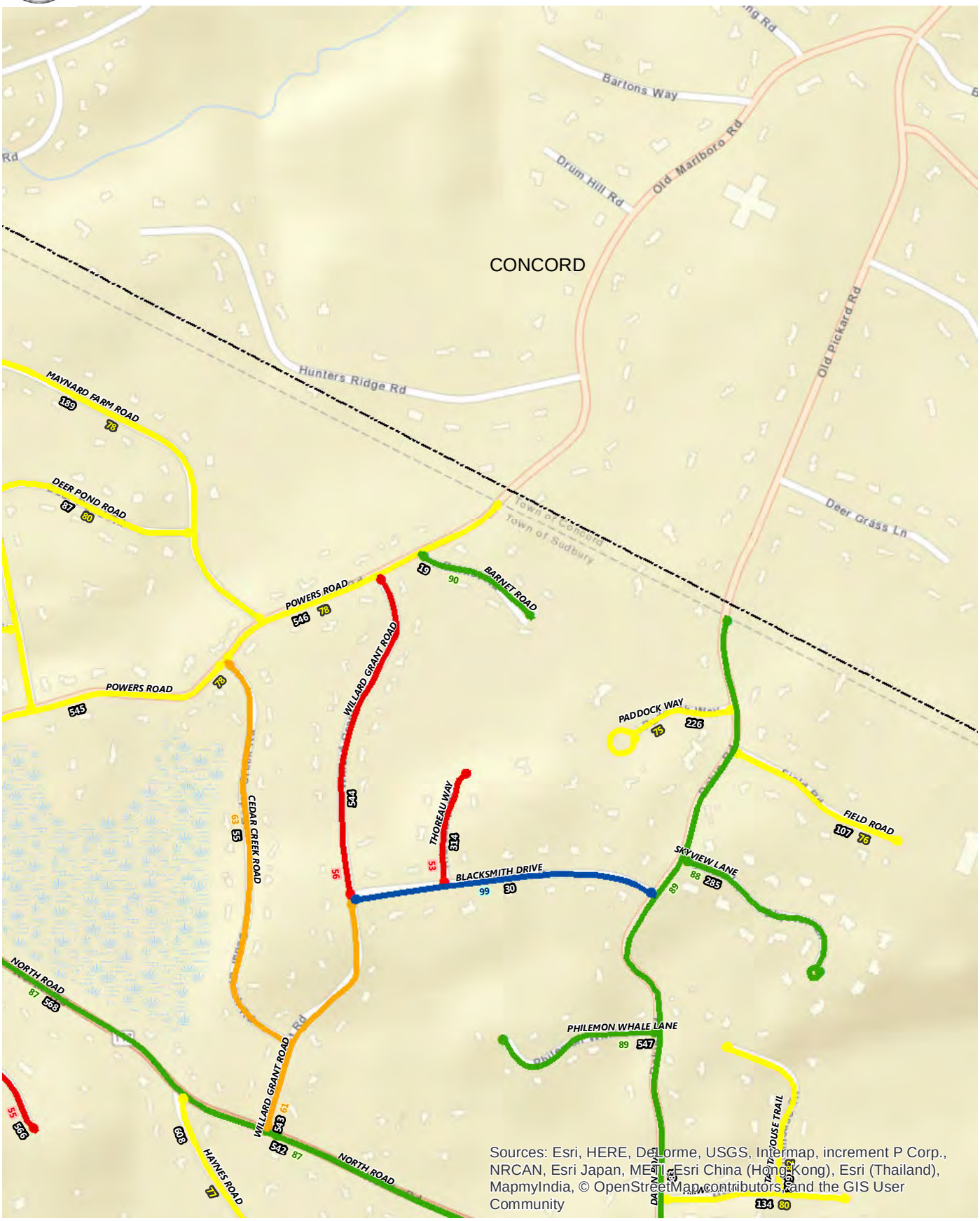
0 187.5 375 750 Feet

PCI
- 40 - 60
- 61 - 72
- 73 - 85
- 86 - 92
- 93 - 100
- Town Boundary



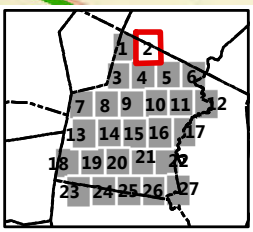
Sudbury, Massachusetts

Pavement Condition Index Map



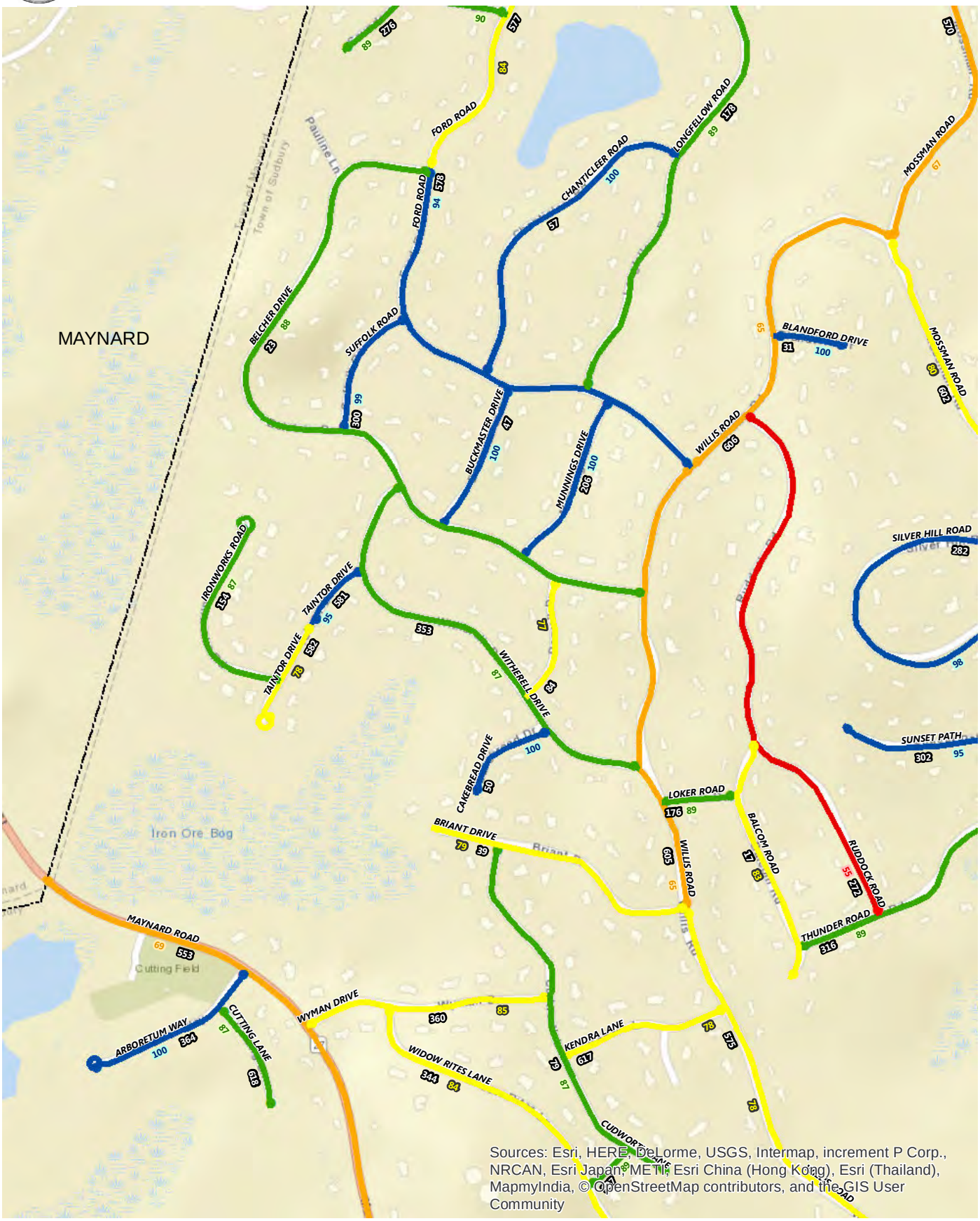
0 187.5 375 750 Feet

PCI
- 40 - 60
- 61 - 72
- 73 - 85
- 86 - 92
- 93 - 100
- Town Boundary



Sudbury, Massachusetts

Pavement Condition Index Map



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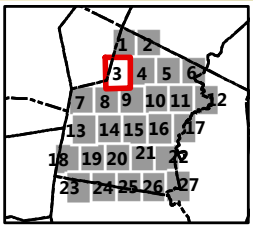

Sudbury, Massachusetts

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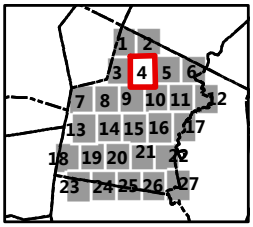
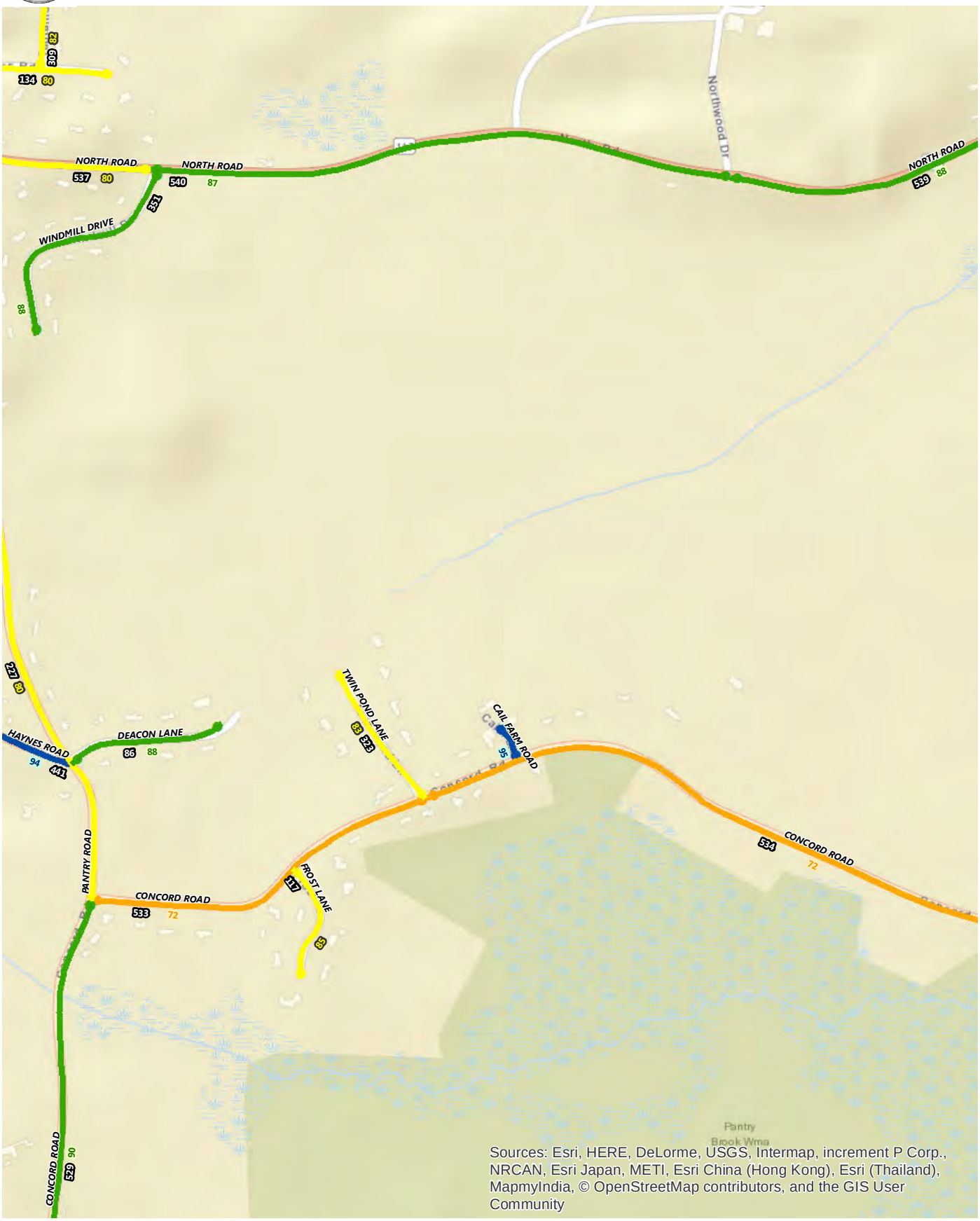


Sudbury, Massachusetts

Pavement Condition Index Map



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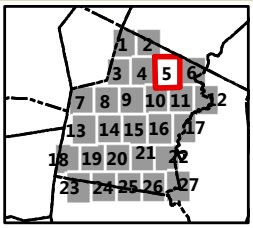

Sudbury, Massachusetts

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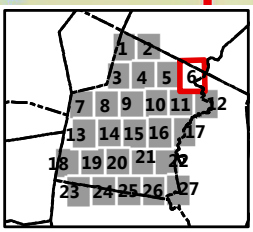

Sudbury, Massachusetts

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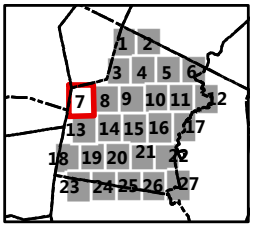
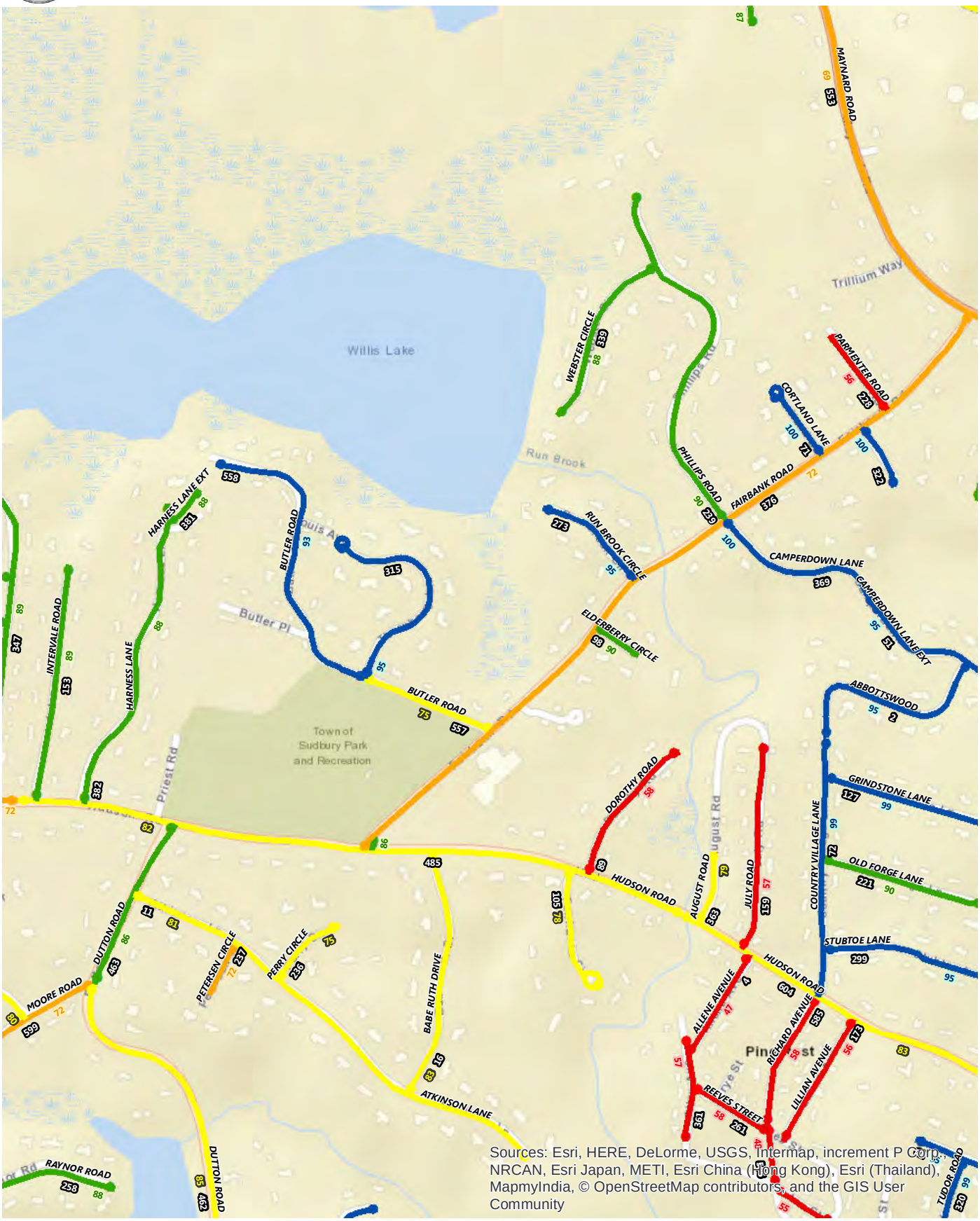


Sudbury, Massachusetts

Pavement Condition Index Map



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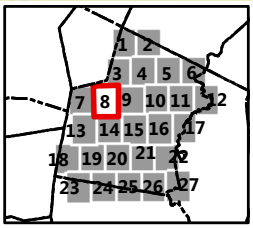

Sudbury, Massachusetts

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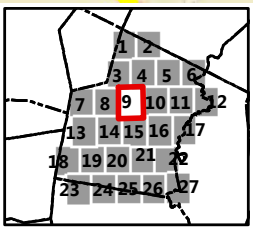

Sudbury, Massachusetts

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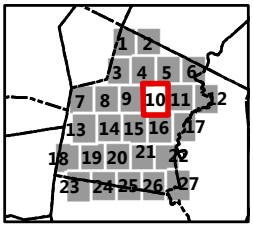
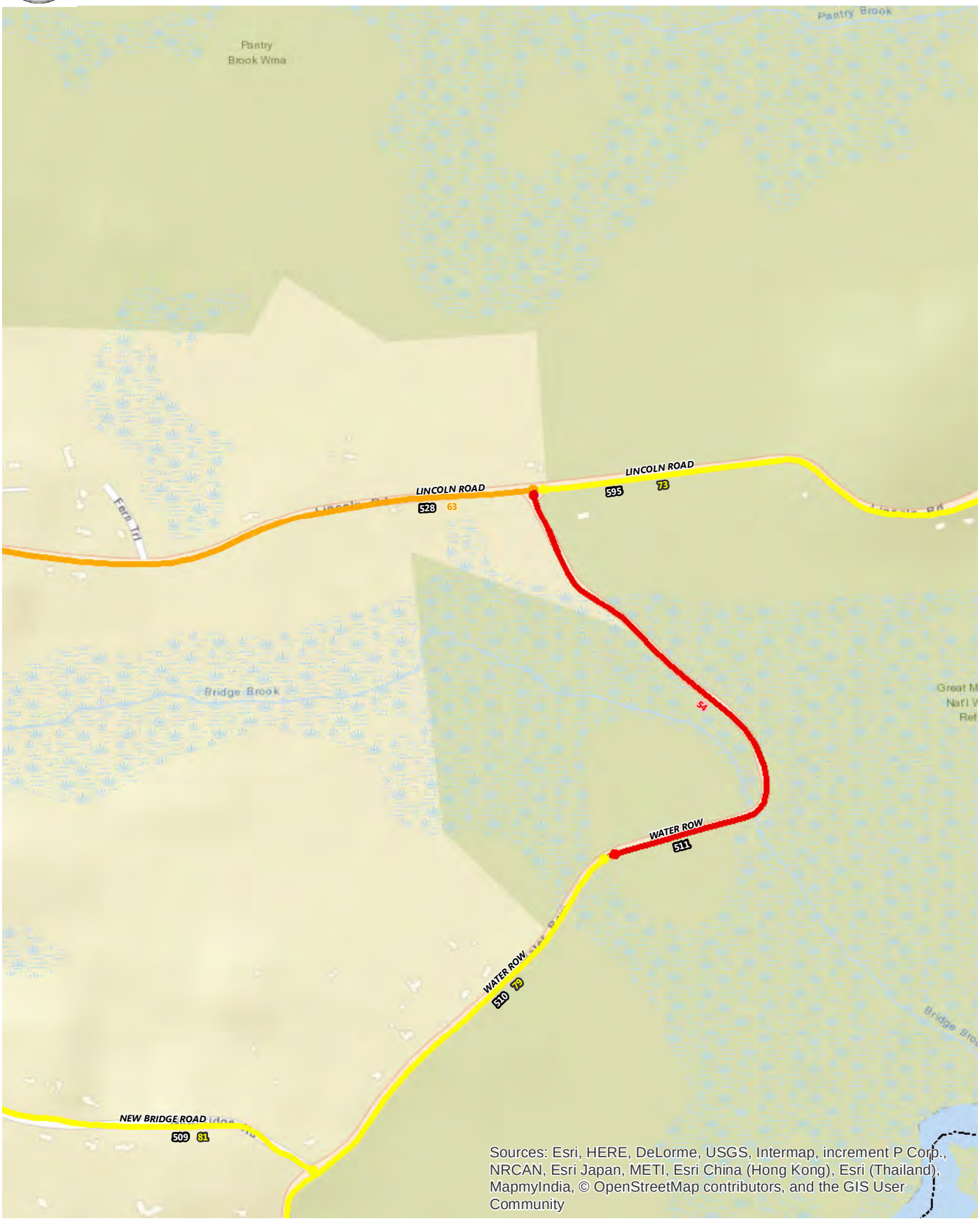


Sudbury, Massachusetts

Pavement Condition Index Map



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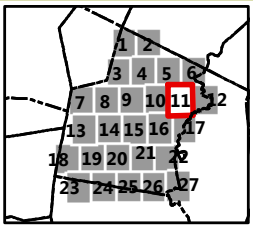
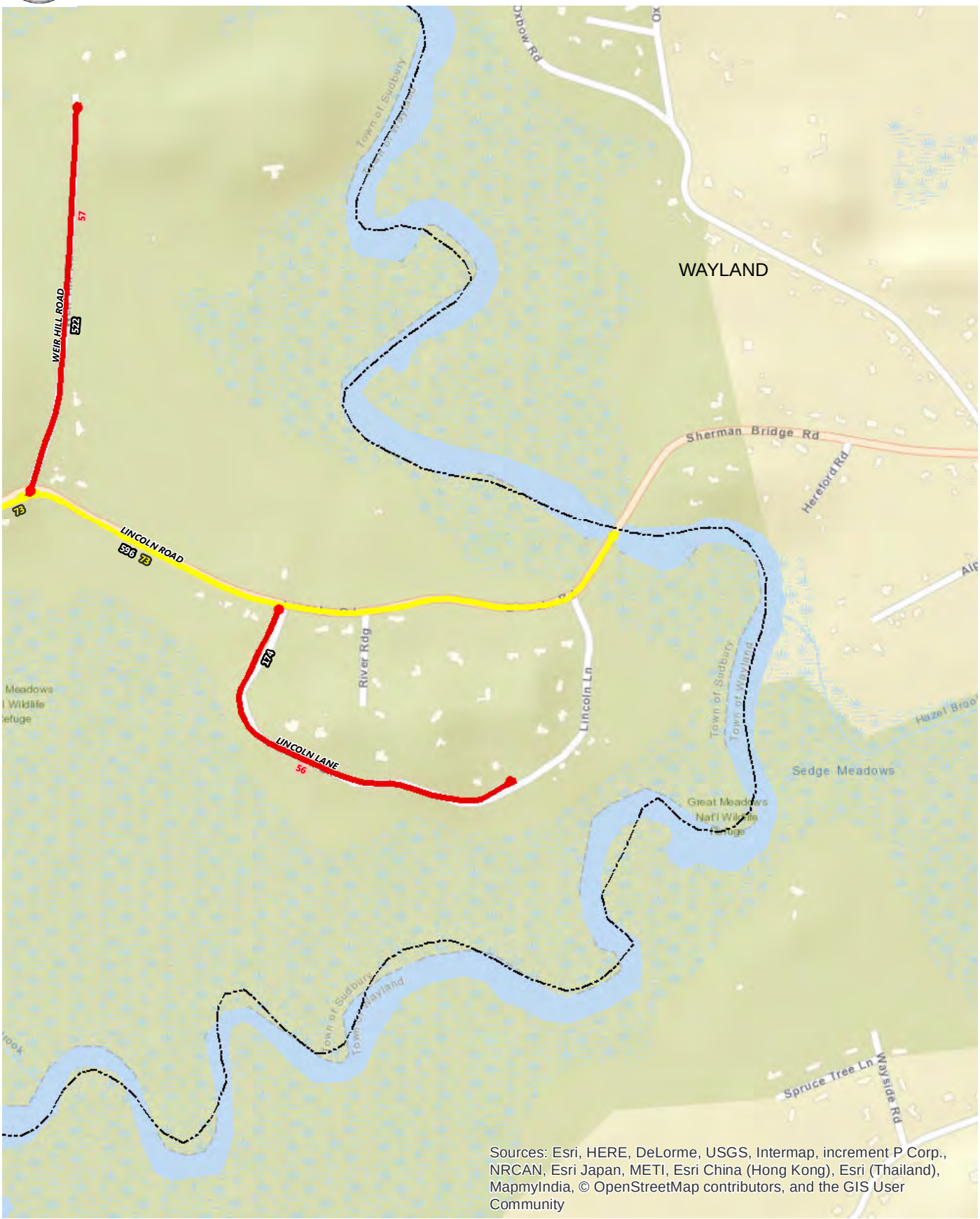


Sudbury, Massachusetts

Pavement Condition Index Map



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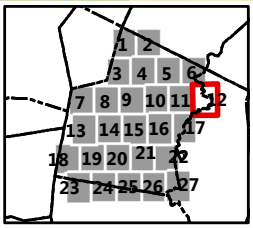
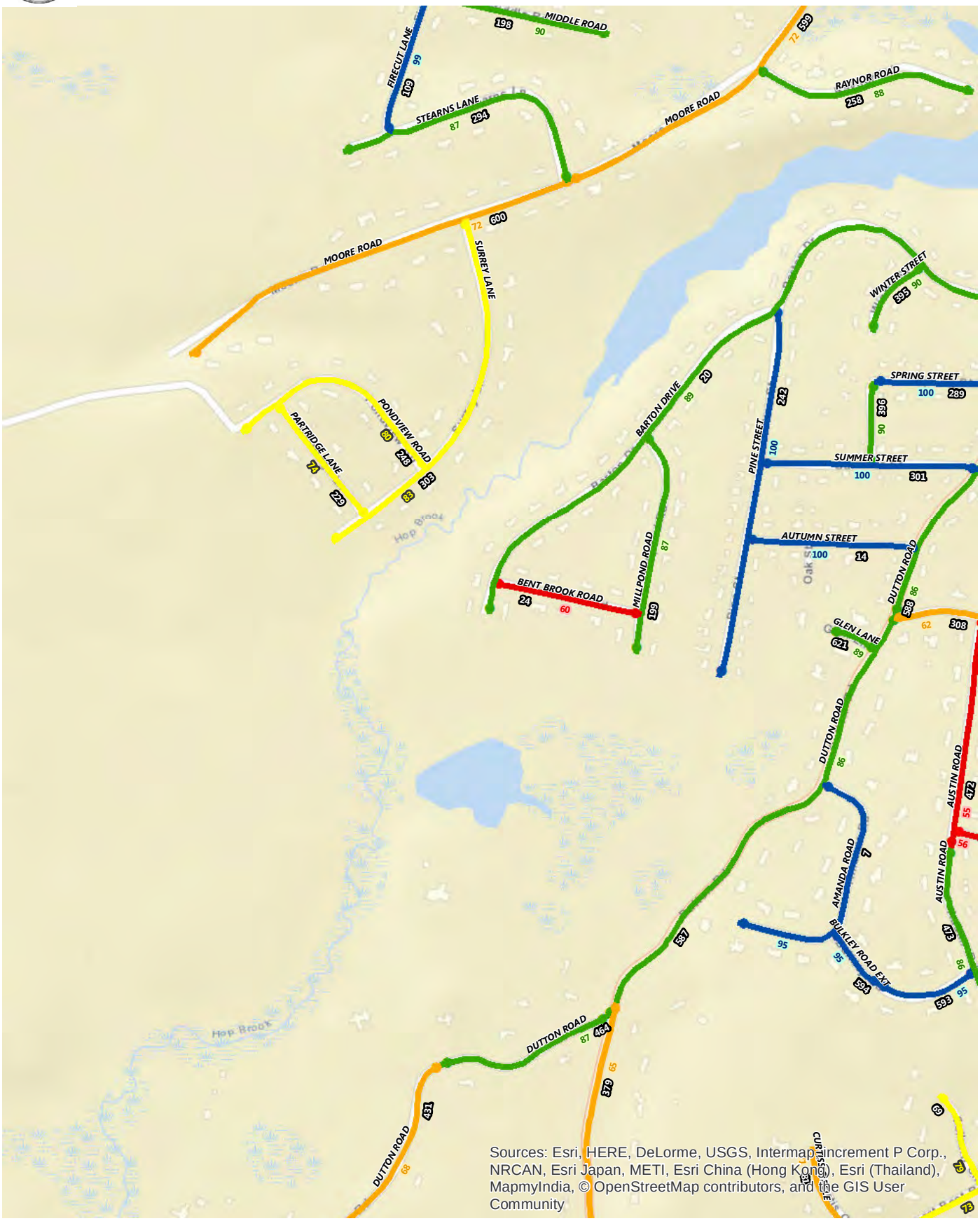


Sudbury, Massachusetts

Pavement Condition Index Map



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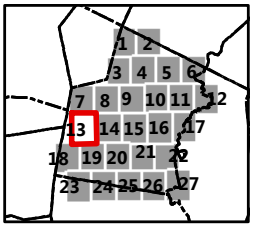
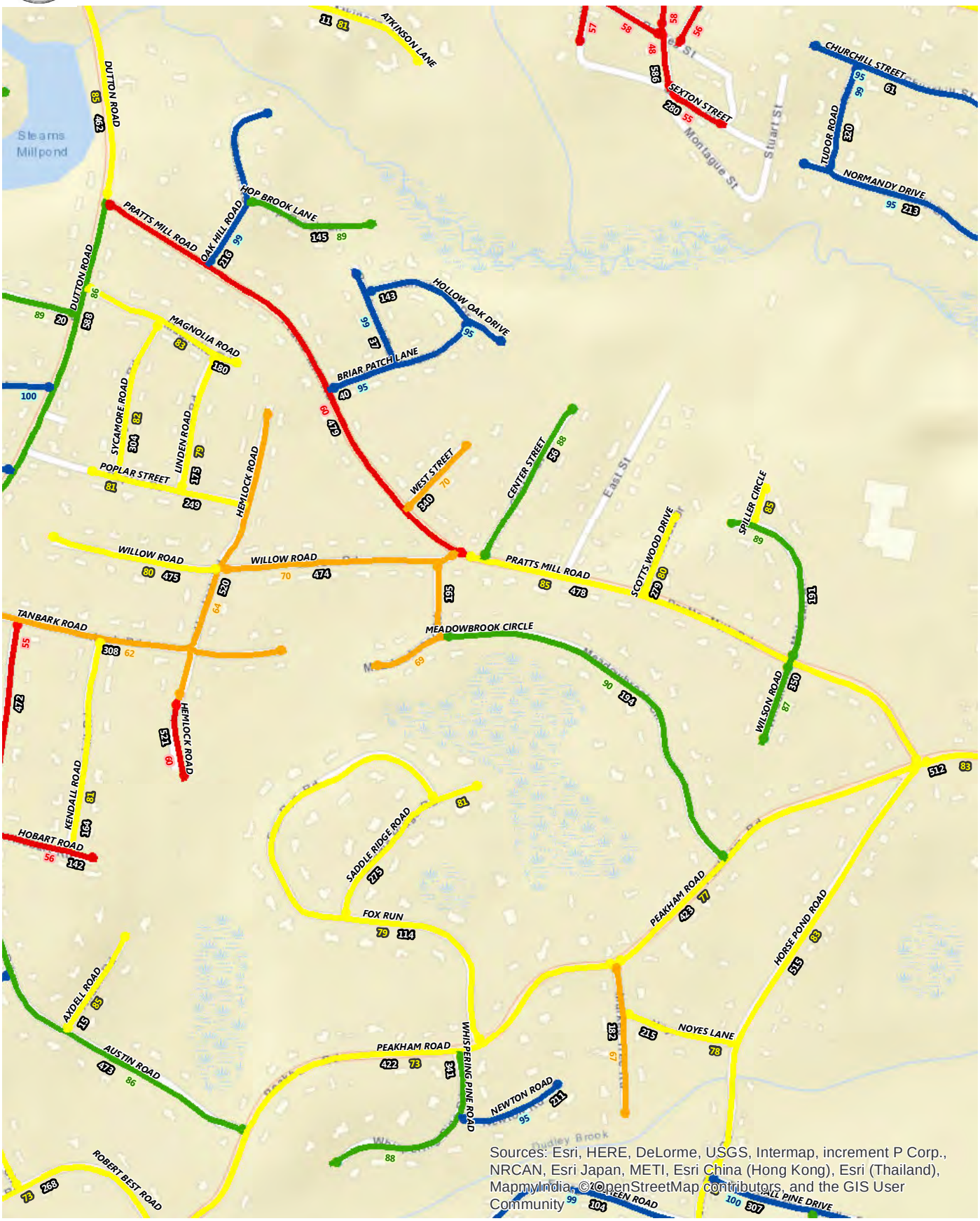


Sudbury, Massachusetts

Pavement Condition Index Map



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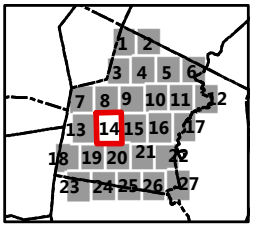

Sudbury, Massachusetts

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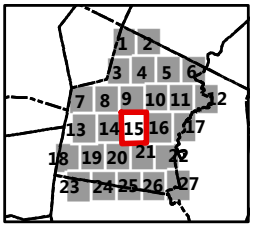

Sudbury, Massachusetts

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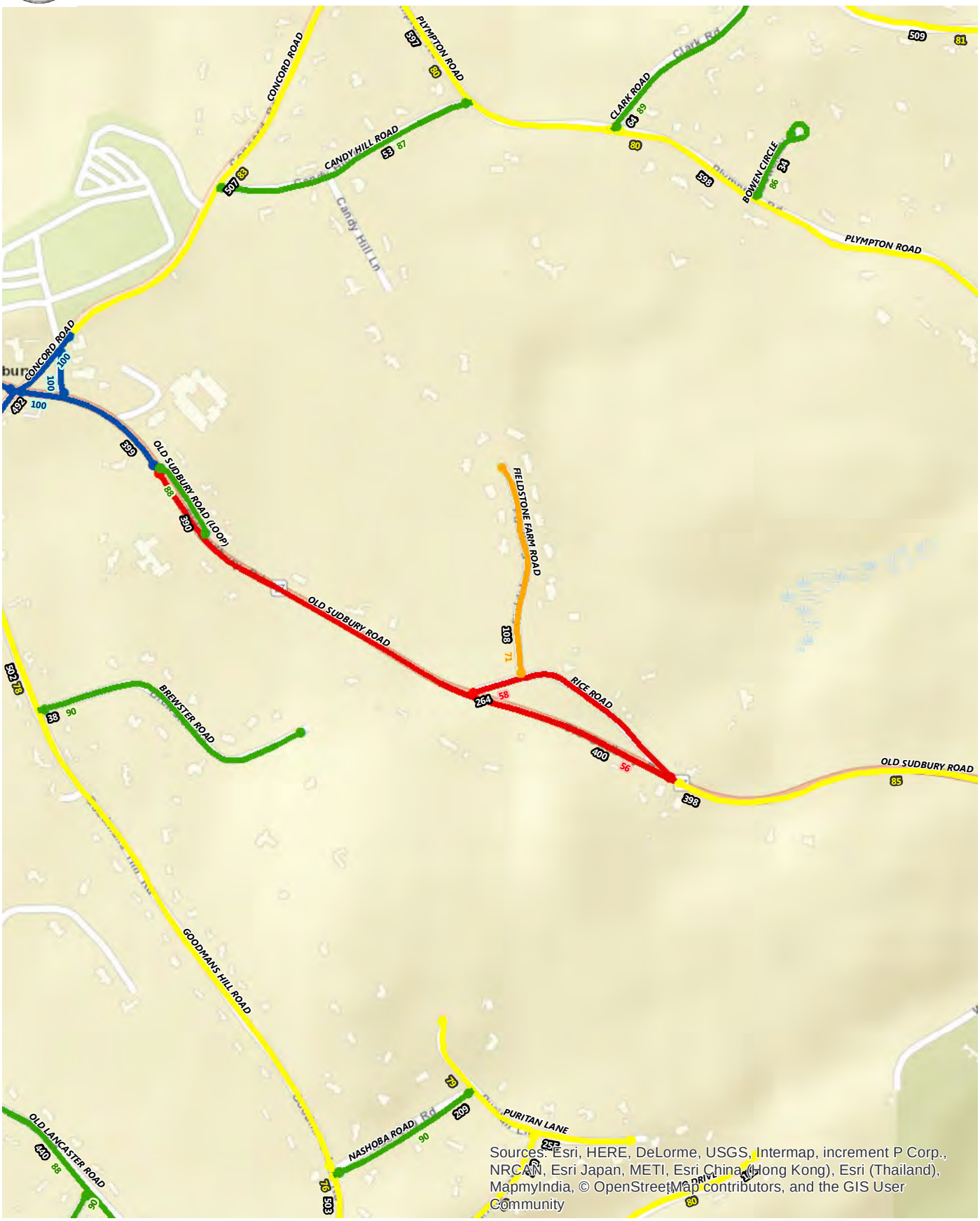

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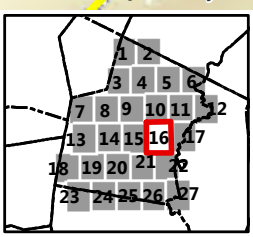

Sudbury, Massachusetts

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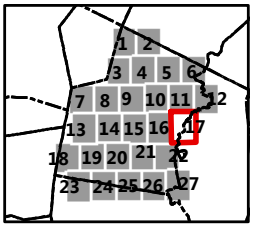

Sudbury, Massachusetts

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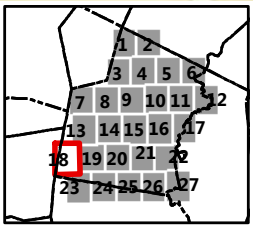

Sudbury, Massachusetts

Pavement Condition Index Map

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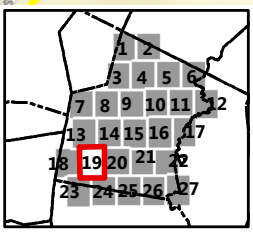
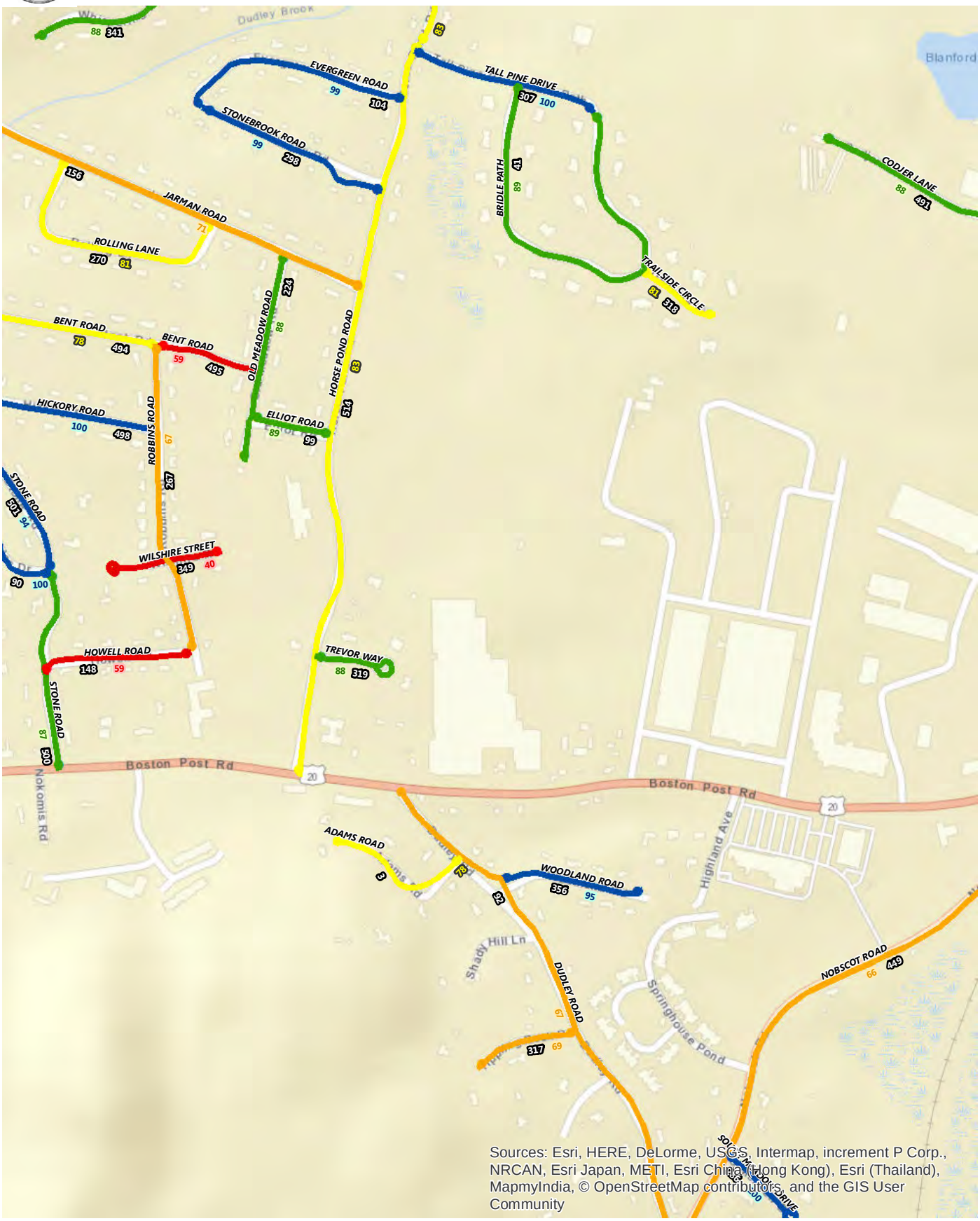


Sudbury, Massachusetts

Pavement Condition Index Map



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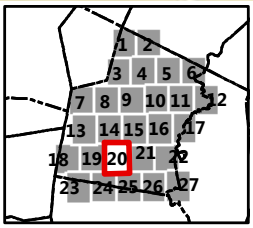

Sudbury, Massachusetts

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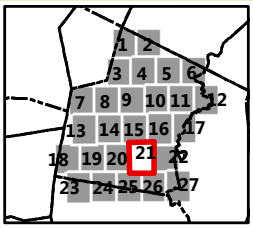

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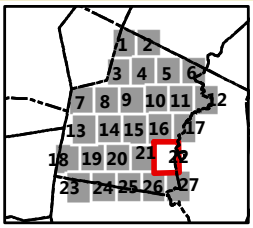

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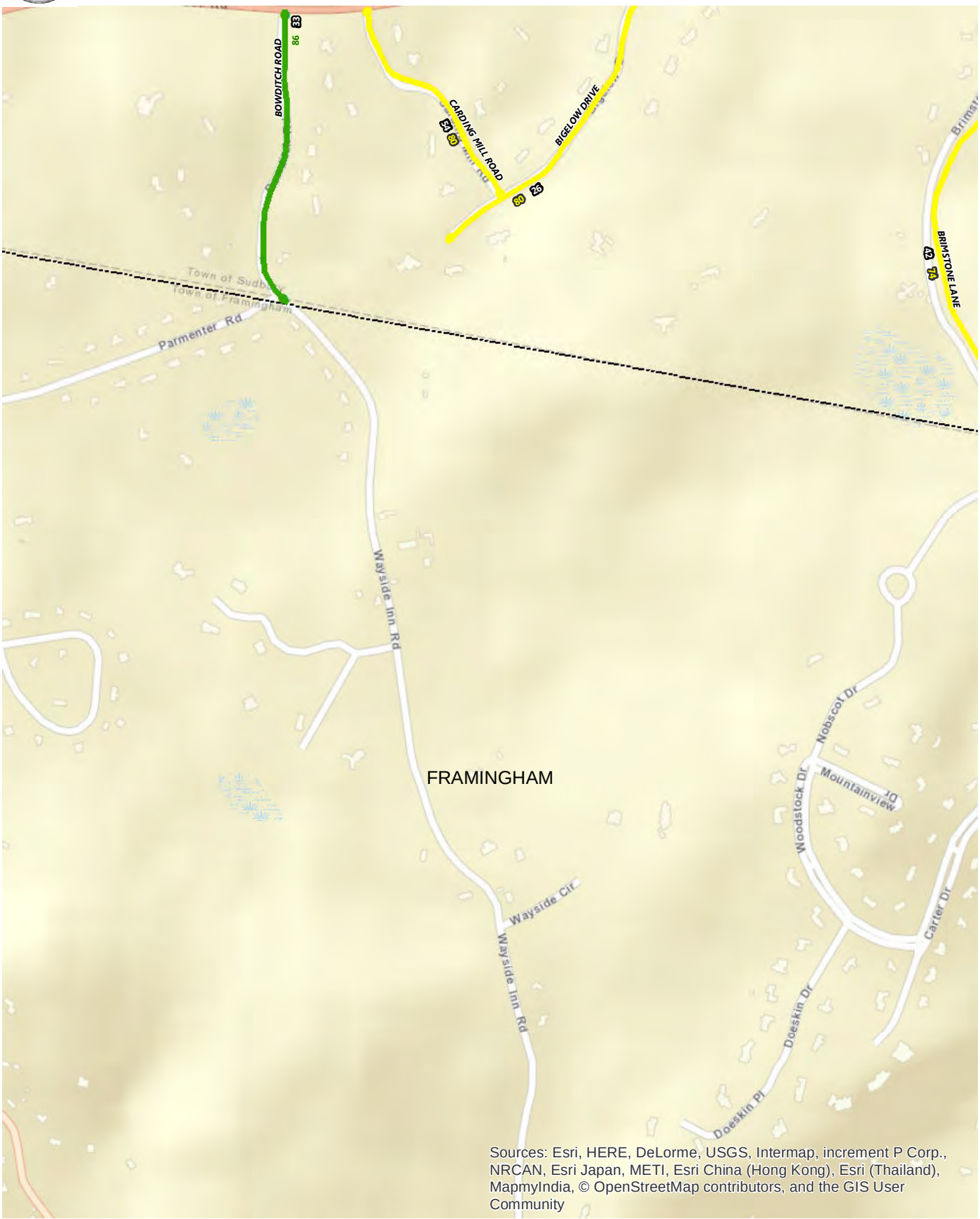

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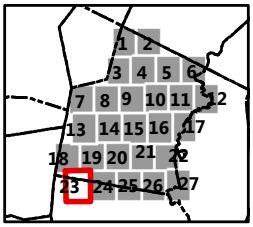
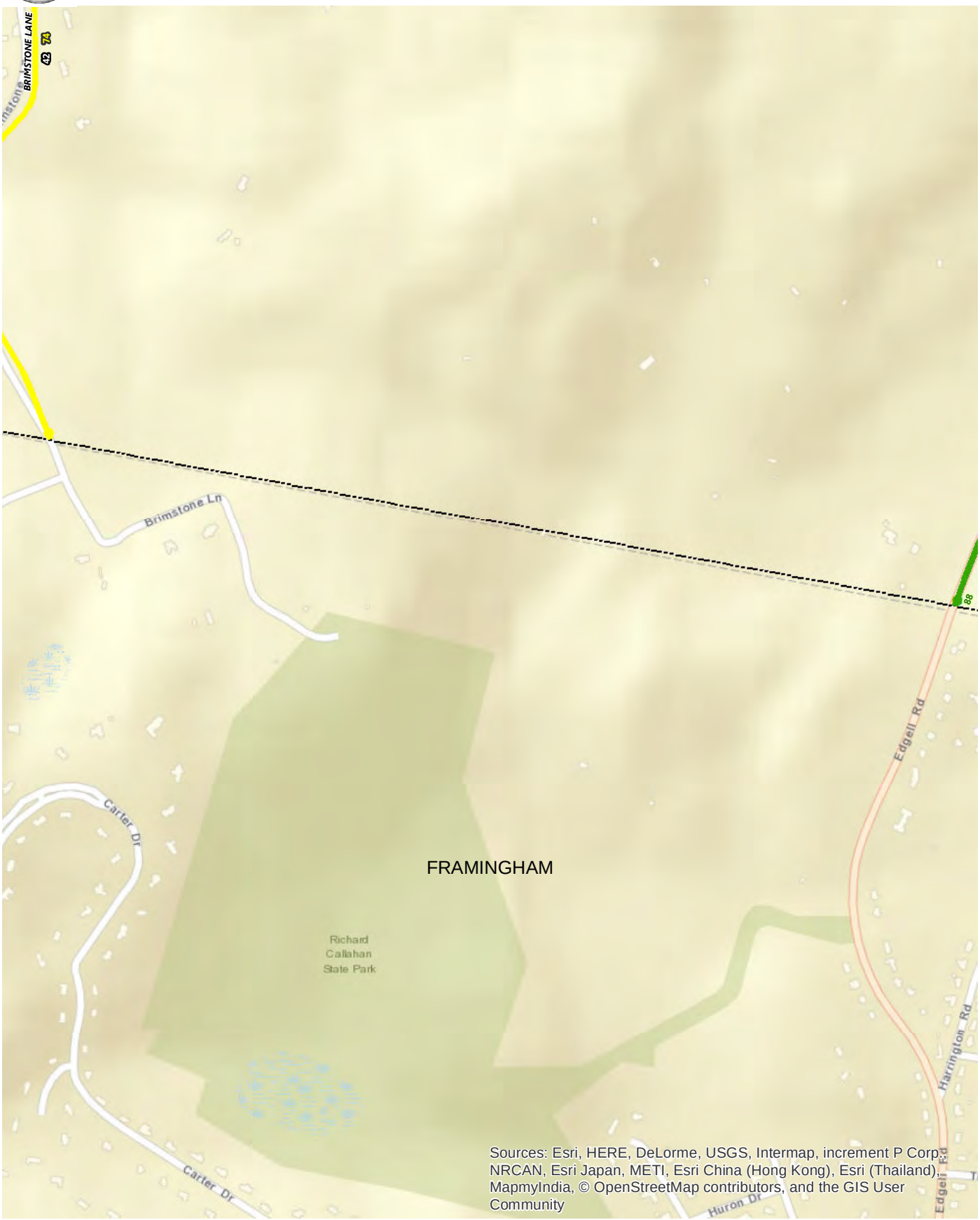


Sudbury, Massachusetts

Pavement Condition Index Map



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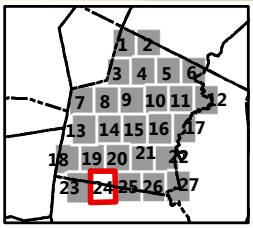

Sudbury, Massachusetts

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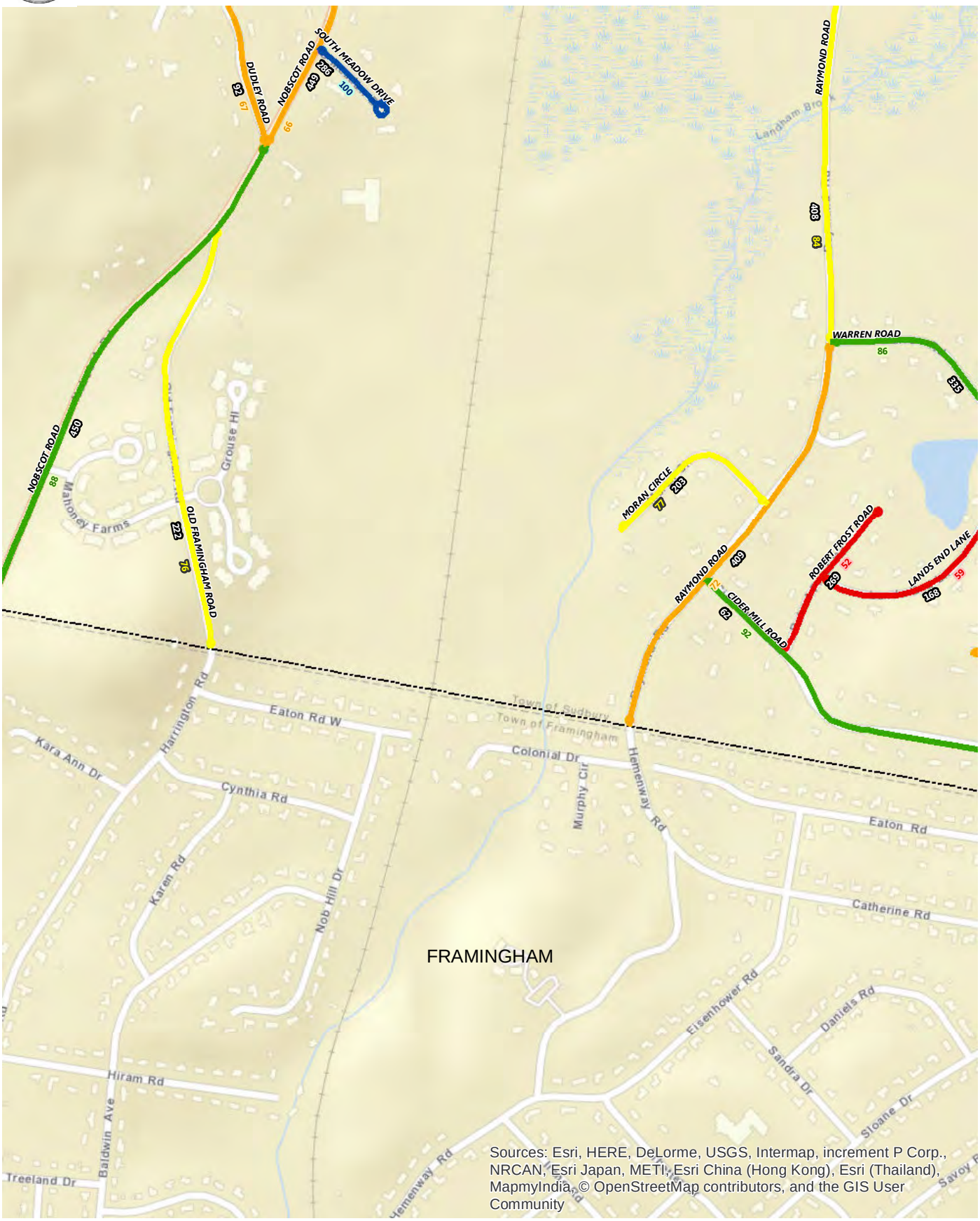

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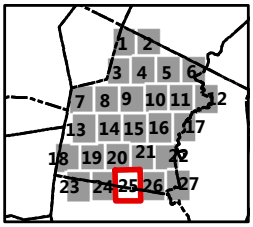

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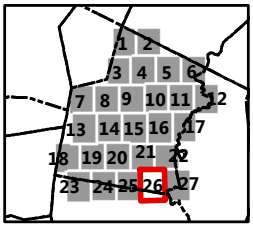

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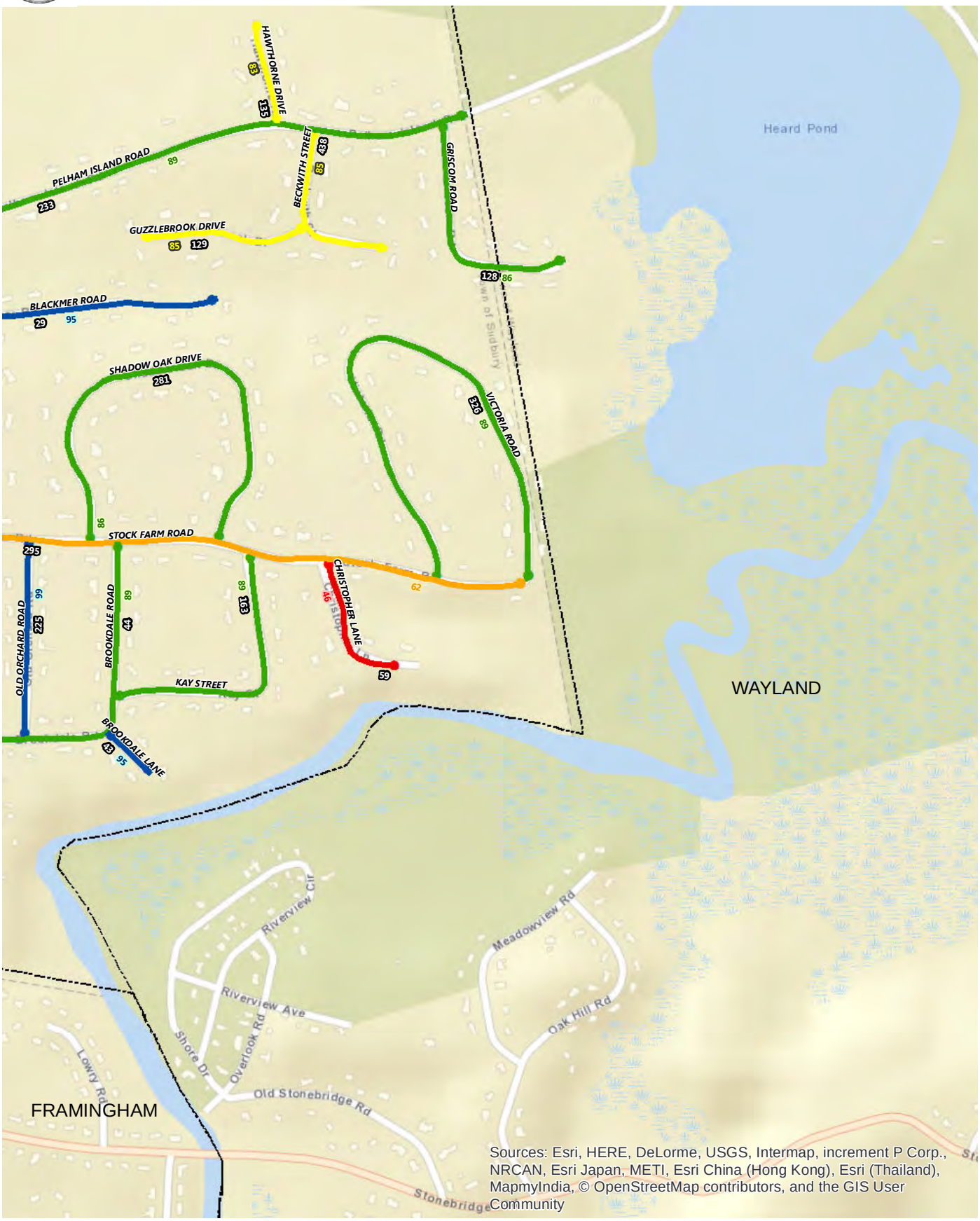

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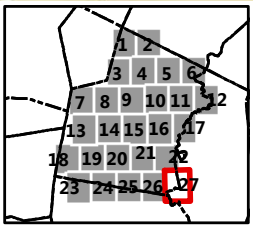


Sudbury, Massachusetts

Pavement Condition Index Map

Appendix D: Pavement Backlog Report (by Treatment Band)

Pavement Backlog Report - by Treatment Band, Benefit Value Descending

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Base Rehabilitation							
OLD SUDBURY ROAD	900' E OF CONCORD ROAD	RICE ROAD	Reclaim Arterial	3,283	55	$431,417	79.22
WOODSIDE ROAD	CUTLER FARM ROAD	WARREN ROAD	Reclaim Collector	1,616	54	$113,093	20.98
WOODSIDE ROAD	HOPESTILL BROWN ROAD (S)	TOWN LINE	Reclaim Collector	662	55	$46,290	20.60
RAMBLING ROAD	CIDER MILL ROAD	WARREN ROAD	Reclaim Local	1,584	54	$103,422	7.49
RUDDOCK ROAD	THUNDER ROAD	WILLIS ROAD	Reclaim Local	3,096	55	$202,092	7.36
SEXTON STREET	RICHARD AVENUE	HOWE STREET	Reclaim Local	335	55	$14,578	7.36
AUSTIN ROAD	TANBARK ROAD	100' S OF HOBART ROAD	Reclaim Local	1,234	55	$67,125	7.36
WILLARD GRANT ROAD	BLACKSMITH DRIVE	PLYMPTON ROAD	Reclaim Local	1,814	56	$118,409	7.22
OAKRIDGE ROAD	LINCOLN ROAD	SAWMILL LANE	Reclaim Local	757	57	$30,883	7.10
ALLAN AVENUE	ALLENE AVENUE	REEVES STREET	Reclaim Local	565	57	$24,587	7.10
LANDS END LANE	WARREN ROAD	ROBERT FROST ROAD	Reclaim Local	1,475	58	$88,258	6.98
RICE ROAD	OLD SUDBURY ROAD	OLD SUDBURY ROAD	Reclaim Local	1,348	58	$43,996	6.98
HOWELL ROAD	STONE ROAD	ROBBINS ROAD	Reclaim Local	823	59	$47,006	6.86
BENT ROAD	ROBBINS ROAD	OLD MEADOW ROAD	Reclaim Local	568	59	$35,540	6.86
BENT BROOK ROAD	BARTON DRIVE	MILLPOND ROAD	Reclaim Local	802	60	$43,626	6.74
ALLENE AVENUE	HUDSON ROAD	ALLAN AVENUE	Reclaim Local w/Drainage	647	47	$43,216	6.31
RICHARD AVENUE	REEVES STREET	SEXTON STREET	Reclaim Local w/Drainage	327	48	$18,210	6.18
WATER ROW	3050' S OF LINCOLN ROAD	LINCOLN ROAD	Reclaim Local w/Drainage	3,051	54	$181,165	5.49
LILLIAN AVENUE	HUDSON ROAD	REEVES STREET	Reclaim Local w/Drainage	755	56	$44,827	5.29
LINCOLN LANE	LINCOLN ROAD	LINCOLN LANE PRIVATE SECT	Reclaim Local w/Drainage	2,250	56	$133,590	5.29
REEVES STREET	ALAN AVENUE	RICHARD AVENUE	Reclaim Local w/Drainage	477	58	$24,781	5.11
RICHARD AVENUE	HUDSON ROAD	REEVES STREET	Reclaim Local w/Drainage	757	58	$42,159	5.11
PRATTS MILL ROAD	WILLOW ROAD	DUTTON ROAD	Reclaim Local w/Drainage	2,760	60	$204,852	4.94
CHRISTOPHER LANE	STOCK FARM ROAD	DEAD END	Reclaim Local	790	46	$34,378	3.52
ROBERT FROST ROAD	CIDER MILL ROAD	CUL DE SAC	Reclaim Local	944	52	$67,770	3.11
THOREAU WAY	BLACKSMITH DRIVE	DEAD END	Reclaim Local	666	53	$45,550	3.05
WILSHIRE STREET	DEAD END	CUL DE SAC	Reclaim Local w/Drainage	713	40	$63,500	2.96
MARTIN DRIVE	MAYNARD ROAD	CUL DE SAC	Reclaim Local	966	55	$79,753	2.94
GREYSTONE LANE	JULIANS WAY	DEAD END	Reclaim Local	3,277	55	$213,907	2.94
DEMARCO ROAD	OLD LANCASTER ROAD	DEAD END	Reclaim Local	839	56	$47,920	2.89
HOBART ROAD	AUSTIN ROAD	CUL DE SAC	Reclaim Local	592	56	$42,403	2.89

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Base Rehabilitation							
JULY ROAD	HUDSON ROAD	DEAD END	Reclaim Local	1,115	57	$66,717	2.84
COLONIAL ROAD	200' W OF HOMESTEAD AVENUE	CUL DE SAC	Reclaim Local	1,145	57	$64,166	2.84
WEIR HILL ROAD	LINCOLN ROAD	DEAD END	Reclaim Local	2,128	57	$92,604	2.84
DOROTHY ROAD	HUDSON ROAD	CUL DE SAC	Reclaim Local	843	58	$62,147	2.79
SYLVAN WAY	LEDGE ROAD	CUL DE SAC	Reclaim Local	165	60	$6,161	2.69
HAYDEN CIRCLE	BENT ROAD	CUL DE SAC	Reclaim Local w/Drainage	619	51	$66,656	2.32
WOLBACH ROAD	OLD SUDBURY ROAD	DEAD END	Reclaim Local w/Drainage	1,121	54	$58,238	2.19
AUSTIN DRIVE	HILLTOP ROAD	CUL DE SAC	Reclaim Local w/Drainage	463	55	$46,383	2.15
PARMENTER ROAD	FAIRBANK ROAD	DEAD END	Reclaim Local w/Drainage	504	56	$44,886	2.11
HAMMOND CIRCLE	BENT ROAD	CUL DE SAC	Reclaim Local w/Drainage	619	59	$74,876	2.01
HEMLOCK ROAD	300' S OF TANBARK ROAD	PARKING LOT	Reclaim Local w/Drainage	443	60	$78,136	1.97
Structural Improvement							
UNION AVENUE	PHEASANT AVENUE	CONCORD ROAD	Mill/Overlay A/C	375	64	$19,689	117.92
NOBSCOT ROAD	BOSTON POST ROAD	DUDLEY ROAD	Mill/Overlay A/C	3,333	65	$175,084	116.11
MAYNARD ROAD	WILLIS ROAD	MARTIN DRIVE	Mill/Overlay A/C	1,285	67	$71,999	112.64
MAYNARD ROAD	MARLBORO ROAD	MAYNARD TOWN LINE	Mill/Overlay A/C	4,279	69	$224,756	109.37
HUDSON ROAD	100' W OF INTERVALE ROAD	CRYSTAL LAKE DRIVE	Mill/Overlay A/C	2,261	71	$106,881	106.29
PEAKHAM ROAD	FRENCH ROAD	ROBERT BEST ROAD (N)	Mill/Overlay A/C	2,927	62	$112,757	24.34
DUTTON ROAD	HUDSON ROAD	FRENCH ROAD	Mill/Overlay A/C	3,802	62	$133,135	24.34
LINCOLN ROAD	CONCORD ROAD	SAWMILL LANE	Mill/Overlay A/C	681	63	$23,842	23.95
LINCOLN ROAD	SAWMILL LANE	WATER ROW	Mill/Overlay A/C	3,870	63	$135,505	23.95
OLD GARRISON ROAD	FRENCH ROAD	DUTTON ROAD	Mill/Overlay A/C	2,966	64	$98,665	23.58
DUDLEY ROAD	BOSTON POST ROAD	NOBSCOT ROAD	Mill/Overlay A/C	2,978	66	$93,850	22.87
DUTTON ROAD	FRENCH ROAD	963' S OF OLD GARRISON ROA	Mill/Overlay A/C	2,446	67	$85,646	22.52
OLD COUNTY ROAD	1300' E OF BOSTON POST ROAD	WAYLAND TOWN LINE	Mill/Overlay A/C	1,265	69	$39,866	21.87
RAYMOND ROAD	WARREN ROAD	FRAMINGHAM TOWN LINE	Mill/Overlay A/C	2,368	71	$91,214	21.25
FAIRBANK ROAD	HUDSON ROAD	MAYNARD ROAD	Mill/Overlay A/C	4,358	72	$152,600	20.96
PEAKHAM ROAD	ROBERT BEST ROAD (N)	MARKED TREE ROAD	Mill/Overlay A/C	3,061	72	$117,918	20.96
WOODSIDE ROAD	WARREN ROAD	HOPESTILL BROWN ROAD (N)	Mill/Overlay A/C	1,278	72	$40,272	20.96
CONCORD ROAD	PANTRY ROAD	TWIN POND LANE	Mill/Overlay A/C	1,968	72	$82,677	20.96
CONCORD ROAD	TWIN POND LANE	4700' S OF TOWN LINE	Mill/Overlay A/C	3,770	72	$171,623	20.96
STOCK FARM ROAD	VICTORIA ROAD	LANDHAM ROAD	Mill/Overlay Local	3,595	61	$118,757	6.55
WILLARD GRANT ROAD	NORTH ROAD	BLACKSMITH DRIVE	Mill/Overlay Local	1,396	61	$55,339	6.55

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Structural Improvement							
DRUM LANE	COLONIAL ROAD	DEMARCO ROAD	Mill/Overlay Local	1,347	62	$48,947	6.45
CEDAR CREEK ROAD	WILLARD GRANT ROAD	POWERS ROAD	Mill/Overlay Local	2,246	63	$89,033	6.34
POWDER MILL ROAD	NORTH ROAD	TAVERN CIRCLE	Mill/Overlay Local	3,633	63	$108,023	6.34
STATION ROAD	BOSTON POST ROAD	UNION AVENUE	Mill/Overlay Local	1,241	65	$40,995	6.15
WILLIS ROAD	BRYANT ROAD	FORD ROAD	Mill/Overlay Local	2,502	65	$78,506	6.15
WILLIS ROAD	FORD ROAD	MOSSMAN ROAD	Mill/Overlay Local	1,991	65	$62,485	6.15
PUFFER LANE	NORTH ROAD	HAYNES ROAD	Mill/Overlay Local	1,685	66	$55,662	6.06
ROBBINS ROAD	BENT ROAD	HOWELL ROAD	Mill/Overlay Local	1,676	67	$55,365	5.97
MOSSMAN ROAD	WILLIS ROAD	NORTH ROAD	Mill/Overlay Local	3,016	67	$99,631	5.97
HARVARD DRIVE	FORD ROAD	LONGFELLOW ROAD	Mill/Overlay Local	1,174	68	$46,538	5.88
ROBERT BEST ROAD	PEAKHAM ROAD	PEAKHAM ROAD	Mill/Overlay Local	2,499	72	$94,935	5.55
OLD GARRISON ROAD (Y INT)	OLD GARRISON ROAD	PEAKHAM ROAD	Mill/Overlay Local	116	72	$3,257	5.55
TANBARK ROAD	DUTTON ROAD	CUL DE SAC	Mill/Overlay Local	1,933	62	$69,514	2.58
STAGECOACH DRIVE	LANDHAM ROAD	CUL DE SAC	Mill/Overlay Local	283	64	$17,020	2.50
HEMLOCK ROAD	CUL DE SAC	300' S OF TANBARK ROAD	Mill/Overlay Local	1,644	64	$57,628	2.50
LOMBARD LANE	RAMBLING ROAD	CUL DE SAC	Mill/Overlay Local	628	66	$33,255	2.42
MARKED TREE ROAD	PEAKHAM ROAD	CUL DE SAC	Mill/Overlay Local	832	66	$34,242	2.42
SAWMILL LANE	LINCOLN ROAD	DEAD END	Mill/Overlay Local	1,012	66	$25,073	2.42
WADSWORTH ROAD	HAYNES ROAD	CUL DE SAC	Mill/Overlay Local	851	66	$41,676	2.42
DUNSTER ROAD	HAYNES ROAD	CUL DE SAC	Mill/Overlay Local	1,117	68	$50,649	2.35
PENNY MEADOW ROAD	CONCORD ROAD	CUL DE SAC	Mill/Overlay Local	1,589	68	$51,517	2.35
TIPPLING ROCK ROAD	DUDLEY ROAD	CUL DE SAC	Mill/Overlay Local	585	68	$21,432	2.35
CANDLEWOOD CIRCLE	PEAKHAM ROAD	CUL DE SAC	Mill/Overlay Local	293	69	$14,519	2.31
MEADOWBROOK ROAD	WILLOW ROAD	CUL DE SAC	Mill/Overlay Local	837	69	$35,810	2.31
PEAKHAM CIRCLE	PEAKHAM ROAD	CUL DE SAC	Mill/Overlay Local	489	69	$13,394	2.31
WILLOW ROAD	PRATTS MILL ROAD	HEMLOCK ROAD	Mill/Overlay Local	1,275	69	$46,323	2.31
CURTISS CIRCLE	ROBERT BEST ROAD	CUL DE SAC	Mill/Overlay Local	682	70	$36,043	2.28
FIELDSTONE FARM ROAD	RICE ROAD	CUL DE SAC	Mill/Overlay Local	1,186	70	$52,145	2.28
JARMAN ROAD	HORSE POND ROAD	DEAD END	Mill/Overlay Local	2,153	70	$85,347	2.28
WEST STREET	PRATTS MILL ROAD	DEAD END	Mill/Overlay Local	505	70	$18,350	2.28
PETERSEN CIRCLE	ATKINSON LANE	CUL DE SAC	Mill/Overlay Local	306	71	$21,138	2.25
VIRGINIA RIDGE ROAD	POWDER MILL ROAD	CUL DE SAC LOOP	Mill/Overlay Local	1,744	71	$69,134	2.25
MOORE ROAD	DUTTON ROAD	FIRECUT LANE	Mill/Overlay Local	2,613	71	$81,996	2.25
MOORE ROAD	FIRECUT LANE	DEAD END	Mill/Overlay Local	2,260	71	$70,929	2.25

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Preventive Maintenance							
HUDSON ROAD	100' W OF AUGUST ROAD	100' W OF INTERVALE ROAD	Prev. Maint	3,618	81	$50,560	183.87
HUDSON ROAD	100' E OF MAYNARD ROAD	OLD LANCASTER ROAD	Prev. Maint	1,638	82	$22,039	181.62
HUDSON ROAD	OLD LANCASTER ROAD	TEAKETTLE LANE	Prev. Maint	2,429	82	$30,164	181.62
HUDSON ROAD	TEAKETTLE LANE	100' W OF AUGUST ROAD	Prev. Maint	2,170	82	$26,948	181.62
MAYNARD ROAD	HUDSON ROAD	400' N OF HUDSON ROAD	Prev. Maint	400	83	$7,867	179.44
MAYNARD ROAD	400' N OF HUDSON ROAD	WILLIS ROAD	Prev. Maint	3,442	83	$57,003	179.44
OLD SUDBURY ROAD	RICE ROAD €	WAYLAND TOWN LINE	Prev. Maint	3,393	84	$49,168	177.30
LANDHAM ROAD	1666' S OF BOSTON POST ROAD	WOODSIDE ROAD	Prev. Maint	2,420	84	$35,071	177.30
LANDHAM ROAD	WOODSIDE ROAD	FRAMINGHAM TOWN LINE	Prev. Maint	3,144	84	$45,552	177.30
GREAT ROAD	NORTH ROAD	MAYNARD TOWN LINE	Prev. Maint	1,647	85	$23,867	175.21
NORTH ROAD	GREAT ROAD	MOSSMAN ROAD	Prev. Maint	1,809	85	$26,208	175.21
LANDHAM ROAD	BOSTON POST ROAD	1666' S OF BOSTON POST ROAI	Prev. Maint w/Patch	1,666	79	$35,962	144.66
NORTH ROAD	PANTRY ROAD	WINDMILL DRIVE	Prev. Maint w/Patch	1,205	79	$26,000	144.66
PANTRY ROAD	NORTH ROAD	CONCORD ROAD	Prev. Maint	4,166	80	$53,901	37.23
PEAKHAM ROAD	HORSE POND ROAD	OLD LANCASTER ROAD	Prev. Maint	2,201	82	$25,054	36.32
PEAKHAM ROAD	OLD LANCASTER ROAD	HUDSON ROAD	Prev. Maint	3,181	82	$36,217	36.32
HORSE POND ROAD	BOSTON POST ROAD	TALL PINE DRIVE	Prev. Maint	4,009	82	$49,799	36.32
HORSE POND ROAD	TALL PINE DRIVE	PEAKHAM ROAD	Prev. Maint	2,460	82	$30,560	36.32
RAYMOND ROAD	BOSTON POST ROAD	WARREN ROAD	Prev. Maint	3,997	83	$45,508	35.88
CONCORD ROAD	100' N OF CONCORD ROAD (Y INT)	NEW BRIDGE ROAD	Prev. Maint	4,531	83	$67,997	35.88
DUTTON ROAD	PRATTS MILL ROAD	MOORE ROAD	Prev. Maint	1,972	84	$22,457	35.46
BOWDITCH ROAD	BOSTON POST ROAD	FRAMINGHAM TOWN LINE	Prev. Maint	1,625	85	$16,820	35.04
WAYSIDE INN ROAD	1500' W OF MARLBOROUGH TOWN LI	MARLBORO TOWN LINE	Prev. Maint	1,502	85	$20,982	35.04
DUTTON ROAD	MOORE ROAD	HUDSON ROAD	Prev. Maint	957	85	$9,908	35.04
DUTTON ROAD	OLD GARRISON ROAD	TANBARK ROAD	Prev. Maint	2,724	85	$28,193	35.04
DUTTON ROAD	TANBARK ROAD	PRATTS MILL ROAD	Prev. Maint	2,450	85	$25,356	35.04
LINCOLN ROAD	WATER ROW	WEIR HILL ROAD	Prev. Maint w/Patch	2,533	73	$39,045	31.31
LINCOLN ROAD	WEIR HILL ROAD	WAYLAND TOWN LINE	Prev. Maint w/Patch	3,479	73	$53,637	31.31
GOODMANS HILL ROAD	GOODMANS HILL ROAD	BOSTON POST ROAD	Prev. Maint w/Patch	4,360	75	$67,213	30.47
PEAKHAM ROAD	MARKED TREE ROAD	HORSE POND ROAD	Prev. Maint w/Patch	1,986	76	$33,679	30.07
WAYSIDE INN ROAD	350' E OF DUTTON ROAD	1500' W OF MARLBOROUGH TC	Prev. Maint w/Patch	759	76	$15,802	30.07
WOODSIDE ROAD	LANDHAM ROAD	CUTLER FARM ROAD	Prev. Maint w/Patch	1,295	76	$17,969	30.07
CONCORD ROAD	NEW BRIDGE ROAD	400' N OF LINCOLN ROAD	Prev. Maint w/Patch	1,405	76	$30,321	30.07
MARLBORO ROAD	MORSE ROAD	HAYNES ROAD	Prev. Maint w/Patch	2,607	76	$40,186	30.07

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Preventive Maintenance							
PEAKHAM ROAD	BOSTON POST ROAD	FRENCH ROAD	Prev. Maint w/Patch	2,366	77	$40,121	29.68
WAYSIDE INN ROAD	MARLBOROUGH TOWN LINE	350' E OF DUTTON ROAD	Prev. Maint w/Patch	3,500	77	$72,840	29.68
CONCORD ROAD	UNION AVENUE	GOODMANS HILL ROAD	Prev. Maint w/Patch	2,379	77	$47,679	29.68
GOODMANS HILL ROAD	CONCORD ROAD	GOODMANS HILL ROAD	Prev. Maint w/Patch	3,929	77	$60,566	29.68
CONCORD ROAD	4700' S OF TOWN LINE	CONCORD TOWN LINE	Prev. Maint w/Patch	4,701	79	$79,715	28.93
MARLBORO ROAD	WILLIS ROAD	MORSE ROAD	Prev. Maint w/Patch	2,917	79	$44,960	28.93
HOMESTEAD STREET	PEAKHAM ROAD	COLONIAL ROAD	Prev. Maint	531	75	$6,595	9.92
BROOKS ROAD	LANDHAM ROAD	MURRAY DRIVE	Prev. Maint	920	77	$11,427	9.67
HAYNES ROAD	MARLBORO ROAD	PUFFER LANE	Prev. Maint	1,659	77	$17,175	9.67
HAYNES ROAD	PUFFER LANE	NORTH ROAD	Prev. Maint	2,704	77	$27,985	9.67
MAYNARD FARM ROAD	POWERS ROAD	POWERS ROAD	Prev. Maint	4,380	78	$54,403	9.54
BENT ROAD	PEAKHAM ROAD	ROBBINS ROAD	Prev. Maint	2,842	78	$33,834	9.54
KENDRA LANE	WILLIS ROAD	CUDWORTH LANE	Prev. Maint	928	78	$10,568	9.54
ALTA ROAD	WOODSIDE ROAD	CLIFFORD ROAD	Prev. Maint	914	79	$10,407	9.42
CARDING MILL ROAD	BOSTON POST ROAD	BIGELOW DRIVE	Prev. Maint	1,311	79	$16,284	9.42
LINDEN ROAD	POPLAR STREET	MAGNOLIA ROAD	Prev. Maint	758	79	$7,061	9.42
AUGUST ROAD	HUDSON ROAD	PRIVATE PORTION	Prev. Maint	400	79	$4,140	9.42
DEER POND ROAD	MAYNARD FARM ROAD	MAYNARD FARM ROAD	Prev. Maint	1,195	80	$14,843	9.30
HOPESTILL BROWN ROAD	WOODSIDE ROAD	WOODSIDE ROAD	Prev. Maint	2,345	80	$29,127	9.30
POPLAR STREET	HEMLOCK ROAD	PRIVATE PORTION	Prev. Maint	878	80	$9,997	9.30
ROLLING LANE	JARMAN ROAD	JARMAN ROAD	Prev. Maint	1,456	80	$18,085	9.30
KENDALL ROAD	HOBART ROAD	TANBARK ROAD	Prev. Maint	1,151	81	$14,296	9.19
COLONIAL ROAD	OLD LANCASTER ROAD	200' W OF HOMESTEAD AVENU	Prev. Maint	876	81	$11,140	9.19
NEW BRIDGE ROAD	CLARK ROAD	WATER ROW	Prev. Maint	2,452	81	$21,576	9.19
BABE RUTH DRIVE	HUDSON ROAD	ATKINSON LANE	Prev. Maint	1,352	82	$16,793	9.08
SYCAMORE ROAD	POPLAR STREET	MAGNOLIA ROAD	Prev. Maint	885	82	$9,160	9.08
REVERE STREET	LAFAYETTE DRIVE	WASHINGTON DRIVE	Prev. Maint	519	84	$5,372	8.86
PRATTS MILL ROAD	PEAKHAM ROAD	WILLOW ROAD	Prev. Maint	2,750	84	$28,462	8.86
MORSE ROAD	CONCORD ROAD	WAKE ROBIN ROAD	Prev. Maint	3,807	84	$37,439	8.86
MORSE ROAD	WAKE ROBIN ROAD	MARLBORO ROAD	Prev. Maint	3,224	84	$31,698	8.86
WILLIS ROAD	MAYNARD ROAD	MARLBORO ROAD	Prev. Maint	2,525	84	$24,829	8.86
FORD ROAD	GREAT ROAD	BELCHER DRIVE	Prev. Maint	2,536	84	$31,500	8.86
JULIANS WAY	HAYNES ROAD	GREYSTONE LANE	Prev. Maint	1,497	85	$18,594	8.76
SHADOW OAK DRIVE	STOCK FARM ROAD	STOCK FARM ROAD	Prev. Maint	2,637	85	$40,942	8.76

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Preventive Maintenance							
WARREN ROAD	RAYMOND ROAD	WOODSIDE ROAD	Prev. Maint	3,039	85	$39,320	8.76
WYMAN DRIVE	MAYNARD ROAD	CUDWORTH LANE	Prev. Maint	1,333	85	$16,557	8.76
MARY CATHERINE LANE	100' N OF NORTH ROAD (E)	NORTH ROAD (E)	Prev. Maint	1,017	85	$12,631	8.76
BRIMSTONE LANE	BOSTON POST ROAD	FRAMINGHAM TOWN LINE	Prev. Maint w/Patch	4,323	73	$46,650	7.82
PARTRIDGE LANE	SURREY LANE	PONDVIEW ROAD	Prev. Maint w/Patch	770	73	$13,574	7.82
MAYBURY ROAD	GREAT ROAD	GREAT ROAD	Prev. Maint w/Patch	691	74	$11,718	7.72
OLD FRAMINGHAM ROAD	NOBSCOT ROAD	FRAMINGHAM TOWN LINE	Prev. Maint w/Patch	2,325	75	$32,258	7.61
FRENCH ROAD	PEAKHAM ROAD	GARRISON ROAD	Prev. Maint w/Patch	105	76	$1,457	7.51
NEW BRIDGE ROAD	CONCORD ROAD	CLARK ROAD	Prev. Maint w/Patch	2,443	76	$32,007	7.51
DARVELL DRIVE	WITHERELL DRIVE	BELCHER DRIVE	Prev. Maint w/Patch	690	77	$12,764	7.42
ENGLISH ROAD	CANTERBURY DRIVE	ARROWHEAD ROAD	Prev. Maint w/Patch	399	77	$7,381	7.42
NOYES LANE	HORSE POND ROAD	MARKED TREE ROAD	Prev. Maint w/Patch	636	77	$11,765	7.42
MARY CATHERINE LANE	NORTH ROAD (W)	100' N OF NORTH ROAD (E)	Prev. Maint w/Patch	848	77	$15,689	7.42
FOX RUN	PEAKHAM ROAD	SADDLE RIDGE ROAD	Prev. Maint w/Patch	3,015	78	$55,775	7.32
WATER ROW	OLD SUDBURY ROAD	PLYMPTON ROAD	Prev. Maint w/Patch	2,979	78	$32,148	7.32
POWERS ROAD	NORTH ROAD	CEDAR CREEK ROAD	Prev. Maint w/Patch	2,987	78	$41,448	7.32
POWERS ROAD	CEDAR CREEK ROAD	CONCORD TOWN LINE	Prev. Maint w/Patch	1,737	78	$24,095	7.32
WILLIS ROAD	MARLBORO ROAD	BRYANT ROAD	Prev. Maint w/Patch	2,721	78	$39,854	7.32
WATER ROW	PLYMPTON ROAD	3050' S OF LINCOLN ROAD	Prev. Maint w/Patch	4,032	79	$43,505	7.23
PLYMPTON ROAD	CONCORD ROAD	CLARK ROAD	Prev. Maint w/Patch	1,795	79	$24,902	7.23
PLYMPTON ROAD	CLARK ROAD	WATER ROW	Prev. Maint w/Patch	3,488	79	$48,396	7.23
MOSSMAN ROAD	MARLBORO ROAD	LEDGE ROAD	Prev. Maint w/Patch	3,492	80	$53,832	7.14
MOSSMAN ROAD	LEDGE ROAD	WILLIS ROAD	Prev. Maint w/Patch	1,257	80	$19,378	7.14
PERRY CIRCLE	ATKINSON LANE	CUL DE SAC	Prev. Maint	453	74	$8,449	4.02
ANSELM WAY	LANDHAM ROAD	CUL DE SAC	Prev. Maint	1,201	76	$16,525	3.91
LEE ANNE CIRCLE	HUDSON ROAD	CUL DE SAC	Prev. Maint	169	76	$4,110	3.91
ADAMS ROAD	DUDLEY ROAD	CUL DE SAC	Prev. Maint	834	77	$13,182	3.86
FAIRHAVEN CIRCLE	HUDSON ROAD	CUL DE SAC LOOP	Prev. Maint	858	77	$10,657	3.86
BISHOP LANE	THOMPSON DRIVE	CUL DE SAC	Prev. Maint	1,094	78	$15,763	3.81
COLBURN CIRCLE	ROBERT BEST ROAD	CUL DE SAC	Prev. Maint	559	78	$9,432	3.81
CANTERBURY DRIVE	HAMPSHIRE STREET	CUL DE SAC	Prev. Maint	1,631	78	$23,081	3.81
TAINTOR DRIVE	400' S OF WITHERELL DRIVE	CUL DE SAC LOOP	Prev. Maint	763	78	$9,471	3.81
BIGELOW DRIVE	BOSTON POST ROAD	CUL DE SAC	Prev. Maint	3,010	79	$39,875	3.77
BRIANT DRIVE	WILLIS ROAD	CUL DE SAC	Prev. Maint	1,514	79	$21,982	3.77

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Preventive Maintenance							
HAMPSHIRE STREET	WILLIS ROAD	CUL DE SAC	Prev. Maint	983	79	$13,817	3.77
PONDVIEW ROAD	SURREY LANE	CUL DE SAC	Prev. Maint	1,282	79	$17,531	3.77
PURITAN LANE	CUL DE SAC	CUL DE SAC	Prev. Maint	1,402	79	$17,157	3.77
WALKUP ROAD	CONCORD ROAD	CUL DE SAC	Prev. Maint	418	79	$6,724	3.77
WHITE OAK LANE	MOORE ROAD	CUL DE SAC	Prev. Maint	1,163	79	$15,355	3.77
WRIGHT ROAD	ALTA ROAD	CUL DE SAC	Prev. Maint	1,005	79	$13,649	3.77
ATKINSON LANE	DUTTON ROAD	CUL DE SAC	Prev. Maint	2,582	80	$34,245	3.72
CRESCENT LANE	MAYNARD ROAD	CUL DE SAC LOOP	Prev. Maint	1,523	80	$18,917	3.72
HAWES ROAD	DAKIN ROAD	CUL DE SAC	Prev. Maint	1,012	80	$13,783	3.72
HUNT ROAD	RIDGE HILL ROAD	CUL DE SAC	Prev. Maint	1,575	80	$20,473	3.72
SADDLE RIDGE ROAD	FOX RUN	CUL DE SAC	Prev. Maint	1,083	80	$15,626	3.72
SCOTTS WOOD DRIVE	PRATTS MILL ROAD	CUL DE SAC	Prev. Maint	490	80	$7,208	3.72
TRAILSIDE CIRCLE	BRIDLE PATH	CUL DE SAC	Prev. Maint	465	80	$6,898	3.72
WALKER FARM ROAD	GOODMANS HILL ROAD	CUL DE SAC	Prev. Maint	962	80	$13,071	3.72
WILDWOOD LANE	OLD LANCASTER ROAD	CUL DE SAC	Prev. Maint	596	80	$10,225	3.72
WILLOW ROAD	HEMLOCK ROAD	CUL DE SAC	Prev. Maint	942	80	$12,782	3.72
PIPSISSEWA CIRCLE	WHITE OAK LANE	CUL DE SAC	Prev. Maint	472	81	$6,985	3.67
CLIFFORD ROAD	ALTA ROAD	WARREN ROAD	Prev. Maint	383	81	$4,560	3.67
FOREST STREET	PEAKHAM ROAD	CUL DE SAC	Prev. Maint	1,465	82	$20,077	3.63
GOODNOW ROAD	HUDSON ROAD	DEAD END	Prev. Maint	1,898	82	$15,716	3.63
HAWTHORNE DRIVE	PELHAM ISLAND ROAD	CUL DE SAC	Prev. Maint	556	82	$8,261	3.63
LAUREL CIRCLE	WHITE OAK LANE	CUL DE SAC	Prev. Maint	445	82	$6,882	3.63
MURRAY DRIVE	AMES ROAD	CUL DE SAC	Prev. Maint	644	82	$9,354	3.63
PILGRIMS PATH	PURITAN LANE	CUL DE SAC	Prev. Maint	916	82	$12,822	3.63
RIDGE HILL ROAD	MORSE ROAD	HUNT ROAD	Prev. Maint	1,684	82	$20,917	3.63
SURREY LANE	MOORE ROAD	CUL DE SAC	Prev. Maint	2,078	82	$26,528	3.63
TANTAMOUSE TRAIL	HAWES ROAD	CUL DE SAC	Prev. Maint	1,038	82	$14,501	3.63
TAVERN CIRCLE	POWDER MILL ROAD	CUL DE SAC	Prev. Maint	1,100	82	$14,215	3.63
WASHINGTON DRIVE	CUL DE SAC	CUL DE SAC	Prev. Maint	2,037	82	$28,334	3.63
WOODBERRY ROAD	FOREST STREET	CUL DE SAC	Prev. Maint	357	82	$5,344	3.63
BALCOM ROAD	RUDDOCK ROAD	CUL DE SAC	Prev. Maint	1,370	83	$18,371	3.58
HERMITAGE STREET	HAMPSHIRE STREET	CUL DE SAC	Prev. Maint	705	83	$10,638	3.58
JASON DRIVE	LANDHAM ROAD	CUL DE SAC	Prev. Maint	634	83	$9,589	3.58
MAGNOLIA ROAD	DUTTON ROAD	CUL DE SAC	Prev. Maint	936	83	$9,624	3.58

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Preventive Maintenance							
TWIN POND LANE	CONCORD ROAD	CUL DE SAC	Prev. Maint	830	83	$10,214	3.58
CONFIDENCE WAY	CONCORD ROAD	DEAD END	Prev. Maint	266	83	$2,474	3.58
AXDELL ROAD	AUSTIN ROAD	CUL DE SAC	Prev. Maint	630	84	$8,864	3.54
GUZZLEBROOK DRIVE	BECKWITH STREET	CUL DE SAC	Prev. Maint	905	84	$12,397	3.54
KATO SUMMIT	KATO DRIVE	CUL DE SAC	Prev. Maint	264	84	$5,768	3.54
MINEBROOK ROAD	MAYNARD ROAD	CUL DE SAC	Prev. Maint	1,039	84	$14,756	3.54
WIDOW RITES LANE	WYMAN DRIVE	CUL DE SAC	Prev. Maint	1,798	84	$24,102	3.54
BECKWITH STREET	PELHAM ISLAND ROAD	CUL DE SAC	Prev. Maint	976	84	$11,879	3.54
FROST LANE	CONCORD ROAD	CUL DE SAC	Prev. Maint	660	85	$9,320	3.50
GRISCOM ROAD	PELHAM ISLAND ROAD	CUL DE SAC	Prev. Maint	1,322	85	$17,661	3.50
SPILLER CIRCLE	MCLEAN DRIVE	CUL DE SAC	Prev. Maint	240	85	$3,773	3.50
CRESTVIEW DRIVE	HUDSON ROAD	CUL DE SAC	Prev. Maint w/Patch	286	74	$6,962	3.08
MAYNARD FARM CIRCLE	MAYNARD FARM ROAD	CUL DE SAC	Prev. Maint w/Patch	653	74	$13,751	3.08
TWILLINGATE LANE	HUDSON ROAD	CUL DE SAC LOOP	Prev. Maint w/Patch	588	74	$10,877	3.08
PADDOCK WAY	DAKIN ROAD	CUL DE SAC LOOP	Prev. Maint w/Patch	1,001	75	$15,431	3.04
BUTLER ROAD	FAIRBANK ROAD	THORNBERRY LANE	Prev. Maint w/Patch	732	75	$12,916	3.04
FIELD ROAD	DAKIN ROAD	CUL DE SAC	Prev. Maint w/Patch	1,038	76	$20,514	3.00
MORAN CIRCLE	RAYMOND ROAD	CUL DE SAC	Prev. Maint w/Patch	1,061	76	$22,866	3.00
GREYSTONE LANE	CUL DE SAC	JULIANS WAY	Prev. Maint w/Patch	850	76	$17,742	3.00
CODJER LANE	CONCORD ROAD	UNION AVENUE	Prev. Maint w/Patch	1,568	78	$19,340	2.93
FOX HILL DRIVE	MARY CATHERINE LANE	CUL DE SAC	Prev. Maint w/Patch	965	80	$21,462	2.85
KATO DRIVE	GOODMANS HILL ROAD	CUL DE SAC	Prev. Maint w/Patch	2,191	80	$37,775	2.85
Routine Maintenance							
NORTH ROAD	MAYNARD TOWN LINE	GREAT ROAD	Crackseal	1,401	86	$1,604	1162.79
NORTH ROAD	WINDMILL DRIVE	NORTHWOOD DRIVE	Crackseal	3,140	86	$3,872	1162.79
NORTH ROAD	MOSSMANN ROAD	WILLARD GRANT ROAD	Crackseal	2,960	86	$3,912	1162.79
NOBSCOT ROAD	DUDLEY ROAD	FRAMINGHAM TOWN LINE	Crackseal	2,914	87	$3,851	1149.42
UNION AVENUE	STATION ROAD	PHEASANT AVENUE	Crackseal	3,777	87	$4,658	1149.42
NORTH ROAD	NORTHWOOD DRIVE	CONCORD TOWN LINE	Crackseal	2,313	87	$2,648	1149.42
LANDHAM ROAD (SB Y INT)	BOSTON POST ROAD	LANDHAM ROAD	Crackseal	119	89	$105	1123.59
NORTH ROAD	WILLARD GRANT ROAD	PANTRY ROAD	Crackseal/Patch	2,225	86	$12,349	322.99
MAYNARD ROAD	MARTIN DRIVE	MARLBORO ROAD	Crackseal/Patch	1,514	86	$9,600	322.99
OLD LANCASTER ROAD	PEAKHAM ROAD	CONCORD ROAD	Crackseal	3,205	87	$3,529	229.88

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Routine Maintenance							
CONCORD ROAD	BOSTON POST ROAD	CODJER LANE	Crackseal	2,523	87	$2,889	229.88
CONCORD ROAD	CODJER LANE	UNION AVENUE	Crackseal	1,968	87	$2,254	229.88
OLD COUNTY ROAD	BOSTON POST ROAD	500' E OF BOSTON POST ROAD	Crackseal	500	88	$573	227.27
DAKIN ROAD	NORTH ROAD	CONCORD TOWN LINE	Crackseal	3,743	89	$3,297	224.71
CONCORD ROAD	400' N OF LINCOLN ROAD	PANTRY ROAD	Crackseal	3,735	90	$4,112	222.22
DUTTON ROAD	963' S OF OLD GARRISON ROAD	OLD GARRISON ROAD	Crackseal/Patch	964	86	$3,820	64.59
MARLBORO ROAD	MAYNARD ROAD	WILLIS ROAD	Crackseal/Patch	2,277	86	$9,026	64.59
CANDY HILL ROAD	CONCORD ROAD	PLYMPTON ROAD	Crackseal	1,472	87	$1,167	57.47
POKONOKET AVENUE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	Crackseal	2,753	87	$2,425	57.47
WITHERELL DRIVE	WILLIS ROAD	BELCHER DRIVE	Crackseal	2,449	87	$2,373	57.47
PRIDES CROSSING ROAD	WAYSIDE INN ROAD	JACK LINE DR	Crackseal	3,396	87	$3,888	57.47
PRIDES CROSSING ROAD	JACK PINE DRIVE	MARLBORO TOWN LINE	Crackseal	1,418	87	$1,624	57.47
STONE ROAD	BOSTON POST ROAD	DOUGLAS DRIVE	Crackseal	1,092	87	$1,010	57.47
BELCHER DRIVE	WILLIS ROAD	FORD ROAD	Crackseal	4,311	88	$4,557	56.81
BROOKDALE ROAD	LANDHAM ROAD	STOCK FARM ROAD	Crackseal	2,348	88	$2,275	56.81
ELMWOOD AVENUE	LAKE SHORE DRIVE	ARBORWOOD ROAD	Crackseal	384	88	$237	56.81
GREEN HILL ROAD	BOSTON POST ROAD	GOODMANS HILL ROAD	Crackseal	2,377	88	$2,094	56.81
JUNIPER ROAD	WAKE ROBIN ROAD	WAKE ROBIN ROAD	Crackseal	1,096	88	$1,159	56.81
KAY STREET	BROOKDALE ROAD	STOCK FARM ROAD	Crackseal	1,544	88	$1,496	56.81
UPLOOK DRIVE	BOSTON POST ROAD	EASY STREET	Crackseal	347	88	$336	56.81
VICTORIA ROAD	STOCK FARM ROAD	STOCK FARM ROAD	Crackseal	3,202	88	$3,526	56.81
WINSOR ROAD	SINGLETARY LANE	OLD LANCASTER ROAD	Crackseal	2,331	88	$2,053	56.81
OLD SUDBURY ROAD (LOOP)	OLD SUDBURY ROAD	OLD SUDBURY ROAD	Crackseal	493	88	$347	56.81
OLD LANCASTER ROAD	CONCORD ROAD	GREEN HILL ROAD	Crackseal	4,101	88	$3,251	56.81
BASSWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Crackseal	1,911	89	$1,515	56.17
BEECHWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Crackseal	1,867	89	$1,480	56.17
BIRCHWOOD AVENUE	WILLIS LAKE DRIVE	GREAT LAKE DRIVE	Crackseal	1,016	89	$895	56.17
BRIDLE PATH	TALL PINE DRIVE	TALL PINE DRIVE	Crackseal	2,547	89	$2,692	56.17
CLARK ROAD	PLYMPTON ROAD	NEW BRIDGE ROAD	Crackseal	1,392	89	$981	56.17
CROWN POINT ROAD	WILLIS ROAD	PENDLETON ROAD	Crackseal	984	89	$1,040	56.17
EDDY STREET	LANDHAM ROAD	BROOKDALE ROAD	Crackseal	1,699	89	$1,646	56.17
ELLIOT ROAD	OLD MEADOW ROAD	HORSE POND ROAD	Crackseal	437	89	$443	56.17
GREAT LAKE DRIVE	HUDSON ROAD	BIRCHWOOD AVENUE	Crackseal	1,179	89	$1,039	56.17
HENRYS MILL LANE	FRENCH ROAD	CARRIAGE WAY	Crackseal	1,539	89	$1,627	56.17

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Routine Maintenance							
INDIAN RIDGE ROAD	KING PHILIP ROAD	OLD LANCASTER ROAD	Crackseal	2,892	89	$2,802	56.17
JACK PINE DRIVE	PRIDES CROSSING ROAD	RED OAK DRIVE	Crackseal	1,825	89	$2,090	56.17
LEDGE ROAD	MOSSMAN ROAD	SYLVAN WAY	Crackseal	996	89	$877	56.17
LOKER ROAD	WILLIS ROAD	BALCOM ROAD	Crackseal	399	89	$422	56.17
LONGFELLOW ROAD	NORTH ROAD	FORD ROAD	Crackseal	4,169	89	$4,407	56.17
MICHAEL LANE	WIDOW RITES LANE	CUDWORTH LANE	Crackseal	297	89	$288	56.17
OAKWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	Crackseal	1,881	89	$1,408	56.17
OLD COACH ROAD	LEDGE ROAD	FARM LANE	Crackseal	2,058	89	$1,813	56.17
PINEWOOD AVENUE	CRYSTAL LAKE DRIVE	WILLIS LAKE DRIVE	Crackseal	2,031	89	$1,431	56.17
POSSUM LANE	NORTH ROAD	MOSSMAN ROAD	Crackseal	1,336	89	$1,412	56.17
READ ROAD	CUTLER FARM ROAD	CUTLER FARM ROAD	Crackseal	761	89	$804	56.17
SINGLETARY LANE	KING PHILIP ROAD	GREEN HILL ROAD	Crackseal	1,412	89	$1,244	56.17
THUNDER ROAD	BALCOM ROAD	SILVER HILL ROAD	Crackseal	3,023	89	$3,329	56.17
WILLIS LAKE DRIVE	HUDSON ROAD	ARBORWOOD ROAD	Crackseal	1,886	89	$1,661	56.17
LINCOLN ROAD (WB Y INT)	CONCORD ROAD	WAYLAND TOWN LINE	Crackseal	292	89	$257	56.17
LINCOLN ROAD (EB Y INT)	LINCOLN ROAD	WAYLAND TOWN LINE	Crackseal	180	89	$159	56.17
MAPLEWOOD AVENUE	LAKEWOOD DRIVE	DEAD END	Crackseal	1,033	89	$819	56.17
LAKEWOOD DRIVE	BASSWOOD AVENUE	LAKE SHORE DRIVE	Crackseal	509	89	$403	56.17
ALLEN PLACE	MASSASOIT AVENUE	INDIAN RIDGE ROAD	Crackseal	507	90	$447	55.55
BLUEBERRY HILL LANE	PEAKHAM ROAD	PEAKHAM ROAD	Crackseal	2,767	90	$2,559	55.55
BOWKER DRIVE	FORD ROAD	ELSBETH ROAD	Crackseal	1,225	90	$1,295	55.55
CLARK LANE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	Crackseal	1,414	90	$996	55.55
FARM LANE	MOSSMAN ROAD	OLD COACH ROAD	Crackseal	449	90	$396	55.55
MEADOWBROOK CIRCLE	PEAKHAM ROAD	MEADOWBROOK ROAD	Crackseal	2,164	90	$1,716	55.55
NASHOBA ROAD	GOODMANS HILL ROAD	PURITAN LANE	Crackseal	872	90	$922	55.55
WINTER STREET (SOUTHERLY)	SPRING STREET	SUMMER STREET	Crackseal	447	90	$295	55.55
CIDER MILL ROAD	RAYMOND ROAD	RAMBLING ROAD	Crackseal	2,327	91	$2,460	54.94
LETTERY CIRCLE	WOODSIDE ROAD	CUL DE SAC LOOP	Crackseal	1,101	86	$1,164	23.25
STEARNS LANE	MOORE ROAD	CUL DE SAC	Crackseal	1,562	86	$1,629	23.25
CUDWORTH LANE	BRIANT DRIVE	CUL DE SAC	Crackseal	2,138	87	$2,415	22.98
IRONWORKS ROAD	TAINTOR DRIVE	CUL DE SAC LOOP	Crackseal	1,356	87	$1,433	22.98
MILLPOND ROAD	BARTON DRIVE	CUL DE SAC	Crackseal	1,215	87	$1,420	22.98
OLD MEADOW ROAD	DEAD END	DEAD END	Crackseal	1,137	87	$1,202	22.98
RAYNOR ROAD	MOORE ROAD	DEAD END	Crackseal	1,207	87	$1,461	22.98

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Routine Maintenance							
RED OAK DRIVE	CUL DE SAC	CUL DE SAC	Crackseal	1,104	87	$1,684	22.98
REVOLUTIONARY ROAD	WASHINGTON DRIVE	CUL DE SAC	Crackseal	693	87	$848	22.98
SPRUCE LANE	HUDSON ROAD	CUL DE SAC LOOP	Crackseal	569	87	$601	22.98
TREVOR WAY	HORSE POND ROAD	CUL DE SAC LOOP	Crackseal	584	87	$669	22.98
WAGONWHEEL ROAD	LANDHAM ROAD	CUL DE SAC	Crackseal	1,772	87	$1,762	22.98
WHISPERING PINE ROAD	PEAKHAM ROAD	CUL DE SAC	Crackseal	1,124	87	$1,325	22.98
CLIFFORD ROAD	WARREN ROAD	CUL DE SAC	Crackseal	714	87	$822	22.98
CUTTING LANE	ARBORETUM WAY	DEAD END	Crackseal	628	87	$554	22.98
CENTER STREET	PRATTS MILL ROAD	CUL DE SAC	Crackseal	970	88	$804	22.72
COBBLESTONE PLACE	GREYSTONE LANE	CUL DE SAC	Crackseal	500	88	$652	22.72
CRANBERRY CIRCLE	POWDER MILL ROAD	CUL DE SAC	Crackseal	2,711	88	$3,003	22.72
DEACON LANE	PANTRY ROAD	CUL DE SAC	Crackseal	852	88	$996	22.72
EASY STREET	CUL DE SAC	CUL DE SAC	Crackseal	1,297	88	$1,360	22.72
HADLEY ROAD	HAYNES ROAD	CUL DE SAC	Crackseal	999	88	$1,231	22.72
MARK LANE	MAYNARD ROAD	CUL DE SAC LOOP	Crackseal	661	88	$582	22.72
PELHAM ISLAND ROAD	LANDHAM ROAD	CUL DE SAC	Crackseal	3,390	88	$3,285	22.72
PENDLETON ROAD	MARLBORO ROAD	CUL DE SAC	Crackseal	777	88	$958	22.72
SKYVIEW LANE	DAKIN ROAD	CUL DE SAC LOOP	Crackseal	1,246	88	$1,207	22.72
THOMPSON DRIVE	CONCORD ROAD	CUL DE SAC	Crackseal	1,663	88	$1,992	22.72
WAKE ROBIN ROAD	MORSE ROAD	DEAD END	Crackseal	2,009	88	$2,124	22.72
WARD ROAD	MAYNARD ROAD	CUL DE SAC	Crackseal	845	88	$1,009	22.72
WEBSTER CIRCLE	PHILLIPS ROAD	CUL DE SAC	Crackseal	996	88	$1,293	22.72
WINDMILL DRIVE	NORTH ROAD	CUL DE SAC	Crackseal	1,308	88	$1,250	22.72
CARRIAGE WAY	FRENCH ROAD	CUL DE SAC LOOP	Crackseal	2,266	88	$2,495	22.72
HARNESS LANE EXT	HARNESS LANE	DEAD END	Crackseal	252	88	$178	22.72
HARNESS LANE	HUDSON ROAD	CUL DE SAC	Crackseal	1,726	88	$1,961	22.72
CODJER LANE	UNION AVENUE	DEAD END	Crackseal	2,629	88	$4,168	22.72
BARTON DRIVE	DUTTON ROAD	CUL DE SAC	Crackseal	4,079	89	$4,115	22.47
BROWNSTONE LANE	OLD LANCASTER ROAD	CUL DE SAC	Crackseal	559	89	$643	22.47
CHECKERBERRY CIRCLE	POSSUM LANE	CUL DE SAC	Crackseal	571	89	$743	22.47
CRAIG LANE	MARLBORO ROAD	CUL DE SAC LOOP	Crackseal	520	89	$550	22.47
CUTLER FARM ROAD	WOODSIDE ROAD	CUL DE SAC	Crackseal	2,418	89	$2,716	22.47
GRIFFIN LANE	CROWN POINT ROAD	CUL DE SAC	Crackseal	518	89	$748	22.47
HILLTOP ROAD	MORSE ROAD	CUL DE SAC	Crackseal	920	89	$1,006	22.47

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Routine Maintenance							
HOP BROOK LANE	OAK HILL ROAD	CUL DE SAC	Crackseal	717	89	$853	22.47
INTERVALE ROAD	HUDSON ROAD	CUL DE SAC	Crackseal	1,270	89	$1,420	22.47
MCLEAN DRIVE	PRATTS MILL ROAD	CUL DE SAC	Crackseal	996	89	$1,148	22.47
METACOMET WAY	WINSOR ROAD	CUL DE SAC	Crackseal	540	89	$569	22.47
SAUNDERS ROAD	BOWKER DRIVE	CUL DE SAC	Crackseal	499	89	$768	22.47
VILLAGE ROAD	PUFFER LANE	CUL DE SAC	Crackseal	1,091	89	$1,179	22.47
MEADOW DRIVE	PHEASANT AVENUE	CUL DE SAC	Crackseal	754	89	$777	22.47
PHILEMON WHALE LANE	DAKIN ROAD	CUL DE SAC	Crackseal	982	89	$1,249	22.47
GLEN LANE	DUTTON ROAD	DEAD END	Crackseal	263	89	$232	22.47
BARNET ROAD	POWERS ROAD	CUL DE SAC	Crackseal	724	90	$700	22.22
BREWSTER ROAD	GOODMANS HILL ROAD	CUL DE SAC	Crackseal	1,642	90	$2,127	22.22
ELDERBERRY CIRCLE	FAIRBANK ROAD	CUL DE SAC	Crackseal	273	90	$435	22.22
FRANKLIN PLACE	MASSASOIT AVENUE	DEAD END	Crackseal	281	90	$223	22.22
LAFAYETTE DRIVE	BOSTON POST ROAD	CUL DE SAC	Crackseal	1,639	90	$1,861	22.22
MIDDLE ROAD	FIRECUT LANE	DEAD END	Crackseal	991	90	$1,048	22.22
OLD FORGE LANE	COUNTRY VILLAGE LANE	CUL DE SAC	Crackseal	893	90	$1,005	22.22
PHILLIPS ROAD	FAIRBANK ROAD	CUL DE SAC	Crackseal	2,067	90	$2,425	22.22
STONE ROOT LANE	MOSSMAN ROAD	CUL DE SAC LOOP	Crackseal	935	90	$824	22.22
WINTER STREET	BARTON DRIVE	CUL DE SAC	Crackseal	466	90	$499	22.22
FAIRBANK ROAD (Y INT)	HUDSON ROAD	FAIRBANK ROAD	Crackseal/Patch	88	86	$436	16.14
UNION AVENUE	BOSTON POST ROAD	STATION ROAD	Crackseal/Patch	986	86	$6,252	16.14
AUSTIN ROAD	100' S OF HOBART ROAD	PEAKHAM ROAD	Crackseal/Patch	2,243	86	$8,891	16.14
KING PHILIP ROAD	BOSTON POST ROAD	BOSTON POST ROAD	Crackseal/Patch	1,773	87	$6,325	15.96
POWDER MILL ROAD	TAVERN CIRCLE	MAYNARD TOWN LINE	Crackseal/Patch	853	87	$3,549	15.96
ARROWHEAD ROAD	ENGLISH ROAD	CUL DE SAC	Crackseal/Patch	437	86	$2,912	6.45
BOWEN CIRCLE	PLYMPTON ROAD	CUL DE SAC LOOP	Crackseal/Patch	665	86	$3,163	6.45
CHURCH STREET	CONCORD ROAD	DEAD END	Crackseal/Patch	549	86	$2,394	6.45
WHITETAIL LANE	SAWMILL LANE	CUL DE SAC	Crackseal/Patch	499	86	$3,094	6.45
HAMBLIN LANE	MARLBORO ROAD	DEAD END	Crackseal/Patch	207	86	$738	6.45
WILSON ROAD	PRATTS MILL ROAD	CUL DE SAC	Crackseal/Patch	468	87	$2,689	6.38
Do Nothing							
AARON ROAD	FIRECUT LANE	CUL DE SAC	Do Nothing	622	100	$0	0.00
ABBOTTSWOOD	CAMPERDOWN LANE	GATE AT COUNTRY VILLAGE I	Do Nothing	1,190	95	$0	0.00

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Do Nothing							
AMANDA ROAD	DUTTON ROAD	CUL DE SAC	Do Nothing	1,461	95	$0	0.00
AMES ROAD	LANDHAM ROAD	DEAD END	Do Nothing	1,798	95	$0	0.00
AUTUMN STREET	DUTTON ROAD	PINE STREET	Do Nothing	909	100	$0	0.00
BARBARA ROAD	RONALD ROAD	AARON ROAD	Do Nothing	985	100	$0	0.00
BLACKMER ROAD	LANDHAM ROAD	CUL DE SAC	Do Nothing	1,854	95	$0	0.00
BLACKSMITH DRIVE	DAKIN ROAD	WILLARD GRANT ROAD	Do Nothing	1,655	99	$0	0.00
BLANDFORD DRIVE	WILLIS ROAD	CUL DE SAC	Do Nothing	395	100	$0	0.00
BRADLEY PLACE	MASSASOIT AVENUE	INDIAN RIDGE ROAD	Do Nothing	535	99	$0	0.00
BRENTWOOD ROAD	BRIAR PATCH LANE	CUL DE SAC	Do Nothing	565	99	$0	0.00
BRIAR PATCH LANE	PRATTS MILL ROAD	HOLLOW OAK DRIVE	Do Nothing	860	95	$0	0.00
BROOKDALE LANE	BROOKDALE ROAD	CUL DE SAC	Do Nothing	323	95	$0	0.00
BUCKMASTER DRIVE	BELCHER DRIVE	FORD ROAD	Do Nothing	832	100	$0	0.00
CAKEBREAD DRIVE	WITHERELL DRIVE	DEAD END	Do Nothing	563	100	$0	0.00
CAMPERDOWN LANE EXT	1000' E OF FAIRBANK ROAD	CUL DE SAC LOOP	Do Nothing	1,504	95	$0	0.00
CHANTICLEER ROAD	FORD ROAD	LONGFELLOW ROAD	Do Nothing	1,817	100	$0	0.00
CHURCHILL STREET	SAXONY DRIVE	CUL DE SAC	Do Nothing	1,750	95	$0	0.00
CODMAN DRIVE	MORSE ROAD	CUL DE SAC	Do Nothing	1,090	99	$0	0.00
CORTLAND LANE	FAIRBANK ROAD	CUL DE SAC LOOP	Do Nothing	559	100	$0	0.00
COUNTRY VILLAGE LANE	HUDSON ROAD	CUL DE SAC	Do Nothing	1,379	99	$0	0.00
CRYSTAL LAKE DRIVE	HUDSON ROAD	BASSWOOD AVENUE	Do Nothing	1,207	93	$0	0.00
CURRY LANE	GREENWOOD ROAD	ELAINE ROAD	Do Nothing	567	100	$0	0.00
DAWSON DRIVE	JOAN AVENUE	CUL DE SAC	Do Nothing	1,403	99	$0	0.00
DOUGLAS DRIVE	STONE ROAD	CUL DE SAC	Do Nothing	679	100	$0	0.00
ELAINE ROAD	FORD ROAD	LONGFELLOW ROAD	Do Nothing	1,313	99	$0	0.00
ELSBETH ROAD	FORD ROAD	MAYNARD TOWN LINE	Do Nothing	771	99	$0	0.00
EMERSON WAY	MORSE ROAD	CUL DE SAC	Do Nothing	849	95	$0	0.00
EVERGREEN ROAD	HORSE POND ROAD	STONEBROOK ROAD	Do Nothing	1,267	99	$0	0.00
FIRECUT LANE	STEARNS LANE	AARON ROAD	Do Nothing	2,157	99	$0	0.00
FLINTLOCK LANE	WOODMERE DRIVE	STARVIEW DRIVE	Do Nothing	963	99	$0	0.00
GERRY DRIVE	OLD LANCASTER ROAD	WASH BROOK ROAD	Do Nothing	937	99	$0	0.00
GREENWOOD ROAD	FORD ROAD	LONGFELLOW ROAD	Do Nothing	1,217	100	$0	0.00
GRINDSTONE LANE	COUNTRY VILLAGE LANE	CUL DE SAC	Do Nothing	977	99	$0	0.00
HOLLOW OAK DRIVE	BRENTWOOD ROAD	CUL DE SAC	Do Nothing	866	95	$0	0.00
HUNTERS RUN	MEACHEN ROAD	CUL DE SAC	Do Nothing	462	99	$0	0.00

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Do Nothing							
LYNNE ROAD	LANDHAM ROAD	BROOKDALE ROAD	Do Nothing	1,060	99	$0	0.00
MASSASOIT AVENUE	BOSTON POST ROAD	FRANKLIN PLACE	Do Nothing	1,522	94	$0	0.00
MEACHEN ROAD	MARLBORO ROAD	CUL DE SAC	Do Nothing	1,334	95	$0	0.00
MINUTEMAN LANE	MARLBORO ROAD	DEAD END	Do Nothing	690	99	$0	0.00
MUNNINGS DRIVE	BELCHER DRIVE	FORD ROAD	Do Nothing	985	100	$0	0.00
MUSKET LANE	STARVIEW DRIVE	WOODMERE DRIVE	Do Nothing	787	95	$0	0.00
NEWTON ROAD	WHISPERING PINE ROAD	CUL DE SAC	Do Nothing	614	95	$0	0.00
NORMANDY DRIVE	SAXONY DRIVE	CUL DE SAC	Do Nothing	1,685	95	$0	0.00
OAK HILL ROAD	PRATTS MILL ROAD	CUL DE SAC	Do Nothing	1,024	99	$0	0.00
OLD ORCHARD ROAD	BROOKDALE ROAD	STOCK FARM ROAD	Do Nothing	1,062	99	$0	0.00
PATRICIA ROAD	LANDHAM ROAD	DAWSON DRIVE	Do Nothing	1,609	95	$0	0.00
PHEASANT AVENUE	UNION AVENUE	MEADOW DRIVE	Do Nothing	650	98	$0	0.00
PINE RIDGE ROAD	GERRY DRIVE	CUL DE SAC	Do Nothing	579	98	$0	0.00
PINE STREET	BARTON DRIVE	DEAD END	Do Nothing	2,003	100	$0	0.00
PLANTATION CIRCLE	OLD LANCASTER ROAD	CUL DE SAC	Do Nothing	781	100	$0	0.00
RONALD ROAD	HUDSON ROAD	FIRECUT LANE	Do Nothing	1,002	100	$0	0.00
RUN BROOK CIRCLE	FAIRBANK ROAD	CUL DE SAC	Do Nothing	646	95	$0	0.00
RUSSET LANE	LANDHAM ROAD	EDDY STREET	Do Nothing	782	95	$0	0.00
SAXONY DRIVE	HUDSON ROAD	CUL DE SAC	Do Nothing	2,229	95	$0	0.00
SILVER HILL ROAD	MOSSMAN ROAD	THUNDER ROAD	Do Nothing	2,407	98	$0	0.00
SINGING HILL CIRCLE	POWDER MILL ROAD	CUL DE SAC	Do Nothing	221	100	$0	0.00
SOUTH MEADOW DRIVE	NOBSCOT ROAD	CUL DE SAC LOOP	Do Nothing	631	100	$0	0.00
SOUTHWEST CIRCLE	PEAKHAM ROAD	CUL DE SAC LOOP	Do Nothing	424	99	$0	0.00
SPRING STREET	DUTTON ROAD	WINTER STREET (SOUTHERLY	Do Nothing	766	100	$0	0.00
STARVIEW DRIVE	MORSE ROAD	MUSKET LANE	Do Nothing	775	95	$0	0.00
STONEBROOK ROAD	HORSE POND ROAD	EVERGREEN ROAD	Do Nothing	1,076	99	$0	0.00
STUBTOE LANE	COUNTRY VILLAGE LANE	TEAKETTLE LANE	Do Nothing	1,306	95	$0	0.00
SUFFOLK ROAD	BELCHER DRIVE	FORD ROAD	Do Nothing	740	99	$0	0.00
SUMMER STREET	DUTTON ROAD	PINE STREET	Do Nothing	1,159	100	$0	0.00
SUNSET PATH	THUNDER ROAD	CUL DE SAC	Do Nothing	1,140	95	$0	0.00
TALL PINE DRIVE	HORSE POND ROAD	BRIDLE PATH	Do Nothing	1,023	100	$0	0.00
TAYLOR ROAD	UNION AVENUE	CUL DE SAC	Do Nothing	605	100	$0	0.00
TEAKETTLE LANE	HUDSON ROAD	STUBTOE LANE	Do Nothing	465	100	$0	0.00
THORNBERRY LANE	BUTLER ROAD	CUL DE SAC LOOP	Do Nothing	1,377	95	$0	0.00

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Do Nothing							
TUDOR ROAD	NORMANDY DRIVE	CHURCHILL STREET	Do Nothing	593	99	$0	0.00
TWIN MEADOW LANE	FAIRBANK ROAD	CUL DE SAC	Do Nothing	367	100	$0	0.00
WASH BROOK ROAD	CUL DE SAC	CUL DE SAC	Do Nothing	698	98	$0	0.00
WOODLAND ROAD	DUDLEY ROAD	CUL DE SAC	Do Nothing	776	95	$0	0.00
WOODMERE DRIVE	MORSE ROAD	CUL DE SAC	Do Nothing	2,488	95	$0	0.00
ARBORWOOD ROAD	BIRCHWOOD AVENUE	ELMWOOD AVENUE	Do Nothing	225	93	$0	0.00
ARBORETUM WAY	MAYNARD ROAD	CUL DE SAC LOOP	Do Nothing	1,127	100	$0	0.00
LANDHAM ROAD (NB Y INT)	LANDHAM ROAD (NB LEG)	BOSTON POST ROAD	Do Nothing	140	99	$0	0.00
LANDHAM ROAD (NB LEG)	LANDHAM ROAD	BOSTON POST ROAD	Do Nothing	176	98	$0	0.00
CAMPERDOWN LANE	FAIRBANK ROAD	1000' E OF FAIRBANK ROAD	Do Nothing	1,000	100	$0	0.00
PLYMPTON ROAD (Y INT)	PLYMPTON ROAD	CONCORD ROAD	Do Nothing	226	98	$0	0.00
CONCORD ROAD (Y INT)	OLD SUDBURY ROAD	CONCORD ROAD	Do Nothing	276	100	$0	0.00
FIRECUT LANE EXT	AARON ROAD	CUL DE SAC	Do Nothing	375	100	$0	0.00
LAKE SHORE DRIVE	LAKEWOOD DRIVE	GREAT LAKE DRIVE	Do Nothing	353	99	$0	0.00
OLD SUDBURY ROAD	CONCORD ROAD	900' E OF CONCORD ROAD	Do Nothing	900	100	$0	0.00
RAMBLING ROAD	WOODSIDE ROAD	CIDER MILL ROAD	Do Nothing	285	94	$0	0.00
FRENCH ROAD	OLD GARRISON ROAD	DUTTON ROAD	Do Nothing	2,914	100	$0	0.00
HAYNES ROAD	PANTRY ROAD	MARLBORO ROAD	Do Nothing	1,609	94	$0	0.00
HUDSON ROAD	CRYSTAL LAKE DRIVE	STOW TOWN LINE	Do Nothing	3,208	94	$0	0.00
MEADOW DRIVE	OLD LANCASTER ROAD	PHEASANT AVENUE	Do Nothing	383	100	$0	0.00
OLD LANCASTER ROAD	HUDSON ROAD	PEAKHAM ROAD	Do Nothing	2,977	98	$0	0.00
HUDSON ROAD	CONCORD ROAD	100' E OF MAYNARD ROAD	Do Nothing	1,814	100	$0	0.00
CONCORD ROAD	GOODMANS HILL ROAD	100' N OF CONCORD ROAD (Y I	Do Nothing	1,310	100	$0	0.00
STONE ROAD	HICKORY ROAD	BENT ROAD	Do Nothing	473	94	$0	0.00
HICKORY ROAD	ROBBINS ROAD	STONE ROAD	Do Nothing	1,094	100	$0	0.00
HICKORY ROAD	STONE ROAD	CUL DE SAC	Do Nothing	735	100	$0	0.00
STONE ROAD	DOUGLAS DRIVE	HICKORY ROAD	Do Nothing	1,081	94	$0	0.00
MAPLE AVENUE	BOSTON POST ROAD	CUL DE SAC LOOP	Do Nothing	1,404	94	$0	0.00
BUTLER ROAD	THORNBERRY LANE	CUL DE SAC	Do Nothing	1,708	93	$0	0.00
SYLVAN WAY	OLD COACH ROAD	LEDGE ROAD	Do Nothing	628	99	$0	0.00
FORD ROAD	BELCHER DRIVE	WILLIS ROAD	Do Nothing	2,624	94	$0	0.00
TAINTOR DRIVE	WITHERELL DRIVE	400' S OF WITHERELL DRIVE	Do Nothing	407	95	$0	0.00
LAKEWOOD DRIVE	HUDSON ROAD	BASSWOOD AVENUE	Do Nothing	1,039	99	$0	0.00
OLD COUNTY ROAD	500' E OF BOSTON POST ROAD	1300' E OF BOSTON POST ROAI	Do Nothing	800	93	$0	0.00

Budget Scenario: **CurrentPCI**

Street Name	From	To	Alternative	Length (ft)	PCI	Repair Cost	Benefit Value
Do Nothing							
BULKLEY ROAD	AUSTIN ROAD	560' W OF AUSTIN ROAD	Do Nothing	560	95	$0	0.00
BULKLEY ROAD EXT	560' W OF AUSTIN ROAD	AMANDA ROAD	Do Nothing	369	95	$0	0.00
BROOKSIDE FARM LANE	LANDHAM ROAD	CUL DE SAC LOOP	Do Nothing	641	93	$0	0.00
CAIL FARM ROAD	CONCORD ROAD	DEAD END	Do Nothing	221	95	$0	0.00
BALDWIN DRIVE	MAYNARD ROAD	DEAD END	Do Nothing	247	95	$0	0.00

Appendix E: Crackseal Candidate List

Crackseal Candidate List

Street Name	From Segment	To Segment	Length (ft)	PCI
ADAMS ROAD	DUDLEY ROAD	CUL DE SAC	834	78
ALLEN PLACE	MASSASOIT AVENUE	INDIAN RIDGE ROAD	507	90
ALTA ROAD	WOODSIDE ROAD	CLIFFORD ROAD	914	80
ANSELM WAY	LANDHAM ROAD	CUL DE SAC	1,201	77
ARROWHEAD ROAD	ENGLISH ROAD	CUL DE SAC	437	86
ATKINSON LANE	DUTTON ROAD	CUL DE SAC	2,582	81
AUGUST ROAD	HUDSON ROAD	PRIVATE PORTION	400	79
AUSTIN ROAD	100' S OF HOBART ROAD	PEAKHAM ROAD	2,243	86
AXDELL ROAD	AUSTIN ROAD	CUL DE SAC	630	85
BABE RUTH DRIVE	HUDSON ROAD	ATKINSON LANE	1,352	83
BALCOM ROAD	RUDDOCK ROAD	CUL DE SAC	1,370	83
BARTON DRIVE	DUTTON ROAD	CUL DE SAC	4,079	89
BASSWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	1,911	89
BECKWITH STREET	PELHAM ISLAND ROAD	CUL DE SAC	976	85
BEECHWOOD AVENUE	WILLIS LAKE DRIVE	CRYSTAL LAKE DRIVE	1,867	89
BENT ROAD	PEAKHAM ROAD	ROBBINS ROAD	2,842	78
BIGELOW DRIVE	BOSTON POST ROAD	CUL DE SAC	3,010	80
BIRCHWOOD AVENUE	WILLIS LAKE DRIVE	GREAT LAKE DRIVE	1,016	89
BISHOP LANE	THOMPSON DRIVE	CUL DE SAC	1,094	78
BLUEBERRY HILL LANE	PEAKHAM ROAD	PEAKHAM ROAD	2,767	90
BOWEN CIRCLE	PLYMPTON ROAD	CUL DE SAC LOOP	665	86
BOWKER DRIVE	FORD ROAD	ELSBETH ROAD	1,225	90
BREWSTER ROAD	GOODMANS HILL ROAD	CUL DE SAC	1,642	90
BRIANT DRIVE	WILLIS ROAD	CUL DE SAC	1,514	79
BRIDLE PATH	TALL PINE DRIVE	TALL PINE DRIVE	2,547	89
BROOKDALE ROAD	LANDHAM ROAD	STOCK FARM ROAD	2,348	89
BROOKS ROAD	LANDHAM ROAD	MURRAY DRIVE	920	78
BROWNSTONE LANE	OLD LANCASTER ROAD	CUL DE SAC	559	89
BUTLER ROAD	FAIRBANK ROAD	THORNBERRY LANE	732	75
CANDY HILL ROAD	CONCORD ROAD	PLYMPTON ROAD	1,472	87
CANTERBURY DRIVE	HAMPSHIRE STREET	CUL DE SAC	1,631	78
CARDING MILL ROAD	BOSTON POST ROAD	BIGELOW DRIVE	1,311	80
CARRIAGE WAY	FRENCH ROAD	CUL DE SAC LOOP	2,266	89
CENTER STREET	PRATTS MILL ROAD	CUL DE SAC	970	88
CHECKERBERRY CIRCLE	POSSUM LANE	CUL DE SAC	571	89
CHURCH STREET	CONCORD ROAD	DEAD END	549	86
CLARK LANE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	1,414	90
CLIFFORD ROAD	ALTA ROAD	WARREN ROAD	383	82
CLIFFORD ROAD	WARREN ROAD	CUL DE SAC	714	88
COBBLESTONE PLACE	GREYSTONE LANE	CUL DE SAC	500	88
CODJER LANE	CONCORD ROAD	UNION AVENUE	1,568	79
COLONIAL ROAD	OLD LANCASTER ROAD	200' W OF HOMESTEAD AVENUE	876	81
CONCORD ROAD	UNION AVENUE	GOODMANS HILL ROAD	2,379	78
CONCORD ROAD	100' N OF CONCORD ROAD (Y INT)	NEW BRIDGE ROAD	4,531	83

Street Name	From Segment	To Segment	Length (ft)	PCI
CONCORD ROAD	NEW BRIDGE ROAD	400' N OF LINCOLN ROAD	1,405	76
CONCORD ROAD	4700' S OF TOWN LINE	CONCORD TOWN LINE	4,701	80
CONFIDENCE WAY	CONCORD ROAD	DEAD END	266	83
CRAIG LANE	MARLBORO ROAD	CUL DE SAC LOOP	520	89
CRANBERRY CIRCLE	POWDER MILL ROAD	CUL DE SAC	2,711	88
CRESCENT LANE	MAYNARD ROAD	CUL DE SAC LOOP	1,523	80
CUDWORTH LANE	BRIANT DRIVE	CUL DE SAC	2,138	87
CUTLER FARM ROAD	WOODSIDE ROAD	CUL DE SAC	2,418	90
DAKIN ROAD	NORTH ROAD	CONCORD TOWN LINE	3,743	89
DARVELL DRIVE	WITHERELL DRIVE	BELCHER DRIVE	690	77
DEACON LANE	PANTRY ROAD	CUL DE SAC	852	88
DEER POND ROAD	MAYNARD FARM ROAD	MAYNARD FARM ROAD	1,195	80
DUTTON ROAD	OLD GARRISON ROAD	TANBARK ROAD	2,724	86
DUTTON ROAD	TANBARK ROAD	PRATTS MILL ROAD	2,450	86
DUTTON ROAD	PRATTS MILL ROAD	MOORE ROAD	1,972	85
EASY STREET	CUL DE SAC	CUL DE SAC	1,297	89
ELDERBERRY CIRCLE	FAIRBANK ROAD	CUL DE SAC	273	90
ELLIOT ROAD	OLD MEADOW ROAD	HORSE POND ROAD	437	89
ENGLISH ROAD	CANTERBURY DRIVE	ARROWHEAD ROAD	399	77
FAIRHAVEN CIRCLE	HUDSON ROAD	CUL DE SAC LOOP	858	78
FIELD ROAD	DAKIN ROAD	CUL DE SAC	1,038	76
FORD ROAD	GREAT ROAD	BELCHER DRIVE	2,536	84
FOX HILL DRIVE	MARY CATHERINE LANE	CUL DE SAC	965	80
FOX RUN	PEAKHAM ROAD	SADDLE RIDGE ROAD	3,015	79
FROST LANE	CONCORD ROAD	CUL DE SAC	660	85
GLEN LANE	DUTTON ROAD	DEAD END	263	89
GOODMANS HILL ROAD	CONCORD ROAD	GOODMANS HILL ROAD	3,929	78
GOODMANS HILL ROAD	GOODMANS HILL ROAD	BOSTON POST ROAD	4,360	76
GREAT LAKE DRIVE	HUDSON ROAD	BIRCHWOOD AVENUE	1,179	89
GREAT ROAD	NORTH ROAD	MAYNARD TOWN LINE	1,647	86
GREYSTONE LANE	CUL DE SAC	JULIANS WAY	850	76
GRIFFIN LANE	CROWN POINT ROAD	CUL DE SAC	518	89
GRISCOM ROAD	PELHAM ISLAND ROAD	CUL DE SAC	1,322	86
GUZZLEBROOK DRIVE	BECKWITH STREET	CUL DE SAC	905	85
HADLEY ROAD	HAYNES ROAD	CUL DE SAC	999	88
HAMBLIN LANE	MARLBORO ROAD	DEAD END	207	86
HAMPSHIRE STREET	WILLIS ROAD	CUL DE SAC	983	79
HARNESS LANE EXT	HARNESS LANE	DEAD END	252	88
HAWES ROAD	DAKIN ROAD	CUL DE SAC	1,012	80
HAWTHORNE DRIVE	PELHAM ISLAND ROAD	CUL DE SAC	556	83
HAYNES ROAD	MARLBORO ROAD	PUFFER LANE	1,659	77
HAYNES ROAD	PUFFER LANE	NORTH ROAD	2,704	77
HERMITAGE STREET	HAMPSHIRE STREET	CUL DE SAC	705	83
HILLTOP ROAD	MORSE ROAD	CUL DE SAC	920	89
HOMESTEAD STREET	PEAKHAM ROAD	COLONIAL ROAD	531	75
HOP BROOK LANE	OAK HILL ROAD	CUL DE SAC	717	89
HOPESTILL BROWN ROAD	WOODSIDE ROAD	WOODSIDE ROAD	2,345	81

Street Name	From Segment	To Segment	Length (ft)	PCI
HOPESTILL BROWN ROAD	WOODSIDE ROAD	WOODSIDE ROAD	2,345	81
HORSE POND ROAD	BOSTON POST ROAD	TALL PINE DRIVE	4,009	83
HORSE POND ROAD	TALL PINE DRIVE	PEAKHAM ROAD	2,460	83
HUDSON ROAD	100' E OF MAYNARD ROAD	OLD LANCASTER ROAD	1,638	83
HUDSON ROAD	OLD LANCASTER ROAD	TEAKETTLE LANE	2,429	83
HUDSON ROAD	TEAKETTLE LANE	100' W OF AUGUST ROAD	2,170	83
HUDSON ROAD	100' W OF AUGUST ROAD	100' W OF INTERVALE ROAD	3,618	82
HUNT ROAD	RIDGE HILL ROAD	CUL DE SAC	1,575	80
INDIAN RIDGE ROAD	KING PHILIP ROAD	OLD LANCASTER ROAD	2,892	89
INTERVALE ROAD	HUDSON ROAD	CUL DE SAC	1,270	89
IRONWORKS ROAD	TAINTOR DRIVE	CUL DE SAC LOOP	1,356	87
JACK PINE DRIVE	PRIDES CROSSING ROAD	RED OAK DRIVE	1,825	90
JASON DRIVE	LANDHAM ROAD	CUL DE SAC	634	84
JULIANS WAY	HAYNES ROAD	GREYSTONE LANE	1,497	85
JUNIPER ROAD	WAKE ROBIN ROAD	WAKE ROBIN ROAD	1,096	88
KATO DRIVE	GOODMANS HILL ROAD	CUL DE SAC	2,191	80
KATO SUMMIT	KATO DRIVE	CUL DE SAC	264	84
KAY STREET	BROOKDALE ROAD	STOCK FARM ROAD	1,544	89
KENDALL ROAD	HOBART ROAD	TANBARK ROAD	1,151	81
KENDRA LANE	WILLIS ROAD	CUDWORTH LANE	928	78
KING PHILIP ROAD	BOSTON POST ROAD	BOSTON POST ROAD	1,773	87
LANDHAM ROAD	BOSTON POST ROAD	1666' S OF BOSTON POST ROAD	1,666	80
LANDHAM ROAD	1666' S OF BOSTON POST ROAD	WOODSIDE ROAD	2,420	85
LANDHAM ROAD	WOODSIDE ROAD	FRAMINGHAM TOWN LINE	3,144	85
LANDHAM ROAD (SB Y INT)	BOSTON POST ROAD	LANDHAM ROAD	119	90
LAUREL CIRCLE	WHITE OAK LANE	CUL DE SAC	445	83
LEE ANNE CIRCLE	HUDSON ROAD	CUL DE SAC	169	76
LETTERY CIRCLE	WOODSIDE ROAD	CUL DE SAC LOOP	1,101	87
LINCOLN ROAD (EB Y INT)	LINCOLN ROAD	WAYLAND TOWN LINE	180	89
LINCOLN ROAD (WB Y INT)	CONCORD ROAD	WAYLAND TOWN LINE	292	89
LINDEN ROAD	POPLAR STREET	MAGNOLIA ROAD	758	79
MAGNOLIA ROAD	DUTTON ROAD	CUL DE SAC	936	83
MAPLEWOOD AVENUE	LAKEWOOD DRIVE	DEAD END	1,033	89
MARK LANE	MAYNARD ROAD	CUL DE SAC LOOP	661	88
MARLBORO ROAD	MAYNARD ROAD	WILLIS ROAD	2,277	86
MARLBORO ROAD	WILLIS ROAD	MORSE ROAD	2,917	79
MARY CATHERINE LANE	NORTH ROAD (W)	100' N OF NORTH ROAD (E)	848	77
MARY CATHERINE LANE	100' N OF NORTH ROAD (E)	NORTH ROAD (E)	1,017	85
MAYNARD FARM ROAD	POWERS ROAD	POWERS ROAD	4,380	78
MAYNARD ROAD	HUDSON ROAD	400' N OF HUDSON ROAD	400	84
MAYNARD ROAD	400' N OF HUDSON ROAD	WILLIS ROAD	3,442	84
MAYNARD ROAD	MARTIN DRIVE	MARLBORO ROAD	1,514	87
MCLEAN DRIVE	PRATTS MILL ROAD	CUL DE SAC	996	89
MEADOW DRIVE	PHEASANT AVENUE	CUL DE SAC	754	89
MEADOWBROOK CIRCLE	PEAKHAM ROAD	MEADOWBROOK ROAD	2,164	90
METACOMET WAY	WINSOR ROAD	CUL DE SAC	540	89
MICHAEL LANE	WIDOW RITES LANE	CUDWORTH LANE	297	89

Street Name	From Segment	To Segment	Length (ft)	PCI
MIDDLE ROAD	FIRECUT LANE	DEAD END	991	90
MILLPOND ROAD	BARTON DRIVE	CUL DE SAC	1,215	87
MINEBROOK ROAD	MAYNARD ROAD	CUL DE SAC	1,039	84
MORAN CIRCLE	RAYMOND ROAD	CUL DE SAC	1,061	77
MORSE ROAD	CONCORD ROAD	WAKE ROBIN ROAD	3,807	84
MORSE ROAD	WAKE ROBIN ROAD	MARLBORO ROAD	3,224	84
MOSSMAN ROAD	MARLBORO ROAD	LEDGE ROAD	3,492	80
MOSSMAN ROAD	LEDGE ROAD	WILLIS ROAD	1,257	80
MURRAY DRIVE	AMES ROAD	CUL DE SAC	644	83
NORTH ROAD	MAYNARD TOWN LINE	GREAT ROAD	1,401	87
NORTH ROAD	GREAT ROAD	MOSSMAN ROAD	1,809	86
NORTH ROAD	MOSSMANN ROAD	WILLARD GRANT ROAD	2,960	87
NORTH ROAD	PANTRY ROAD	WINDMILL DRIVE	1,205	80
NORTH ROAD	WINDMILL DRIVE	NORTHWOOD DRIVE	3,140	87
NORTH ROAD	NORTHWOOD DRIVE	CONCORD TOWN LINE	2,313	88
NOYES LANE	HORSE POND ROAD	MARKED TREE ROAD	636	78
OLD COACH ROAD	LEDGE ROAD	FARM LANE	2,058	89
OLD FRAMINGHAM ROAD	NOBSCOT ROAD	FRAMINGHAM TOWN LINE	2,325	76
OLD MEADOW ROAD	DEAD END	DEAD END	1,137	88
OLD SUDBURY ROAD (LOOP)	OLD SUDBURY ROAD	OLD SUDBURY ROAD	493	88
PADDOCK WAY	DAKIN ROAD	CUL DE SAC LOOP	1,001	75
PANTRY ROAD	NORTH ROAD	CONCORD ROAD	4,166	80
PEAKHAM ROAD	BOSTON POST ROAD	FRENCH ROAD	2,366	78
PEAKHAM ROAD	MARKED TREE ROAD	HORSE POND ROAD	1,986	77
PEAKHAM ROAD	HORSE POND ROAD	OLD LANCASTER ROAD	2,201	83
PEAKHAM ROAD	OLD LANCASTER ROAD	HUDSON ROAD	3,181	83
PELHAM ISLAND ROAD	LANDHAM ROAD	CUL DE SAC	3,390	89
PENDLETON ROAD	MARLBORO ROAD	CUL DE SAC	777	88
PERRY CIRCLE	ATKINSON LANE	CUL DE SAC	453	75
PHILEMON WHALE LANE	DAKIN ROAD	CUL DE SAC	982	89
PILGRIMS PATH	PURITAN LANE	CUL DE SAC	916	82
PIPSISSEWA CIRCLE	WHITE OAK LANE	CUL DE SAC	472	82
POKONOKET AVENUE	INDIAN RIDGE ROAD	OLD LANCASTER ROAD	2,753	87
PONDVIEW ROAD	SURREY LANE	CUL DE SAC	1,282	80
POPLAR STREET	HEMLOCK ROAD	PRIVATE PORTION	878	81
POSSUM LANE	NORTH ROAD	MOSSMAN ROAD	1,336	89
POWERS ROAD	NORTH ROAD	CEDAR CREEK ROAD	2,987	78
PRATTS MILL ROAD	PEAKHAM ROAD	WILLOW ROAD	2,750	85
PRIDES CROSSING ROAD	WAYSIDE INN ROAD	JACK LINE DR	3,396	88
PRIDES CROSSING ROAD	JACK PINE DRIVE	MARLBORO TOWN LINE	1,418	88
PURITAN LANE	CUL DE SAC	CUL DE SAC	1,402	79
RAYMOND ROAD	BOSTON POST ROAD	WARREN ROAD	3,997	84
RAYNOR ROAD	MOORE ROAD	DEAD END	1,207	88
READ ROAD	CUTLER FARM ROAD	CUTLER FARM ROAD	761	90
RED OAK DRIVE	CUL DE SAC	CUL DE SAC	1,104	88
REVERE STREET	LAFAYETTE DRIVE	WASHINGTON DRIVE	519	85
REVOLUTIONARY ROAD	WASHINGTON DRIVE	CUL DE SAC	693	88

Street Name	From Segment	To Segment	Length (ft)	PCI
RIDGE HILL ROAD	MORSE ROAD	HUNT ROAD	1,684	82
ROLLING LANE	JARMAN ROAD	JARMAN ROAD	1,456	81
SADDLE RIDGE ROAD	FOX RUN	CUL DE SAC	1,083	81
SAUNDERS ROAD	BOWKER DRIVE	CUL DE SAC	499	89
SCOTTS WOOD DRIVE	PRATTS MILL ROAD	CUL DE SAC	490	80
SHADOW OAK DRIVE	STOCK FARM ROAD	STOCK FARM ROAD	2,637	86
SPILLER CIRCLE	MCLEAN DRIVE	CUL DE SAC	240	85
STEARNS LANE	MOORE ROAD	CUL DE SAC	1,562	87
STONE ROAD	BOSTON POST ROAD	DOUGLAS DRIVE	1,092	87
STONE ROOT LANE	MOSSMAN ROAD	CUL DE SAC LOOP	935	90
SURREY LANE	MOORE ROAD	CUL DE SAC	2,078	83
SYCAMORE ROAD	POPLAR STREET	MAGNOLIA ROAD	885	82
TAINTOR DRIVE	400' S OF WITHERELL DRIVE	CUL DE SAC LOOP	763	78
TANTAMOUSE TRAIL	HAWES ROAD	CUL DE SAC	1,038	82
TAVERN CIRCLE	POWDER MILL ROAD	CUL DE SAC	1,100	82
THOMPSON DRIVE	CONCORD ROAD	CUL DE SAC	1,663	88
THUNDER ROAD	BALCOM ROAD	SILVER HILL ROAD	3,023	89
TRAILSIDE CIRCLE	BRIDLE PATH	CUL DE SAC	465	81
TREVOR WAY	HORSE POND ROAD	CUL DE SAC LOOP	584	88
TWIN POND LANE	CONCORD ROAD	CUL DE SAC	830	83
UPLOOK DRIVE	BOSTON POST ROAD	EASY STREET	347	89
VICTORIA ROAD	STOCK FARM ROAD	STOCK FARM ROAD	3,202	89
VILLAGE ROAD	PUFFER LANE	CUL DE SAC	1,091	89
WAKE ROBIN ROAD	MORSE ROAD	DEAD END	2,009	88
WALKER FARM ROAD	GOODMANS HILL ROAD	CUL DE SAC	962	80
WALKUP ROAD	CONCORD ROAD	CUL DE SAC	418	80
WARD ROAD	MAYNARD ROAD	CUL DE SAC	845	88
WARREN ROAD	RAYMOND ROAD	WOODSIDE ROAD	3,039	86
WASHINGTON DRIVE	CUL DE SAC	CUL DE SAC	2,037	83
WATER ROW	OLD SUDBURY ROAD	PLYMPTON ROAD	2,979	79
WAYSIDE INN ROAD	MARLBOROUGH TOWN LINE	350' E OF DUTTON ROAD	3,500	78
WAYSIDE INN ROAD	350' E OF DUTTON ROAD	1500' W OF MARLBOROUGH TOWN LI	759	77
WAYSIDE INN ROAD	350' E OF DUTTON ROAD	1500' W OF MARLBOROUGH TOWN LI	759	77
WAYSIDE INN ROAD	1500' W OF MARLBOROUGH TOWN LI	MARLBORO TOWN LINE	1,502	86
WEBSTER CIRCLE	PHILLIPS ROAD	CUL DE SAC	996	88
WHISPERING PINE ROAD	PEAKHAM ROAD	CUL DE SAC	1,124	88
WHITE OAK LANE	MOORE ROAD	CUL DE SAC	1,163	80
WHITETAIL LANE	SAWMILL LANE	CUL DE SAC	499	86
WIDOW RITES LANE	WYMAN DRIVE	CUL DE SAC	1,798	84
WILDWOOD LANE	OLD LANCASTER ROAD	CUL DE SAC	596	80
WILLIS LAKE DRIVE	HUDSON ROAD	ARBORWOOD ROAD	1,886	89
WILLIS ROAD	MAYNARD ROAD	MARLBORO ROAD	2,525	84
WILLIS ROAD	MARLBORO ROAD	BRYANT ROAD	2,721	78
WILLOW ROAD	HEMLOCK ROAD	CUL DE SAC	942	80
WILSON ROAD	PRATTS MILL ROAD	CUL DE SAC	468	87
WINDMILL DRIVE	NORTH ROAD	CUL DE SAC	1,308	88
WINTER STREET	BARTON DRIVE	CUL DE SAC	466	90

Street Name	From Segment	To Segment	Length (ft)	PCI
WITHERELL DRIVE	WILLIS ROAD	BELCHER DRIVE	2,449	87
WOODBERRY ROAD	FOREST STREET	CUL DE SAC	357	83
WOODSIDE ROAD	LANDHAM ROAD	CUTLER FARM ROAD	1,295	77
WRIGHT ROAD	ALTA ROAD	CUL DE SAC	1,005	80
WYMAN DRIVE	MAYNARD ROAD	CUDWORTH LANE	1,333	85

Appendix F: Full Depth Patching Candidates

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
AUGUST ROAD			400	79	80	85
HUDSON ROAD TO PRIVATE PORTION						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
BRIANT DRIVE			1,514	79	80	90
WILLIS ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
BRIMSTONE LANE			4,323	74	73	85
BOSTON POST ROAD TO FRAMINGHAM TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
BUTLER ROAD			732	75	73	90
FAIRBANK ROAD TO THORNBERRY LANE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
CARDING MILL ROAD			1,311	80	85	85
BOSTON POST ROAD TO BIGELOW DRIVE						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
CODJER LANE			1,568	79	75	90
CONCORD ROAD TO UNION AVENUE						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
CONCORD ROAD			2,379	78	75	85
UNION AVENUE TO GOODMANS HILL ROAD						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
NEW BRIDGE ROAD TO 400' N OF LINCOLN ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
PANTRY ROAD TO TWIN POND LANE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
TWIN POND LANE TO 4700' S OF TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
4700' S OF TOWN LINE TO CONCORD TOWN LINE						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
CRESCENT LANE			1,523	80	85	85
MAYNARD ROAD TO CUL DE SAC LOOP						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
DARVELL DRIVE			690	77	75	90
WITHERELL DRIVE TO BELCHER DRIVE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
DEER POND ROAD			1,195	80	85	85
MAYNARD FARM ROAD TO MAYNARD FARM ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
ENGLISH ROAD			399	77	78	85
CANTERBURY DRIVE TO ARROWHEAD ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
FAIRBANK ROAD			4,358	72	67	92
HUDSON ROAD TO MAYNARD ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
FAIRHAVEN CIRCLE			858	78	80	87
HUDSON ROAD TO CUL DE SAC LOOP						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
FIELD ROAD			1,038	76	75	85
DAKIN ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
FOX HILL DRIVE			965	80	78	85
MARY CATHERINE LANE TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
FRENCH ROAD			105	77	75	93
PEAKHAM ROAD TO GARRISON ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
GOODMANS HILL ROAD			3,929	78	75	85
CONCORD ROAD TO GOODMANS HILL ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
GOODMANS HILL ROAD TO BOSTON POST ROAD						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
GREYSTONE LANE			850	76	75	88
CUL DE SAC TO JULIANS WAY						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
HARVARD DRIVE			1,174	68	70	85
FORD ROAD TO LONGFELLOW ROAD						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
HEMLOCK ROAD			1,644	64	60	88
CUL DE SAC TO 300' S OF TANBARK ROAD						
Alligator Cracking	*Heavy*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
HUDSON ROAD			2,261	72	67	87
100' W OF INTERVALE ROAD TO CRYSTAL LAKE DRIVE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
HUNT ROAD			1,575	80	85	85
RIDGE HILL ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
KATO DRIVE			2,191	80	78	88
GOODMANS HILL ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
LANDHAM ROAD			1,666	80	78	90
BOSTON POST ROAD TO 1666' S OF BOSTON POST ROAD						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
LINCOLN ROAD			681	63	60	85
CONCORD ROAD TO SAWMILL LANE						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
SAWMILL LANE TO WATER ROW						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
WATER ROW TO WEIR HILL ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
WEIR HILL ROAD TO WAYLAND TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
LINDEN ROAD			758	79	80	85
POPLAR STREET TO MAGNOLIA ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
MARLBORO ROAD			2,917	79	78	85
WILLIS ROAD TO MORSE ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
MORSE ROAD TO HAYNES ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
MARY CATHERINE LANE			848	77	78	85
NORTH ROAD (W) TO 100' N OF NORTH ROAD (E)						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
MAYBURY ROAD			691	74	70	85
GREAT ROAD TO GREAT ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
MEADOWBROOK ROAD			837	69	70	90
WILLOW ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
MOSSMAN ROAD			3,492	80	78	88
MARLBORO ROAD TO LEDGE ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
LEDGE ROAD TO WILLIS ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
NEW BRIDGE ROAD			2,443	76	75	92
CONCORD ROAD TO CLARK ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
NORTH ROAD			1,205	80	78	89
PANTRY ROAD TO WINDMILL DRIVE						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
OLD FRAMINGHAM ROAD			2,325	76	75	85
NOBSCOT ROAD TO FRAMINGHAM TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
OLD GARRISON ROAD			2,966	65	63	88
FRENCH ROAD TO DUTTON ROAD						
Alligator Cracking	*Heavy*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
OLD GARRISON ROAD (Y INT)			116	73	68	89
OLD GARRISON ROAD TO PEAKHAM ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
PADDOCK WAY			1,001	75	73	90
DAKIN ROAD TO CUL DE SAC LOOP						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
PEAKHAM CIRCLE			489	69	70	93
PEAKHAM ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
PEAKHAM ROAD			2,366	78	75	85
BOSTON POST ROAD TO FRENCH ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
ROBERT BEST ROAD (N) TO MARKED TREE ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
MARKED TREE ROAD TO HORSE POND ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
PLYMPTON ROAD			1,795	80	78	88
CONCORD ROAD TO CLARK ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
CLARK ROAD TO WATER ROW						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
POWDER MILL ROAD			3,633	63	60	85
NORTH ROAD TO TAVERN CIRCLE						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
POWERS ROAD			2,987	78	75	85
NORTH ROAD TO CEDAR CREEK ROAD						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
CEDAR CREEK ROAD TO CONCORD TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
PUFFER LANE			1,685	66	65	88
NORTH ROAD TO HAYNES ROAD						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
RAYMOND ROAD			2,368	72	68	88
WARREN ROAD TO FRAMINGHAM TOWN LINE						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
SAWMILL LANE			1,012	66	65	87
LINCOLN ROAD TO DEAD END						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
STATION ROAD			1,241	65	63	90
BOSTON POST ROAD TO UNION AVENUE						
Alligator Cracking	*Heavy*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
STOCK FARM ROAD			3,595	62	58	87
VICTORIA ROAD TO LANDHAM ROAD						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
TAINTOR DRIVE			763	78	80	87
400' S OF WITHERELL DRIVE TO CUL DE SAC LOOP						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
TIPPLING ROCK ROAD			585	69	70	94
DUDLEY ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*3) 5% - 10%*				
TWILLINGATE LANE			588	74	70	88
HUDSON ROAD TO CUL DE SAC LOOP						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
Potholes & Non-Utility Pat	*Medium*	*1) 0% - 1%*				
UNION AVENUE			375	65	65	85
PHEASANT AVENUE TO CONCORD ROAD						
Alligator Cracking	*Heavy*	*2) 1% - 5%*				

Full Depth Patching Candidates

			Length (ft)	PCI	Base Index	Surface Index
WALKER FARM ROAD			962	80	85	85
GOODMANS HILL ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
WATER ROW			2,979	79	78	85
OLD SUDBURY ROAD TO PLYMPTON ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
PLYMPTON ROAD TO 3050' S OF LINCOLN ROAD						
Alligator Cracking	*Heavy*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
WAYSIDE INN ROAD			3,500	78	75	85
MARLBOROUGH TOWN LINE TO 350' E OF DUTTON ROAD						
Alligator Cracking	*Medium*	*1) 0% - 1%*				
Potholes & Non-Utility Pat	*Heavy*	*1) 0% - 1%*				
WILLIS ROAD			2,721	78	78	89
MARLBORO ROAD TO BRYANT ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
WOODSIDE ROAD			1,295	77	78	88
LANDHAM ROAD TO CUTLER FARM ROAD						
Alligator Cracking	*Medium*	*2) 1% - 5%*				
WRIGHT ROAD			1,005	80	85	85
ALTA ROAD TO CUL DE SAC						
Alligator Cracking	*Medium*	*1) 0% - 1%*				

Appendix G: Sidewalk Inventory Report

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
ABBOTTSWOOD			CAMPERDOWN LANE				To	GATE AT COUNTRY VILLAGE LANE						
264	39	1,186	1,122	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
AMANDA ROAD			DUTTON ROAD				To	CUL DE SAC						
217	50	908	929	Left	BC	Good	1	48	None	BC Ramp	BC Ramp	Low	0	53.20
219	941	1,461	637	Left	BC	Good	3	60	None	BC Ramp	No Access	Low	0	46.40
ANSELM WAY			LANDHAM ROAD				To	CUL DE SAC						
75	27	1,126	1,128	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
ARBORETUM WAY			MAYNARD ROAD				To	CUL DE SAC LOOP						
185	13	222	211	Left	BC	Excellent	0	66	BC Berm	BC Ramp	BC Ramp	Low	0	26.60
186	265	1,081	755	Left	BC	Excellent	0	66	BC Berm	BC Ramp	BC Ramp	Low	0	26.60
ATKINSON LANE			DUTTON ROAD				To	CUL DE SAC						
252	28	858	837	Left	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
255	937	1,793	854	Left	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
256	1,879	2,521	658	Left	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
BABE RUTH DRIVE			HUDSON ROAD				To	ATKINSON LANE						
257	24	1,326	1,296	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
BECKWITH STREET			PELHAM ISLAND ROAD				To	CUL DE SAC						
78	34	864	801	Left	BC	Good	15	60	None	No Access	No Access	Low	0	46.40
BIGELOW DRIVE			BOSTON POST ROAD				To	CUL DE SAC						
339	23	2,751	2,547	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
BOWKER DRIVE			FORD ROAD				To	ELSBETH ROAD						

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
102	31	511	493	Left	BC	Good	10	54	BC Berm	Intersection- No Ramp	BC Ramp	Low	0	46.40
100	453	1,211	747	Right	BC	Fair	25	54	BC Berm	BC Ramp	BC Ramp	Low	7,677	59.60
BRIANT DRIVE				WILLIS ROAD			To	CUL DE SAC						
169	10	1,334	1,312	Right	BC	Good	15	54	BC Berm	BC Ramp	No Access	Low	0	46.40
170	1,161	1,339	181	Left	BC	Fair	10	54	BC Berm	Intersection- No Ramp	No Access	Low	1,856	59.60
BROWNSTONE LANE				OLD LANCASTER ROAD			To	CUL DE SAC						
142	60	488	450	Right	BC	Good	10	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
BULKLEY ROAD				AUSTIN ROAD			To	AMANDA ROAD						
218	18	864	819	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
CAKEBREAD DRIVE				WITHERELL DRIVE			To	DEAD END						
171	563	563	204	End	BC	Good	10	66	None	No Access	BC Ramp	Low	0	39.80
CAMPERDOWN LANE				FAIRBANK ROAD			To	1000' E OF FAIRBANK ROAD						
332	30	300	945	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
CAMPERDOWN LANE EXT				1000' E OF FAIRBANK ROAD			To	CUL DE SAC LOOP						
333	0	400	1,368	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
CARDING MILL ROAD				BOSTON POST ROAD			To	BIGELOW DRIVE						
338	23	1,268	1,275	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
CARRIAGE WAY				FRENCH ROAD			To	CUL DE SAC LOOP						
212	36	636	734	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
331	710	1,209	547	Right	BC	Good	3	48	None	BC Ramp	BC Ramp	Low	0	53.20
215	1,229	2,249	793	Left	BC	Good	3	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
CIDER MILL ROAD				RAYMOND ROAD			To	RAMBLING ROAD						
43	25	528	505	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
44	602	2,275	1,661	Left	BC	Good	10	60	None	BC Ramp	Intersection- No Ramp	Low	0	46.40
COBBLESTONE PLACE				GREYSTONE LANE			To	CUL DE SAC						
26	35	427	414	Left	BC	Fair	35	54	BC Berm	BC Ramp	BC Ramp	High	4,253	79.60
CODMAN DRIVE				MORSE ROAD			To	CUL DE SAC						
128	18	1,019	970	Right	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
CONCORD ROAD				BOSTON POST ROAD			To	CONCORD TOWN LINE						
129	29	545	524	Right	BC	Good	15	60	Granite	PCC Ramp	BC Ramp	Low	0	46.40
130	591	2,896	2,318	Right	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
131	2,966	3,850	893	Right	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
283	3,917	5,204	1,279	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
282	5,214	6,854	1,640	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
281	6,912	7,720	799	Right	BC	Excellent	0	60	Sloped Granite	PCC Ramp	BC Ramp	High	0	53.20
310	7,973	7,973	54	Right	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	53.20
280	8,801	9,268	493	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	High	0	66.40
279	9,200	11,693	2,491	Left	BC	Good	1	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
278	11,727	12,672	938	Right	BC	Good	0	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
277	12,705	13,003	295	Right	BC	Good	0	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
335	13,046	13,490	460	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	High	0	66.40
317	13,887	15,312	1,451	Right	BC	Good	5	60	BC Berm	PCC Ramp	BC Ramp	High	0	66.40
CONCORD ROAD (Y INT)				OLD SUDBURY ROAD			To	CONCORD ROAD						
308	30	146	115	Right	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	53.20
309	167	266	122	Right	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	Continuous	High	0	53.20

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
CRANBERRY CIRCLE				POWDER MILL ROAD			To	CUL DE SAC						
40	56	2,599	2,558	Left	BC	Fair	40	48	BC Berm	BC Ramp	BC Ramp	Low	26,293	66.40
CRESCENT LANE				MAYNARD ROAD			To	CUL DE SAC LOOP						
319	20	1,246	1,197	Left	BC	Fair	15	54	BC Berm	BC Ramp	BC Ramp	Low	12,304	59.60
CUDWORTH LANE				BRIANT DRIVE			To	CUL DE SAC						
182	26	928	896	Right	BC	Good	10	60	BC Berm	Intersection- No Ramp	Intersection- No Ramp	Low	0	46.40
178	974	1,850	895	Right	BC	Good	20	54	BC Berm	Intersection- No Ramp	Intersection- No Ramp	Low	0	46.40
177	1,893	2,138	285	Right	BC	Good	10	54	BC Berm	No Access	BC Ramp	Low	0	46.40
CUTLER FARM ROAD				WOODSIDE ROAD			To	CUL DE SAC						
60	37	2,326	2,363	Left	BC	Good	5	48	None	BC Ramp	BC Ramp	Low	0	53.20
DAKIN ROAD				NORTH ROAD			To	CONCORD TOWN LINE						
16	1,310	2,084	784	Left	BC	Good	20	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
17	2,144	3,209	1,054	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
DUDLEY ROAD				BOSTON POST ROAD			To	NOBSCOT ROAD						
230	506	888	347	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
229	931	1,638	695	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
228	1,661	1,933	280	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
226	1,977	2,949	970	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
DUTTON ROAD				HUDSON ROAD			To	HUDSON ROAD						
216	7,783	8,938	1,176	Right	BC	Good	5	48	None	No Access	BC Ramp	Low	0	53.20
248	8,970	9,914	972	Right	BC	Good	5	48	None	BC Ramp	BC Ramp	Low	0	53.20
250	12,416	14,858	2,452	Right	BC	Good	4	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
251	14,910	15,284	374	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	High	0	66.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
ELDERBERRY CIRCLE				FAIRBANK ROAD			To	CUL DE SAC						
275	55	202	147	Left	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
FAIRBANK ROAD				HUDSON ROAD			To	MAYNARD ROAD						
327	59	897	840	Left	BC	Excellent	0	60	None	BC Ramp	BC Ramp	High	0	53.20
274	959	2,033	1,079	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	High	11,094	79.60
273	2,090	2,628	536	Left	BC	Good	5	60	BC	BC Ramp	BC Ramp	Low	0	46.40
269	2,683	3,239	555	Left	BC	Good	10	54	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
267	3,285	3,654	370	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
266	3,704	4,341	633	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
FOX HILL DRIVE				MARY CATHERINE LANE			To	CUL DE SAC						
99	31	853	873	Left	BC	Good	10	54	BC Berm	BC Ramp	No Access	Low	0	46.40
FOX RUN				PEAKHAM ROAD			To	SADDLE RIDGE ROAD						
220	46	1,165	1,157	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
221	1,279	1,314	2,394	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
FRENCH ROAD				PEAKHAM ROAD			To	DUTTON ROAD						
211	86	1,334	1,249	Left	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
FROST LANE				CONCORD ROAD			To	CUL DE SAC						
11	22	566	559	Left	BC	Fair	33	54	BC Berm	BC Ramp	BC Ramp	Low	5,740	59.60
GOODMANS HILL ROAD				CONCORD ROAD			To	BOSTON POST ROAD						
135	30	974	1,027	Left	BC	Fair	20	60	BC	BC Ramp	Continuous	High	10,558	79.60
134	1,021	3,906	2,918	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	Low	29,986	59.60
89	3,939	5,666	1,705	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
132	7,894	8,241	367	Left	BC	Good	15	48	None	BC Ramp	BC Ramp	Low	0	53.20
GREYSTONE LANE				CUL DE SAC			To	DEAD END						
27	86	812	724	Left	BC	Good	20	54	BC Berm	BC Ramp	BC Ramp	High	0	66.40
24	815	3,302	2,517	Left	BC	Fair	20	60	BC Berm	BC Ramp	BC Ramp	High	25,874	79.60
25	3,359	4,051	726	Left	BC	Good	20	54	BC Berm	BC Ramp	No Access	High	0	66.40
GUZZLEBROOK DRIVE				BECKWITH STREET			To	CUL DE SAC						
79	12	803	801	Left	BC	Good	10	60	None	No Access	No Access	Low	0	46.40
HAYNES ROAD				PANTRY ROAD			To	NORTH ROAD						
112	2,209	2,382	180	Right	BC	Good	10	60	None	BC Ramp	No Access	High	0	66.40
111	2,593	3,289	697	Right	BC	Fair	20	60	BC Berm	BC Ramp	BC Ramp	High	7,168	79.60
19	3,304	3,758	454	Left	BC	Fair	25	54	None	BC Ramp	BC Ramp	High	4,670	79.60
20	3,800	3,997	199	Left	BC	Good	10	54	None	BC Ramp	BC Ramp	High	0	66.40
21	4,283	4,589	306	Left	BC	Fair	25	54	None	BC Ramp	BC Ramp	High	3,145	79.60
22	4,624	5,098	474	Left	BC	Fair	35	48	None	BC Ramp	BC Ramp	High	4,875	86.40
28	5,134	5,177	43	Left	BC	Fair	25	60	None	BC Ramp	BC Ramp	High	444	79.60
29	5,226	5,913	711	Left	BC	Fair	35	60	None	BC Ramp	BC Ramp	High	7,305	79.60
HENRYS MILL LANE				FRENCH ROAD			To	CARRIAGE WAY						
213	21	900	845	Left	BC	Good	2	0	None	BC Ramp	BC Ramp	Low	0	53.20
330	938	1,483	547	Right	BC	Good	3	48	None	BC Ramp	BC Ramp	Low	0	53.20
HILLSIDE PLACE							To							
206	1,462	1,462	372	Left	BC	Fair	30	48	None	Continuous	BC Ramp	Low	3,828	66.40
HOPESTILL BROWN ROAD				WOODSIDE ROAD			To	WOODSIDE ROAD						
53	17	2,330	2,359	Left	BC	Fair	30	60	None	BC Ramp	BC Ramp	Low	24,245	59.60

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
HORSE POND ROAD				BOSTON POST ROAD			To	PEAKHAM ROAD						
153	39	1,265	1,223	Left	BC	Fair	25	60	None	BC Ramp	BC Ramp	Low	12,567	59.60
152	1,318	1,478	158	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
151	1,541	1,845	311	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
150	1,891	2,648	760	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
149	2,701	3,161	465	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
148	3,214	3,670	450	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
147	3,730	4,670	944	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
146	4,708	6,348	1,649	Left	BC	Good	15	48	None	BC Ramp	BC Ramp	High	0	73.20
HUDSON ROAD				CONCORD ROAD			To	STOW TOWN LINE						
313	64	438	379	Left	BC	Good	0	60	Sloped Granite	BC Ramp	PCC Compliant Ramp	High	0	66.40
318	595	820	251	Left	BC	Excellent	0	60	Sloped Granite	BC Ramp	BC Ramp	High	0	53.20
287	874	1,397	530	Left	BC	Excellent	0	60	None	BC Ramp	PCC Compliant Ramp	High	0	53.20
288	1,438	1,559	122	Left	BC	Good	0	60	None	PCC Compliant Ramp	PCC Compliant Ramp	High	0	66.40
289	1,624	1,769	138	Left	BC	Good	1	60	None	PCC Compliant Ramp	PCC Compliant Ramp	High	0	66.40
290	1,852	3,417	1,564	Left	BC	Good	4	60	None	PCC Compliant Ramp	BC Ramp	High	0	66.40
291	3,471	4,808	1,329	Left	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
292	4,896	5,260	362	Left	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
293	5,304	5,742	442	Left	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
294	5,790	7,171	1,373	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
295	7,233	7,658	426	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
296	7,707	7,934	228	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
297	7,979	8,574	600	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
298	8,629	9,742	1,123	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
299	9,833	11,289	1,519	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
300	11,361	11,538	178	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
301	11,605	11,781	176	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
302	11,824	12,692	867	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
303	12,755	13,090	343	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
304	13,129	13,920	784	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
305	13,914	14,288	388	Left	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
306	14,319	16,126	1,833	Left	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
HUNTERS RUN				MEACHEN ROAD			To	CUL DE SAC						
108	31	365	356	Left	BC	Good	5	54	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
JASON DRIVE				LANDHAM ROAD			To	CUL DE SAC						
68	26	595	572	Right	BC	Fair	20	60	None	BC Ramp	Continuous	High	5,879	79.60
204	634	634	269	End	BC	Good	5	60	None	Continuous	Continuous	High	0	66.40
JULIANS WAY				HAYNES ROAD			To	GREYSTONE LANE						
23	20	1,473	1,455	Left	BC	Good	20	54	BC Berm	BC Ramp	BC Ramp	High	0	66.40
KATO DRIVE				GOODMANS HILL ROAD			To	CUL DE SAC						
88	44	1,442	1,414	Left	BC	Fair	20	48	None	BC Ramp	No Access	Low	14,537	66.40
KENDRA LANE				WILLIS ROAD			To	CUDWORTH LANE						
179	18	891	877	Right	BC	Good	15	60	BC Berm	Intersection- No Ramp	BC Ramp	Low	0	46.40
LANDHAM ROAD				BOSTON POST ROAD			To	FRAMINGHAM TOWN LINE						
87	61	242	190	Right	BC	Good	10	60	Granite	Continuous	PCC Compliant Ramp	Low	0	46.40
86	246	289	43	Right	Concrete	Good	5	60	Granite	Continuous	Continuous	Low	0	46.40
85	293	423	128	Right	BC	Good	10	60	Granite	Continuous	Continuous	Low	0	46.40
84	438	1,356	915	Right	BC	Good	10	60	Granite	Continuous	Continuous	Low	0	46.40
83	1,364	1,575	211	Right	BC	Excellent	0	60	Granite	Continuous	Continuous	Low	0	33.20
82	1,585	2,133	554	Right	BC	Good	10	60	None	BC Ramp	Continuous	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
80	2,230	4,056	1,824	Right	BC	Fair	20	48	None	Continuous	BC Ramp	High	18,745	86.40
77	3,439	3,925	495	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
76	3,985	4,215	230	Left	BC	Good	15	48	None	BC Ramp	BC Ramp	High	0	73.20
74	4,281	4,642	360	Left	BC	Good	10	48	None	BC Ramp	BC Ramp	High	0	73.20
73	4,720	5,024	304	Left	BC	Good	10	48	None	BC Ramp	BC Ramp	High	0	73.20
69	5,128	5,454	338	Left	BC	Fair	20	48	None	BC Ramp	BC Ramp	Low	3,476	66.40
70	5,490	6,165	679	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	Low	6,981	59.60
71	6,199	6,555	360	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	Low	3,703	59.60
72	6,604	6,932	330	Left	BC	Fair	15	60	None	BC Ramp	BC Ramp	Low	3,389	59.60
LANDS END LANE				WARREN ROAD			To	ROBERT FROST ROAD						
50	46	1,468	1,447	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
LAUREL CIRCLE				WHITE OAK LANE			To	CUL DE SAC						
261	16	410	401	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
LETTERY CIRCLE				WOODSIDE ROAD			To	CUL DE SAC LOOP						
66	393	1,099	473	Left	BC	Good	5	48	None	Continuous	BC Ramp	High	0	73.20
205	834	978	158	Right	BC	Good	5	48	None	BC Ramp	Continuous	High	0	73.20
LINCOLN ROAD				CONCORD ROAD			To	WAYLAND TOWN LINE						
125	3,201	3,241	130	Left	Other	Fair	0	60	Other	No Access	No Access	Low	1,340	59.60
LINCOLN ROAD (WB Y INT)				CONCORD ROAD			To	WAYLAND TOWN LINE						
334	0	292	300	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	High	0	66.40
MARLBORO ROAD				MAYNARD ROAD			To	HAYNES ROAD						
207	2,340	2,591	231	Left	BC	Good	0	60	None	BC Ramp	Continuous	Low	0	46.40
106	2,607	3,358	749	Right	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
110	3,417	3,643	238	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
328	5,219	5,316	96	Right	BC	Good	5	54	None	BC Ramp	Continuous	Low	0	46.40
MARTIN DRIVE			MAYNARD ROAD				To	CUL DE SAC						
265	37	867	816	Left	BC	Good	10	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
MARY CATHERINE LANE			NORTH ROAD (W)				To	NORTH ROAD (E)						
98	13	1,845	1,776	Right	BC	Good	5	60	BC Berm	Intersection- No Ramp	BC Ramp	Low	0	46.40
MAYNARD ROAD			HUDSON ROAD				To	MAYNARD TOWN LINE						
320	1,064	2,539	1,469	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
321	2,596	5,083	2,487	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
322	5,145	5,250	106	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
323	5,288	6,607	1,322	Right	BC	Good	1	54	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
324	6,271	6,985	708	Left	BC	Fair	20	54	BC Berm	BC Ramp	BC Ramp	Low	7,273	59.60
325	6,691	9,241	2,541	Right	BC	Fair	15	60	BC Berm	BC Ramp	BC Ramp	Low	26,114	59.60
326	9,308	9,722	423	Right	BC	Fair	10	60	None	BC Ramp	BC Ramp	Low	4,350	59.60
MCLEAN DRIVE			PRATTS MILL ROAD				To	CUL DE SAC						
239	17	830	943	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	High	0	66.40
MEACHEN ROAD			MARLBORO ROAD				To	CUL DE SAC						
107	11	779	753	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
109	811	1,257	459	Left	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
MEADOWBROOK CIRCLE			PEAKHAM ROAD				To	MEADOWBROOK ROAD						
237	26	2,023	2,023	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	High	0	66.40
MICHAEL LANE			WIDOW RITES LANE				To	CUDWORTH LANE						

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
176	41	262	226	Left	BC	Good	10	54	BC Berm	Intersection- No Ramp	Intersection- No Ramp	Low	0	46.40
MORAN CIRCLE				RAYMOND ROAD			To	CUL DE SAC						
59	33	951	900	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
MORSE ROAD				CONCORD ROAD			To	MARLBORO ROAD						
127	38	694	654	Left	BC	Fair	25	54	None	BC Ramp	BC Ramp	High	6,717	79.60
124	736	3,811	3,096	Left	BC	Good	10	54	None	BC Ramp	BC Ramp	High	0	66.40
123	3,854	6,161	2,294	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
122	6,194	6,267	73	Left	BC	Fair	10	48	None	BC Ramp	BC Ramp	Low	754	66.40
121	6,311	7,027	706	Right	BC	Good	10	60	BC Berm	BC Ramp	Continuous	Low	0	46.40
MOSSMAN ROAD				MARLBORO ROAD			To	NORTH ROAD						
120	55	1,331	1,297	Right	BC	Good	5	54	None	BC Ramp	BC Ramp	Low	0	46.40
119	1,356	3,474	2,105	Right	BC	Fair	20	60	None	BC Ramp	BC Ramp	Low	21,636	59.60
117	3,508	4,107	599	Right	BC	Fair	25	60	None	BC Ramp	BC Ramp	Low	6,152	59.60
118	4,128	4,760	643	Left	BC	Fair	25	60	None	BC Ramp	BC Ramp	Low	6,611	59.60
116	4,790	5,257	476	Left	BC	Good	0	66	BC Berm	Continuous	BC Ramp	Low	0	39.80
115	5,261	6,586	1,315	Left	BC	Fair	20	54	None	BC Ramp	Continuous	Low	13,516	59.60
114	6,625	6,985	362	Left	BC	Fair	20	54	None	BC Ramp	BC Ramp	Low	3,721	59.60
NOBSCOT ROAD				BOSTON POST ROAD			To	FRAMINGHAM TOWN LINE						
223	80	178	91	Right	BC	Good	8	72	Granite	PCC Ramp	BC Ramp	Low	0	39.80
224	403	2,724	2,322	Left	BC	Good	3	60	BC Berm	No Access	BC Ramp	Low	0	46.40
225	2,785	3,918	1,141	Left	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
236	5,506	6,039	532	Left	BC	Good	1	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
NORTH ROAD				MAYNARD TOWN LINE			To	CONCORD TOWN LINE						
104	1,430	2,775	1,334	Right	BC	Fair	25	60	None	BC Ramp	BC Ramp	Low	13,706	59.60

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
103	1,689	1,889	200	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
105	2,838	3,176	337	Right	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
32	3,251	5,629	2,412	Right	BC	Good	10	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
33	5,623	6,116	486	Left	BC	Fair	20	54	None	BC Ramp	BC Ramp	High	4,994	79.60
30	5,730	7,604	1,879	Right	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
31	7,649	8,354	707	Right	BC	Fair	25	60	None	BC Ramp	BC Ramp	High	7,267	79.60
13	8,425	9,582	1,158	Right	BC	Excellent	0	60	BC Berm	BC Ramp	BC Ramp	Low	0	33.20
14	9,656	10,091	435	Right	BC	Excellent	1	60	None	BC Ramp	BC Ramp	Low	0	33.20
OLD FRAMINGHAM ROAD			NOBSCOT ROAD				To	FRAMINGHAM TOWN LINE						
231	608	1,415	806	Left	BC	Excellent	0	60	None	BC Ramp	BC Ramp	Low	0	33.20
232	1,413	1,497	85	Right	BC	Excellent	0	60	None	BC Ramp	BC Ramp	Low	0	33.20
233	1,540	2,242	736	Right	BC	Excellent	0	60	None	BC Ramp	BC Ramp	Low	0	33.20
OLD LANCASTER ROAD			HUDSON ROAD				To	GREEN HILL ROAD						
138	27	2,882	2,906	Right	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
141	2,971	3,633	664	Left	BC	Good	15	48	None	BC Ramp	BC Ramp	Low	0	53.20
143	3,687	6,138	2,447	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	Low	25,151	59.60
OLD SUDBURY ROAD			CONCORD ROAD				To	WAYLAND TOWN LINE						
312	12	783	752	Right	BC	Excellent	0	60	Sloped Granite	BC Ramp	PCC Compliant Ramp	High	0	53.20
311	65	201	136	Left	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	53.20
307	259	341	85	Left	BC	Good	3	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	66.40
316	411	479	71	Left	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	53.20
315	523	567	47	Left	BC	Excellent	0	60	Sloped Granite	PCC Compliant Ramp	PCC Compliant Ramp	High	0	53.20
314	603	747	147	Left	BC	Excellent	0	60	Sloped Granite	BC Ramp	PCC Compliant Ramp	High	0	53.20
PEAKHAM ROAD			BOSTON POST ROAD				To	HUDSON ROAD						
208	70	820	738	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
209	852	1,712	856	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
160	2,395	3,701	1,350	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
159	3,767	5,247	1,488	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
158	5,315	6,024	694	Left	BC	Good	15	60	BC	BC Ramp	BC Ramp	Low	0	46.40
157	6,067	6,319	261	Left	BC	Good	15	60	BC	No Access	BC Ramp	Low	0	46.40
156	6,518	7,430	911	Left	BC	Good	15	60	BC	BC Ramp	No Access	Low	0	46.40
155	7,480	9,122	1,650	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	High	0	66.40
154	9,199	10,282	1,108	Left	BC	Fair	20	60	None	BC Ramp	BC Ramp	High	11,386	79.60
337	10,373	10,932	566	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	High	0	66.40
140	10,999	11,403	402	Left	BC	Good	10	60	BC Berm	BC Ramp	BC Ramp	High	0	66.40
139	11,456	12,434	995	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
137	12,547	13,670	1,119	Left	BC	Good	15	60	None	BC Ramp	Other	High	0	66.40
136	13,728	15,682	1,941	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	High	0	66.40
PERRY CIRCLE			ATKINSON LANE				To	CUL DE SAC						
254	29	362	354	Left	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
PETERSEN CIRCLE			ATKINSON LANE				To	CUL DE SAC						
253	7	223	223	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	Low	0	46.40
PHILLIPS ROAD			FAIRBANK ROAD				To	CUL DE SAC						
270	716	1,631	909	Left	BC	Fair	20	54	BC Berm	BC Ramp	Intersection- No Ramp	Low	9,345	59.60
272	1,676	1,962	286	Left	BC	Fair	25	60	BC Berm	BC Ramp	BC Ramp	Low	2,941	59.60
PIPSISSEWA CIRCLE			WHITE OAK LANE				To	CUL DE SAC						
260	51	422	387	Left	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
POWDER MILL ROAD			NORTH ROAD				To	MAYNARD TOWN LINE						
41	22	331	340	Right	BC	Excellent	0	60	BC Berm	BC Ramp	BC Ramp	Low	0	33.20

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
34	412	922	510	Right	BC	Good	5	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
35	964	1,938	975	Right	BC	Good	10	68	BC Berm	BC Ramp	BC Ramp	Low	0	39.80
37	1,994	3,615	1,618	Right	BC	Fair	35	54	None	BC Ramp	BC Ramp	Low	16,633	59.60
39	3,668	4,423	753	Right	BC	Excellent	0	60	None	BC Ramp	BC Ramp	Low	0	33.20
PRATTS MILL ROAD			PEAKHAM ROAD				To	DUTTON ROAD						
336	50	291	252	Right	BC	Good	15	60	None	BC Ramp	BC Ramp	High	0	66.40
145	349	781	456	Right	BC	Good	10	60	None	BC Ramp	PCC Compliant Ramp	High	0	66.40
240	819	899	80	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
241	945	1,765	825	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	High	0	66.40
242	1,810	2,188	384	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
243	2,239	2,638	397	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
244	2,679	3,158	470	Right	BC	Good	0	60	None	BC Ramp	BC Ramp	High	0	66.40
245	3,176	3,907	726	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	High	0	66.40
246	3,944	4,844	913	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
247	4,893	5,484	592	Right	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
PUFFER LANE			NORTH ROAD				To	HAYNES ROAD						
18	699	1,651	948	Left	BC	Fair	33	60	None	BC Ramp	BC Ramp	High	9,744	79.60
RAMBLING ROAD			WOODSIDE ROAD				To	WARREN ROAD						
45	25	248	231	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
46	325	773	446	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
47	838	1,833	989	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
RAYMOND ROAD			BOSTON POST ROAD				To	FRAMINGHAM TOWN LINE						
90	21	672	665	Left	BC	Good	10	60	None	PCC Compliant Ramp	BC Ramp	Low	0	46.40
91	733	1,231	484	Left	BC	Fair	25	60	None	BC Ramp	BC Ramp	Low	4,973	59.60
92	1,235	1,491	252	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
93	1,493	1,698	206	Left	BC	Good	5	60	None	BC Ramp	BC Ramp	Low	0	46.40
94	1,715	3,028	1,317	Left	BC	Fair	20	60	None	BC Ramp	Continuous	Low	13,535	59.60
95	3,031	3,969	940	Left	BC	Fair	20	60	None	Continuous	BC Ramp	Low	9,664	59.60
96	4,028	4,478	453	Left	BC	Good	10	54	None	BC Ramp	BC Ramp	Low	0	46.40
97	4,513	5,450	942	Left	BC	Good	10	60	BC	BC Ramp	BC Ramp	Low	0	46.40
42	5,491	5,776	286	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
READ ROAD				CUTLER FARM ROAD			To	CUTLER FARM ROAD						
61	70	729	668	Left	BC	Good	5	48	None	BC Ramp	BC Ramp	Low	0	53.20
ROBERT FROST ROAD				CIDER MILL ROAD			To	CUL DE SAC						
51	26	411	389	Right	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
RUN BROOK CIRCLE				FAIRBANK ROAD			To	CUL DE SAC						
262	28	563	527	Left	BC	Good	10	60	BC Berm	BC Ramp	BC Ramp	Low	0	46.40
SAUNDERS ROAD				BOWKER DRIVE			To	CUL DE SAC						
101	22	418	404	Left	BC	Fair	25	54	BC Berm	Intersection- No Ramp	BC Ramp	Low	4,155	59.60
SCOTTS WOOD DRIVE				PRATTS MILL ROAD			To	CUL DE SAC						
238	30	433	405	Left	BC	Fair	12	60	None	BC Ramp	BC Ramp	High	4,159	79.60
SHADOW OAK DRIVE				STOCK FARM ROAD			To	STOCK FARM ROAD						
67	30	2,625	2,559	Right	BC	Fair	30	60	None	BC Ramp	BC Ramp	Low	26,305	59.60
SOUTHWEST CIRCLE				PEAKHAM ROAD			To	CUL DE SAC LOOP						
210	30	374	240	Left	BC	Good	2	60	None	BC Ramp	BC Ramp	Low	0	46.40
STAGECOACH DRIVE				LANDHAM ROAD			To	CUL DE SAC						

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
81	24	202	184	Left	BC	Good	15	60	None	BC Ramp	BC Ramp	Low	0	46.40
TANBARK ROAD				DUTTON ROAD			To	CUL DE SAC						
249	32	241	211	Right	BC	Good	1	60	None	BC Ramp	No Access	Low	0	46.40
TAVERN CIRCLE				POWDER MILL ROAD			To	CUL DE SAC						
38	24	1,002	1,014	Left	BC	Fair	35	54	BC Berm	BC Ramp	BC Ramp	Low	10,427	59.60
THOMPSON DRIVE				CONCORD ROAD			To	CUL DE SAC						
162	15	473	459	Left	BC	Fair	30	48	None	BC Ramp	BC Ramp	High	4,712	86.40
163	513	1,556	1,077	Left	BC	Fair	20	54	BC Berm	BC Ramp	BC Ramp	High	11,064	79.60
TWIN MEADOW LANE				FAIRBANK ROAD			To	CUL DE SAC						
268	13	269	260	Left	BC	Fair	20	60	BC Berm	BC Ramp	BC Ramp	Low	2,675	59.60
TWIN POND LANE				CONCORD ROAD			To	CUL DE SAC						
12	46	729	686	Left	BC	Fair	33	54	BC Berm	BC Ramp	BC Ramp	Low	7,047	59.60
UNION AVENUE				BOSTON POST ROAD			To	CONCORD ROAD						
286	46	146	103	Right	BC	Excellent	0	60	Granite	PCC Compliant Ramp	PCC Compliant Ramp	Low	0	33.20
285	168	276	108	Right	BC	Excellent	0	60	Granite	PCC Compliant Ramp	PCC Compliant Ramp	Low	0	33.20
284	4,768	5,113	345	Left	BC	Good	0	60	Sloped Granite	BC Ramp	BC Ramp	Low	0	46.40
VIRGINIA RIDGE ROAD				POWDER MILL ROAD			To	CUL DE SAC LOOP						
36	21	1,477	1,449	Left	BC	Fair	25	60	BC Berm	BC Ramp	BC Ramp	Low	14,893	59.60
WADSWORTH ROAD				HAYNES ROAD			To	CUL DE SAC						
113	33	754	719	Left	BC	Fair	25	54	BC Berm	BC Ramp	BC Ramp	High	7,388	79.60
WALKER FARM ROAD				GOODMANS HILL ROAD			To	CUL DE SAC						

Sidewalk Inventory Report

ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
133	26	894	890	Left	BC	Good	15	48	None	BC Ramp	BC Ramp	Low	0	53.20
WARREN ROAD				RAYMOND ROAD			To	WOODSIDE ROAD						
52	21	524	499	Right	BC	Fair	20	60	None	Continuous	BC Ramp	High	5,127	79.60
49	547	1,275	722	Right	BC	Good	5	60	None	BC Ramp	Continuous	High	0	66.40
48	1,336	1,735	419	Right	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
58	1,819	2,329	496	Right	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
57	2,403	3,003	599	Right	BC	Good	5	60	None	BC Ramp	BC Ramp	High	0	66.40
WEBSTER CIRCLE				PHILLIPS ROAD			To	CUL DE SAC						
271	40	850	803	Left	BC	Fair	20	60	BC Berm	Intersection- No Ramp	BC Ramp	Low	8,250	59.60
WHITE OAK LANE				MOORE ROAD			To	CUL DE SAC						
258	31	446	425	Right	BC	Good	3	60	None	BC Ramp	BC Ramp	Low	0	46.40
259	482	1,070	560	Right	BC	Good	1	60	None	BC Ramp	BC Ramp	Low	0	46.40
WHITETAIL LANE				SAWMILL LANE			To	CUL DE SAC						
126	14	397	413	Right	BC	Fair	25	54	BC Berm	BC Ramp	BC Ramp	High	4,241	79.60
WIDOW RITES LANE				WYMAN DRIVE			To	CUL DE SAC						
173	10	75	70	Right	BC	Fair	20	54	Sloped Granite	Intersection- No Ramp	No Access	Low	716	59.60
174	32	1,510	1,475	Left	BC	Fair	20	54	BC Berm	Intersection- No Ramp	Intersection- No Ramp	Low	15,157	59.60
175	1,561	1,798	274	Left	BC	Fair	30	60	BC Berm	No Access	BC Ramp	Low	2,814	59.60
WILLIS ROAD				MAYNARD ROAD			To	MOSSMAN ROAD						
184	2,757	3,996	1,234	Right	BC	Excellent	0	66	None	Continuous	BC Ramp	Low	0	26.60
183	3,996	4,637	636	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
168	4,692	5,214	528	Left	BC	Good	10	66	BC Berm	BC Ramp	BC Ramp	Low	0	39.80
166	5,273	5,459	186	Left	BC	Good	5	84	Sloped Granite	Continuous	BC Ramp	Low	0	39.80

Sidewalk Inventory Report

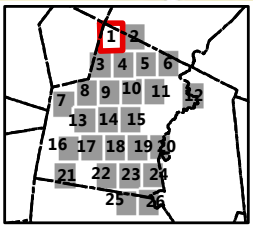
ID	From Station	To Station	Length (ft)	Side	Material	Condition	Repair Percentage	Avg. Width (in)	Curb Type	Start Terminus	End Terminus	Location Priority	Estimated Cost	Priority
167	5,463	6,025	563	Left	BC	Good	10	60	None	BC Ramp	Continuous	Low	0	46.40
165	6,089	6,974	902	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
164	7,025	7,738	735	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	Low	0	46.40
WOODSIDE ROAD				HOPESTILL BROWN ROAD (S)			To	CUTLER FARM ROAD						
54	139	1,344	1,192	Left	BC	Good	15	60	None	Continuous	BC Ramp	Low	0	46.40
56	1,344	1,888	551	Left	BC	Good	15	60	None	BC Ramp	Continuous	High	0	66.40
55	1,968	2,472	512	Left	BC	Good	10	60	None	BC Ramp	BC Ramp	High	0	66.40
62	2,539	2,905	376	Left	BC	Good	10	60	None	BC Ramp	PCC Ramp	High	0	66.40
63	2,989	3,231	237	Left	BC	Good	5	60	None	BC Ramp	PCC Ramp	High	0	66.40
64	3,293	3,527	235	Left	BC	Good	10	60	None	PCC Ramp	BC Ramp	High	0	66.40
65	3,618	4,841	1,226	Left	BC	Fair	25	60	None	BC Ramp	Continuous	High	12,599	79.60
WYMAN DRIVE				MAYNARD ROAD			To	CUDWORTH LANE						
172	26	439	407	Right	BC	Fair	20	60	BC Berm	BC Ramp	Intersection- No Ramp	Low	4,184	59.60
181	515	1,298	793	Right	BC	Good	10	60	BC Berm	Intersection- No Ramp	Intersection- No Ramp	Low	0	46.40
180	1,244	1,310	65	Left	BC	Fair	25	72	BC Berm	Intersection- No Ramp	No Access	Low	669	53.00

Appendix H: Sidewalk Inventory Maps

0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



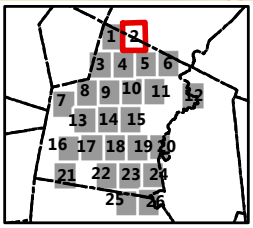
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

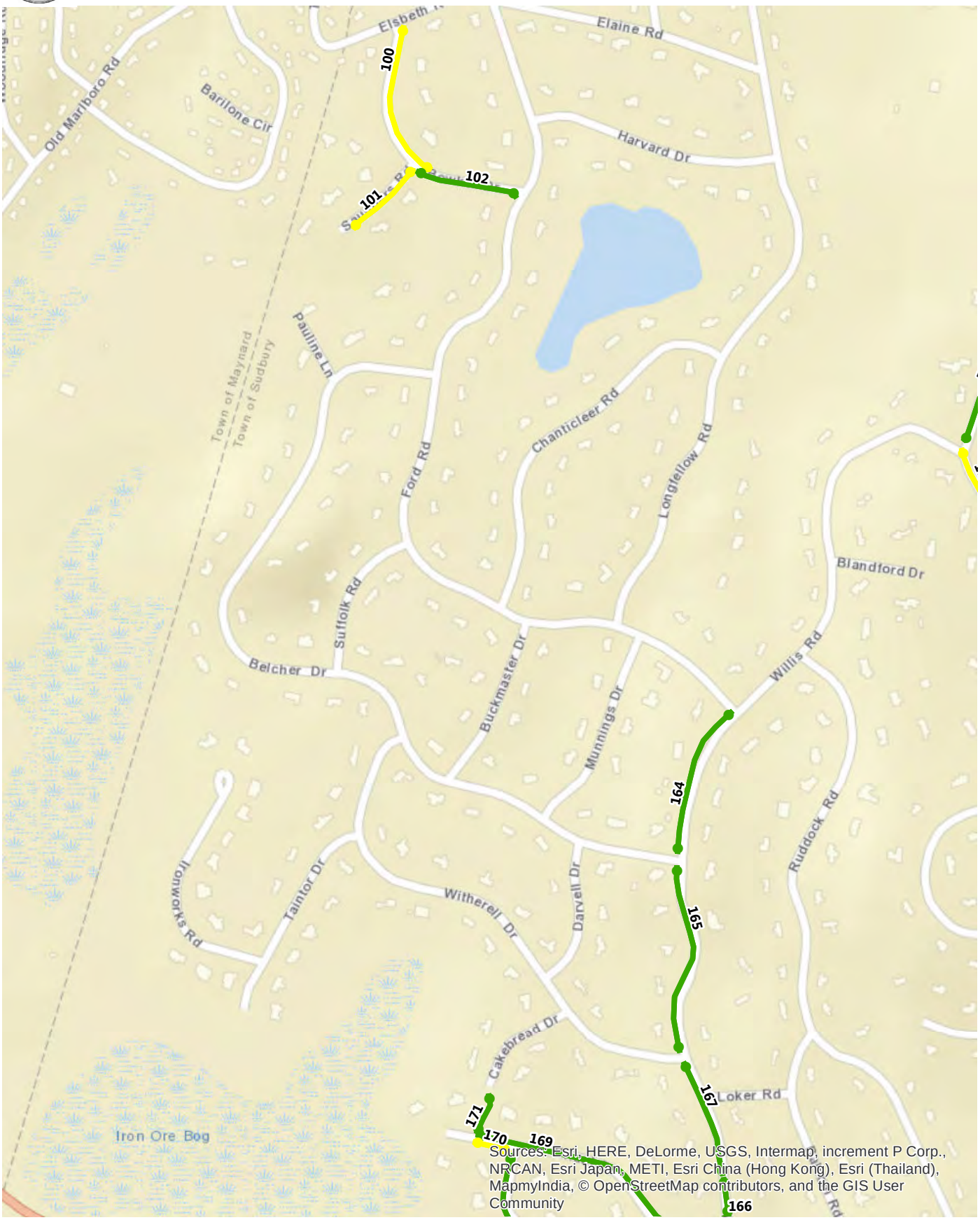
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

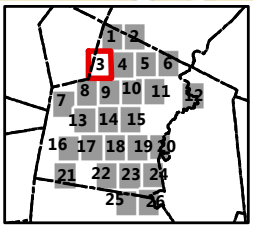
Sidewalk Condition Map



0 125 250 500 Feet

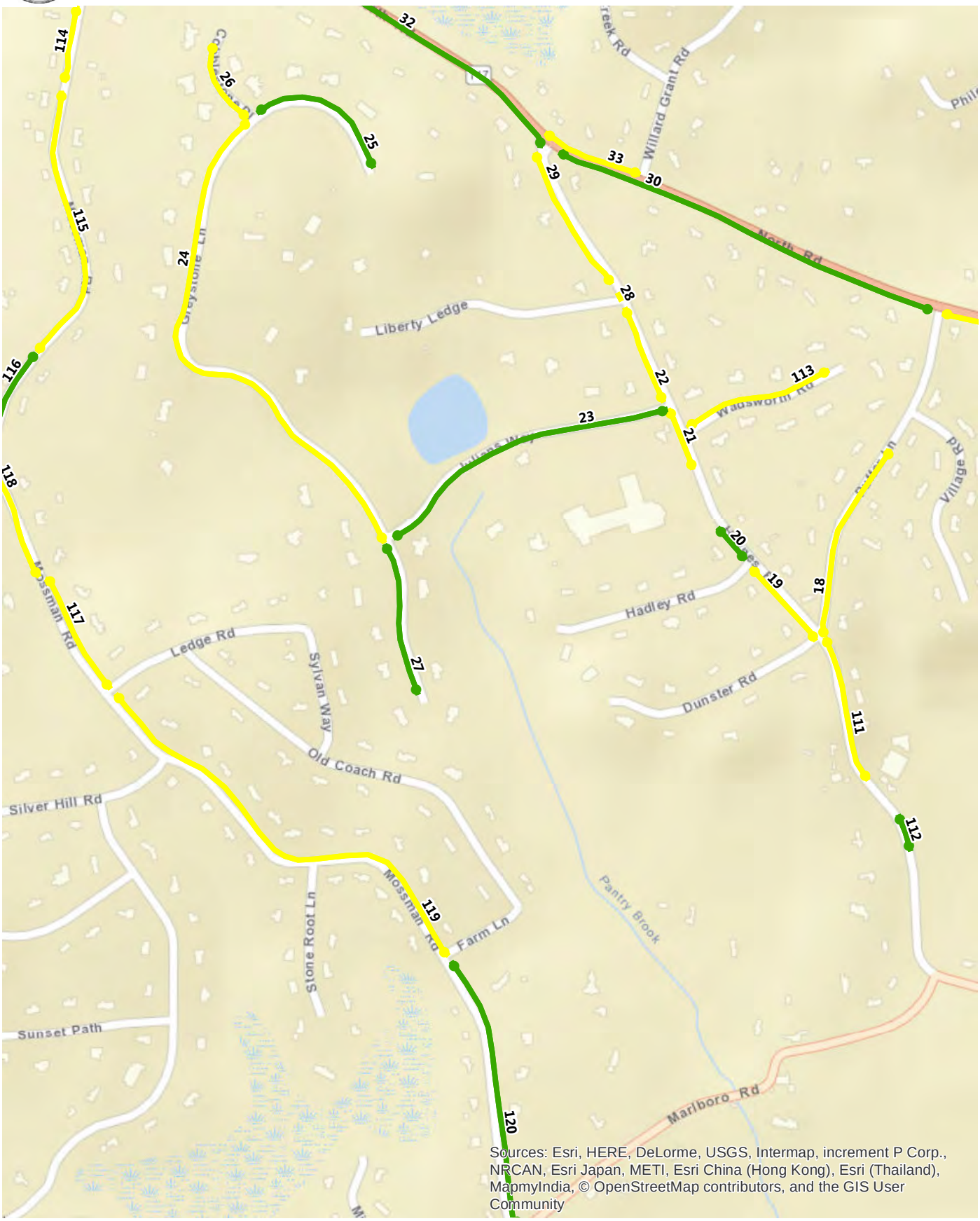
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

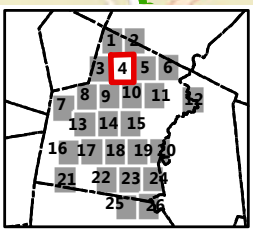
Sidewalk Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



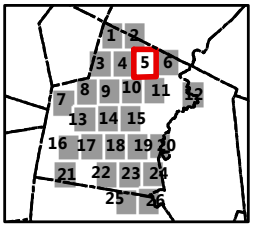
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



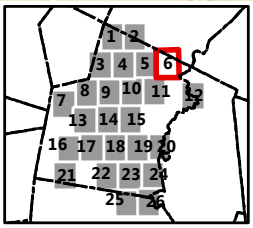
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

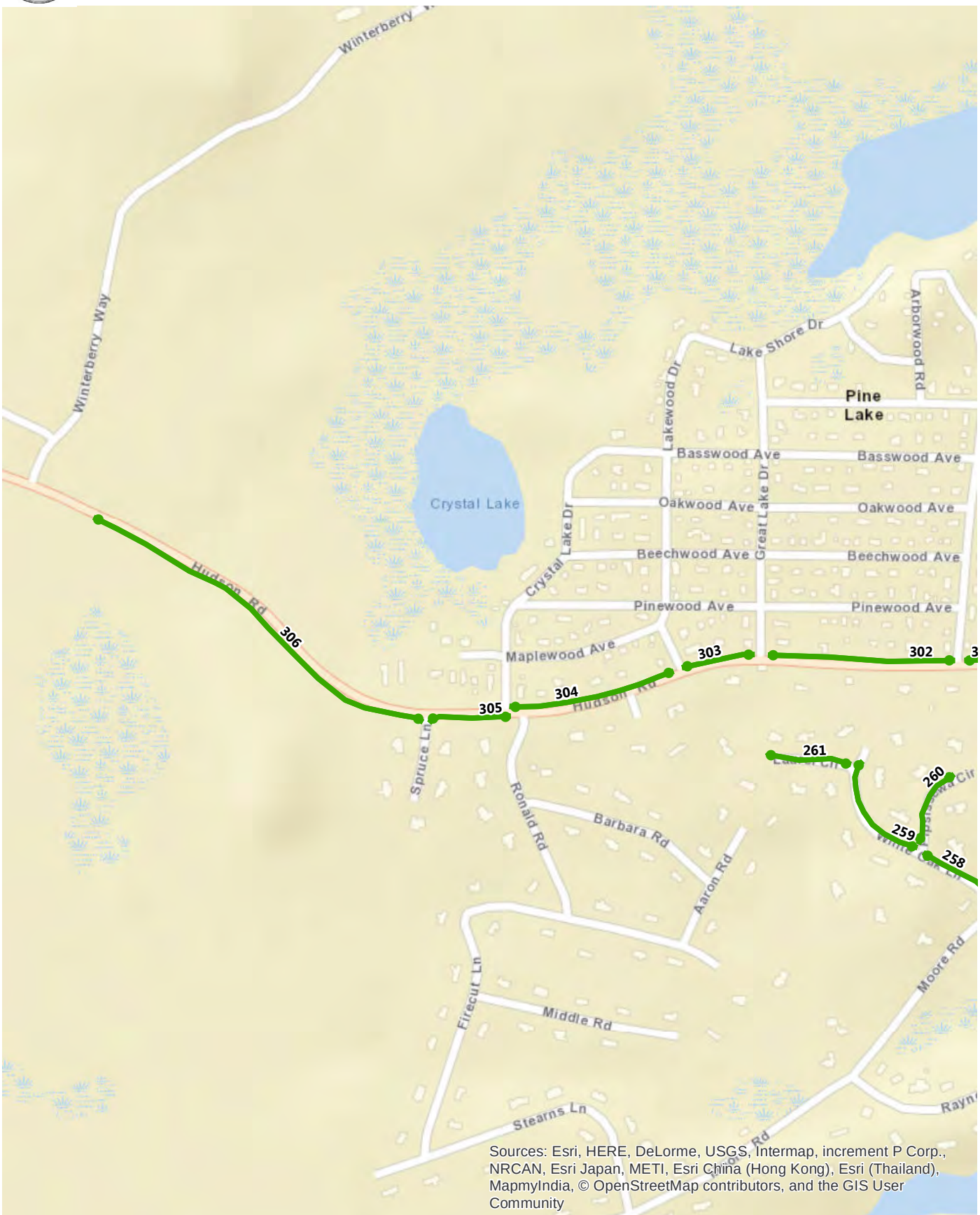
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

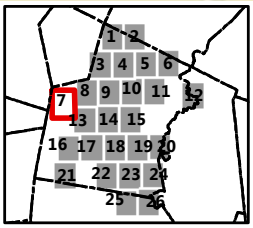
Sidewalk Condition Map



0 125 250 500 Feet

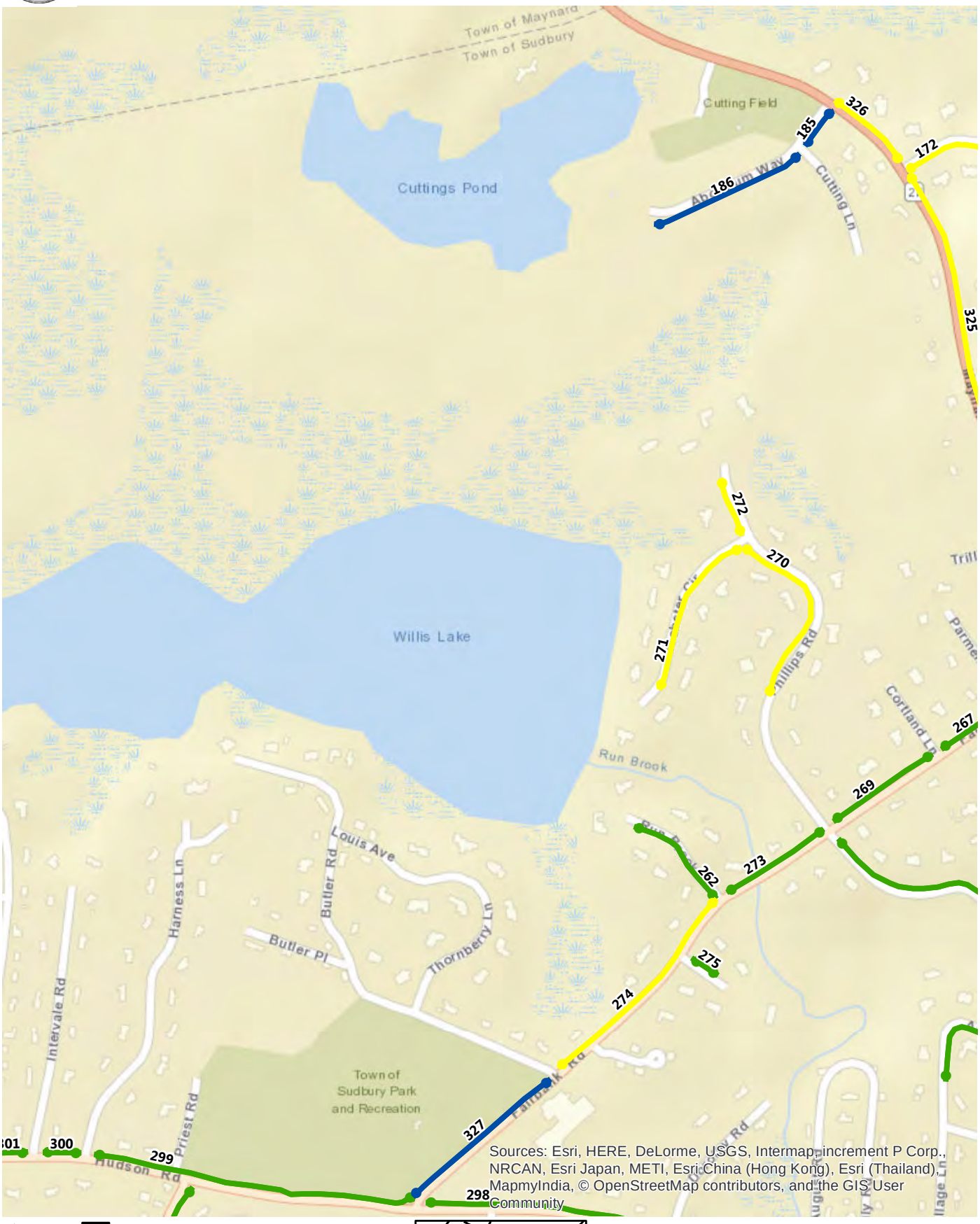
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

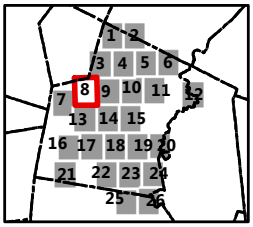
Sidewalk Condition Map



0 125 250 500 Feet

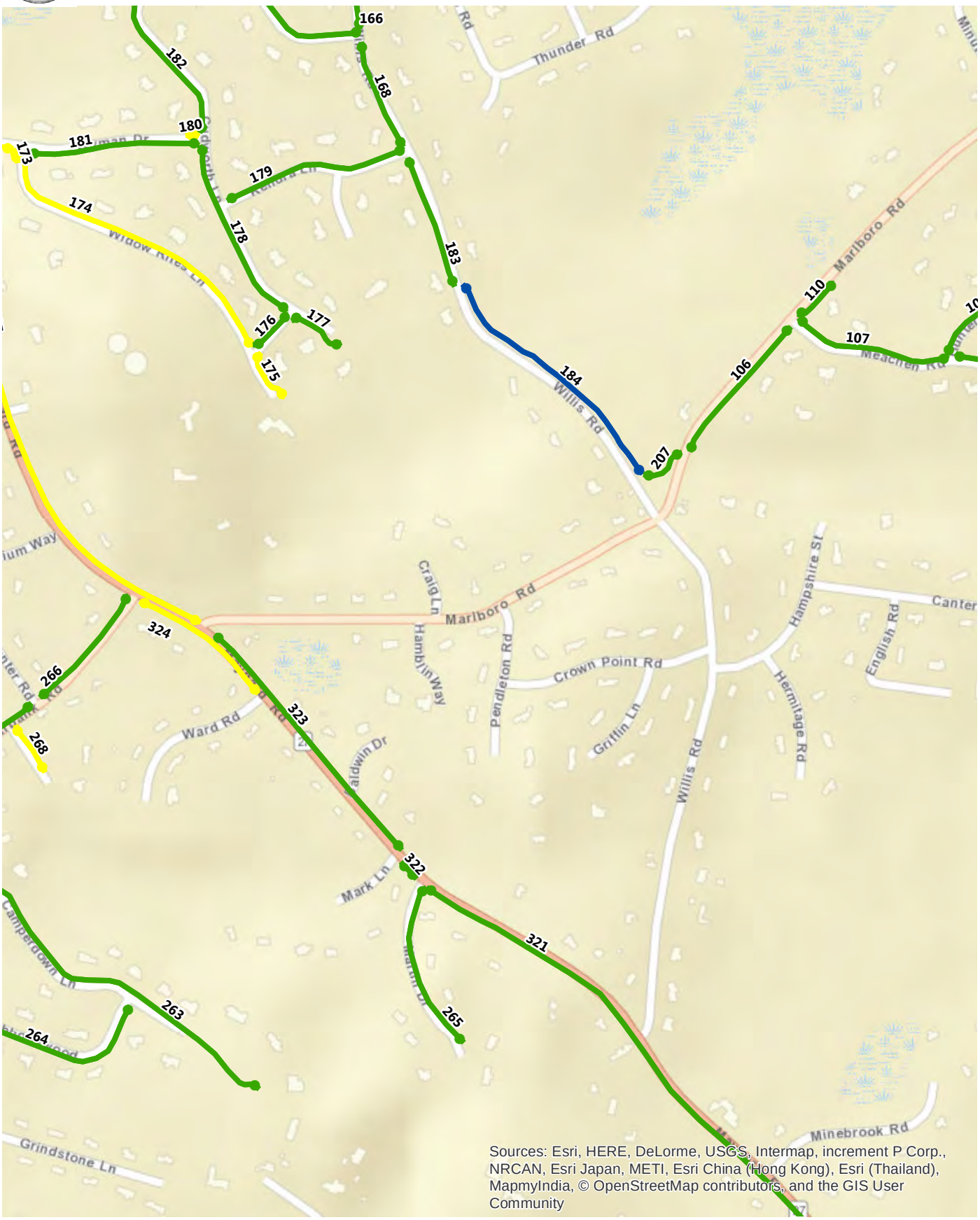
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

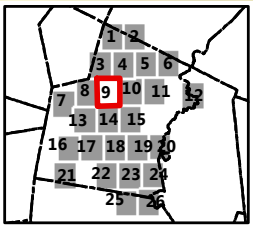
Sidewalk Condition Map



0 125 250 500 Feet

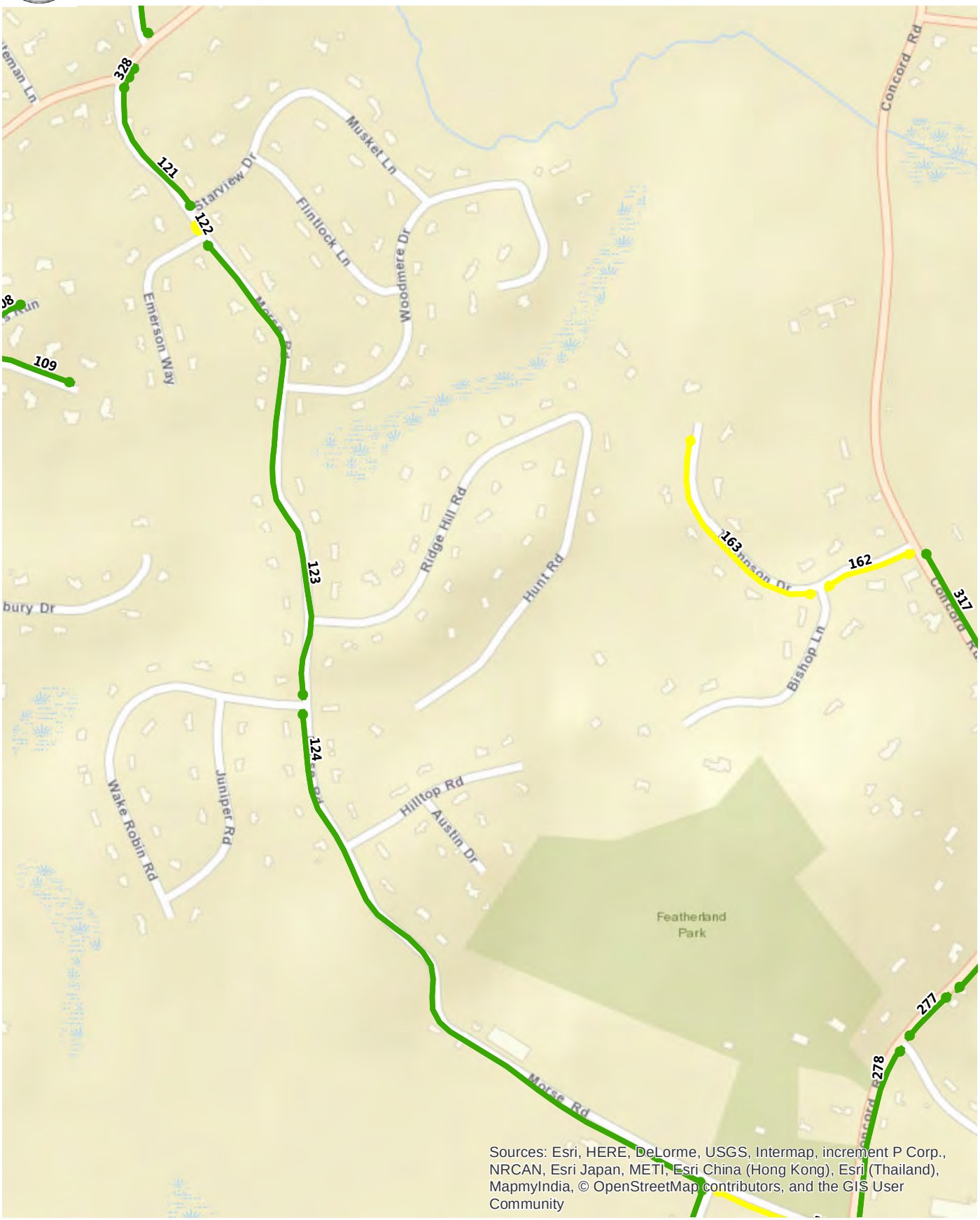
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

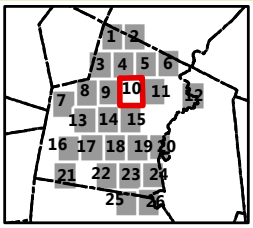
Sidewalk Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

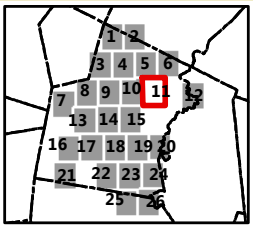
Sidewalk Condition Map

Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



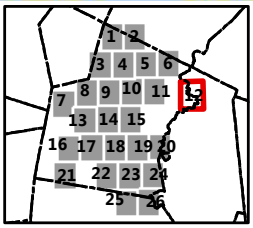
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

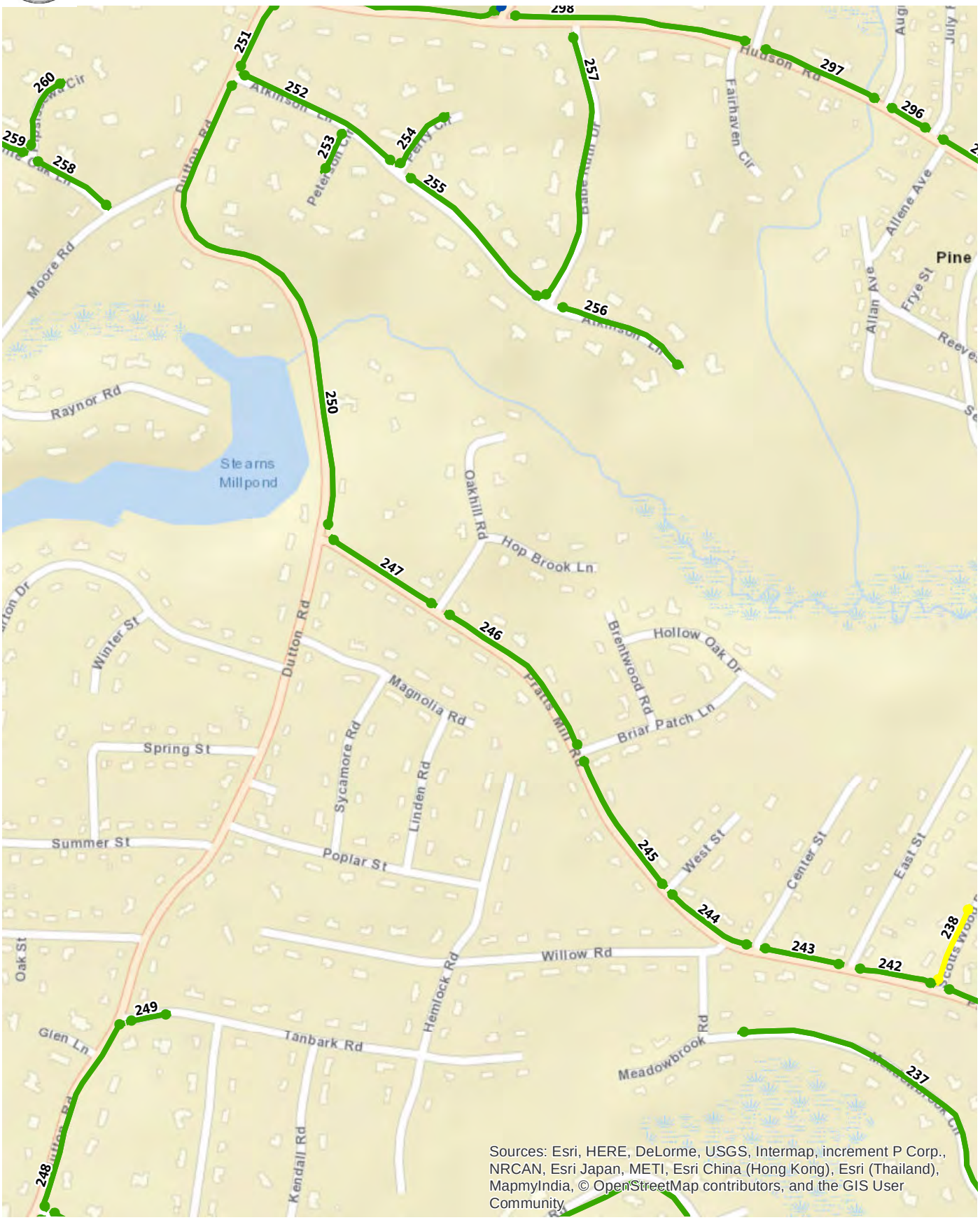
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

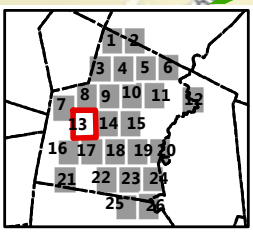
Sidewalk Condition Map



0 125 250 500 Feet

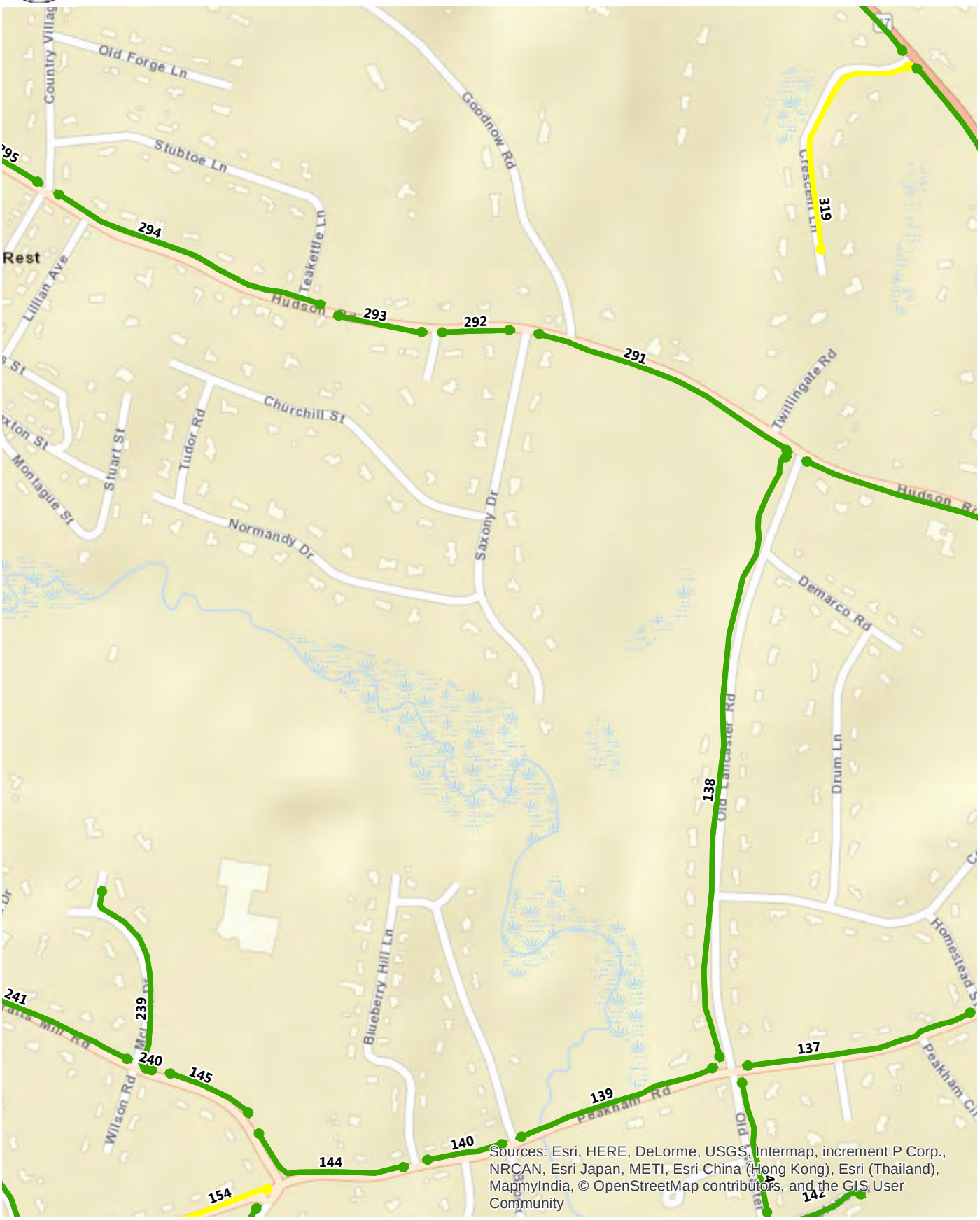
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

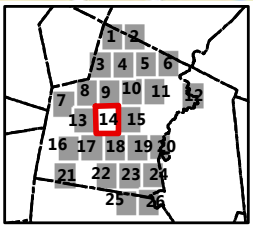
Sidewalk Condition Map



0 125 250 500 Feet

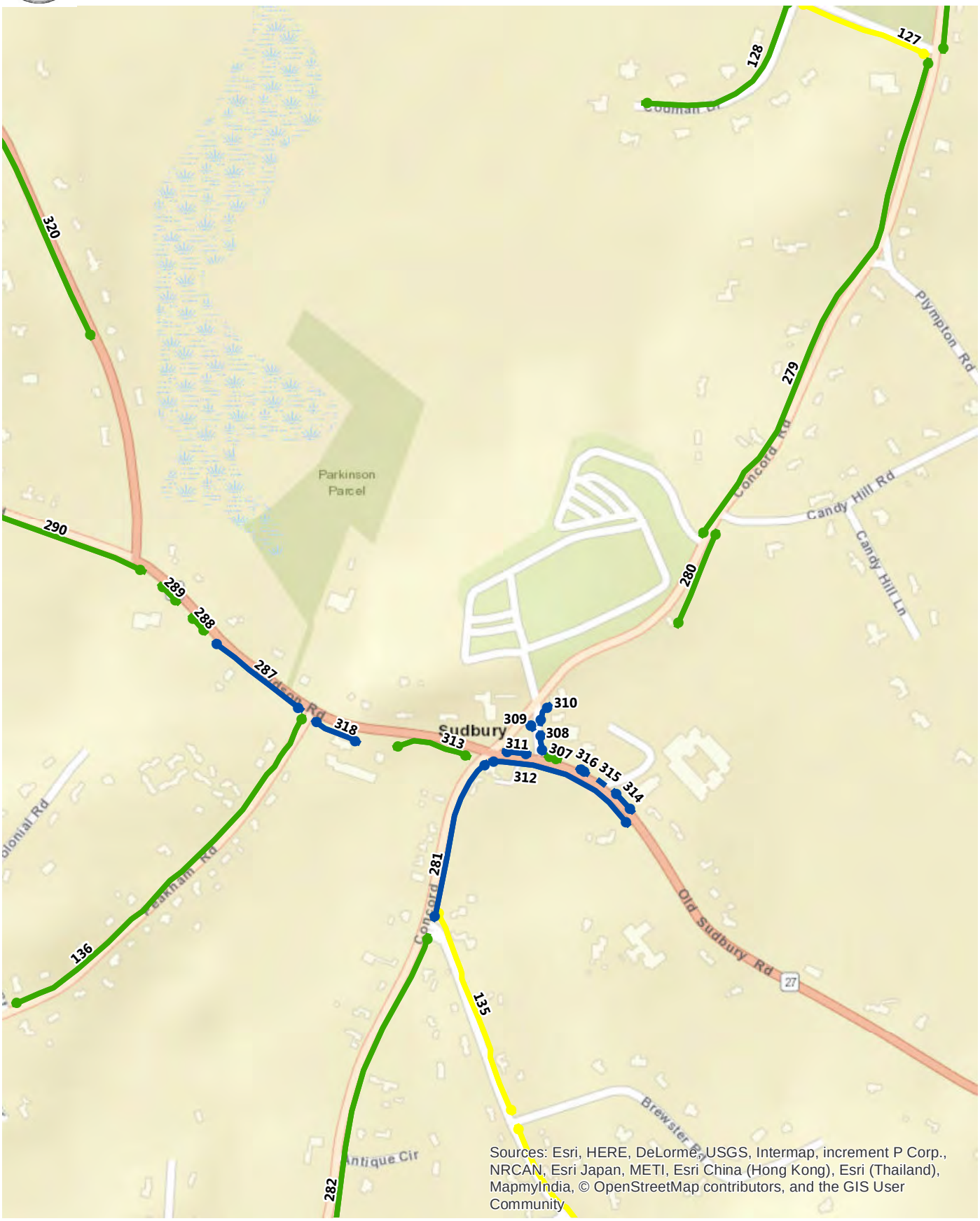
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

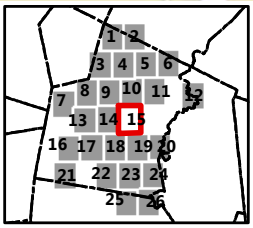
Sidewalk Condition Map



0 125 250 500 Feet

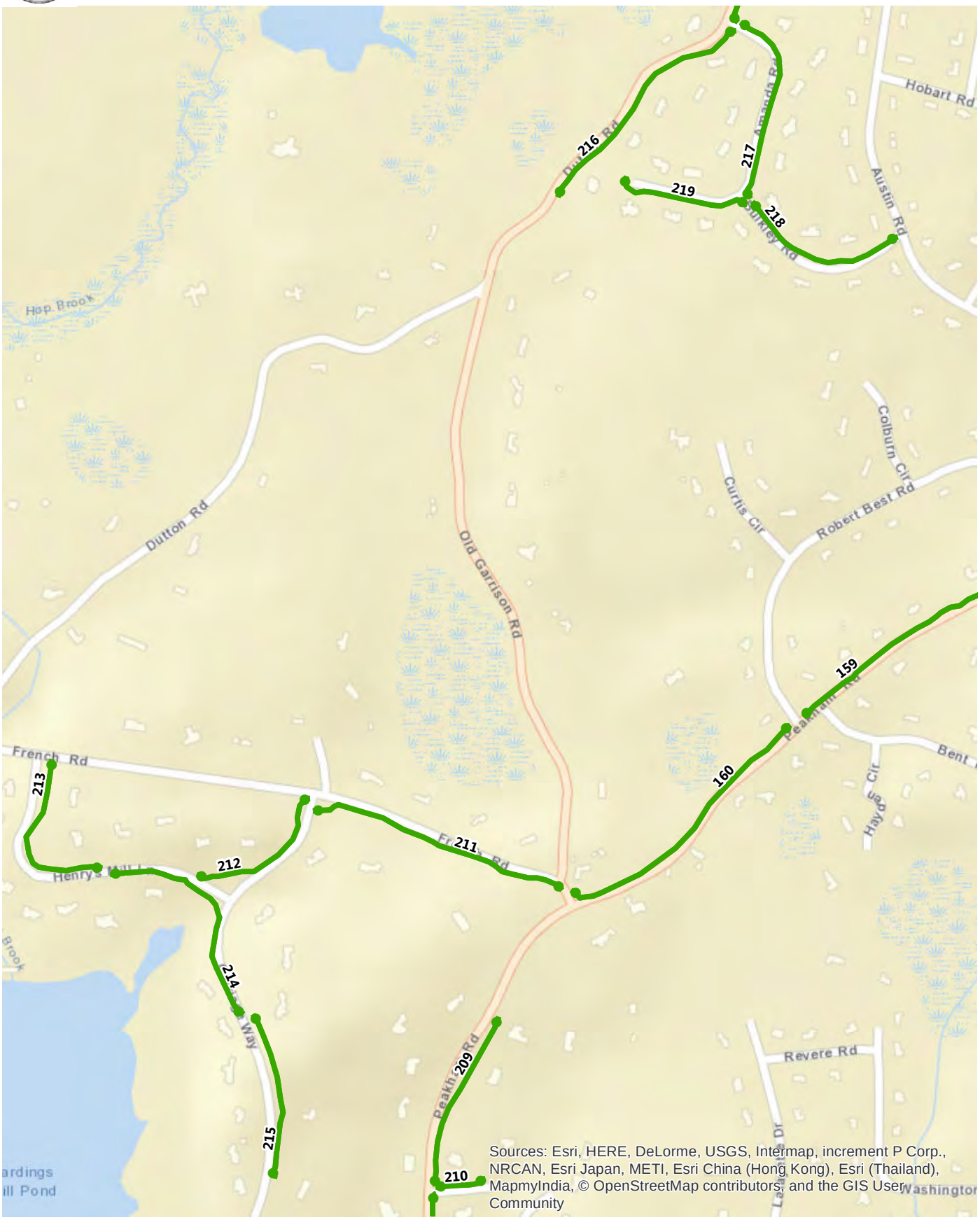
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

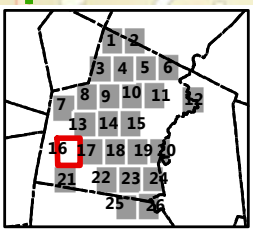
Sidewalk Condition Map



0 125 250 500 Feet

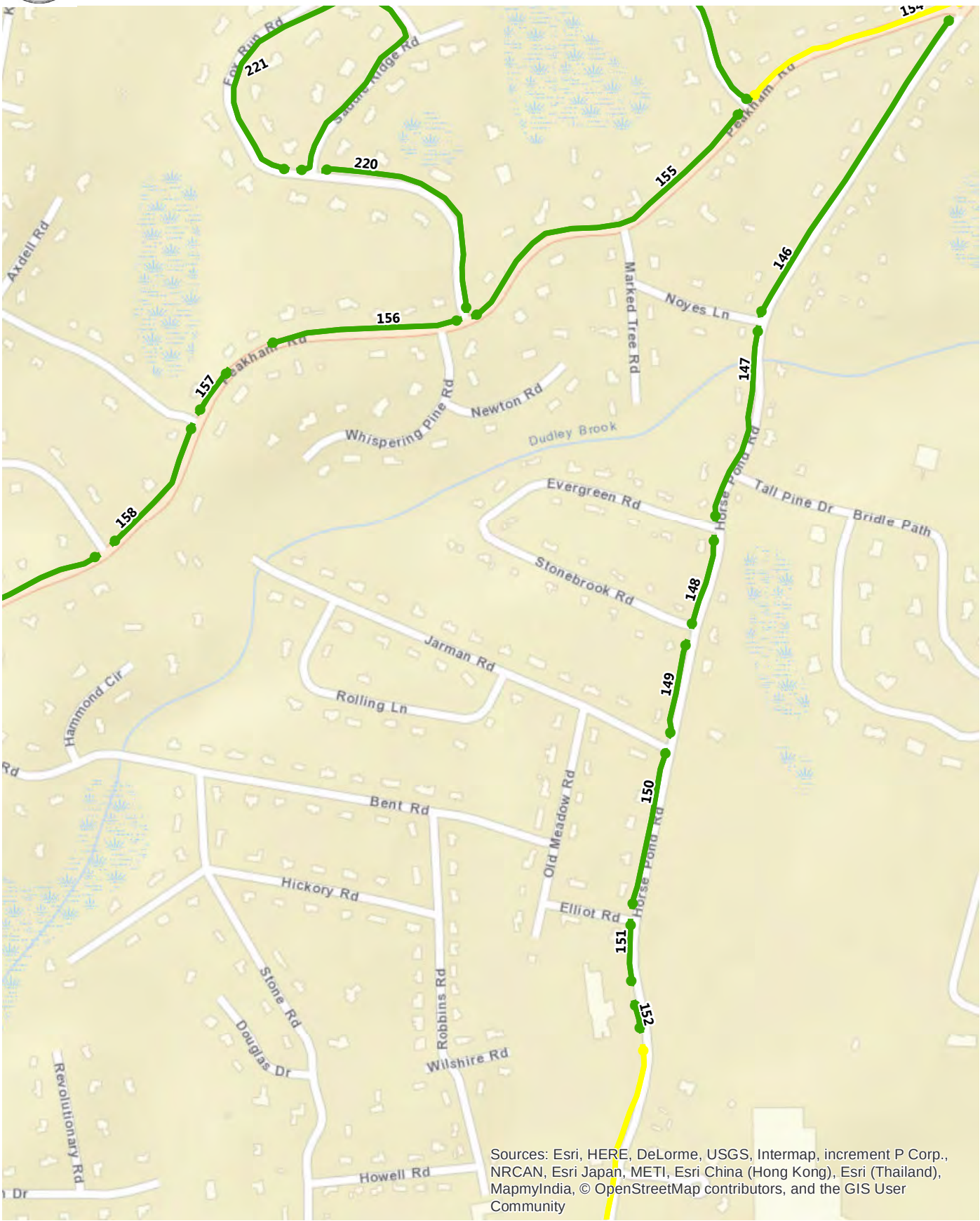
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

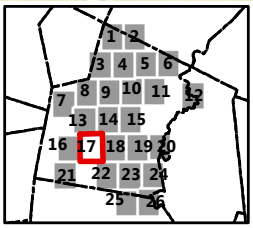
Sidewalk Condition Map



0 125 250 500 Feet

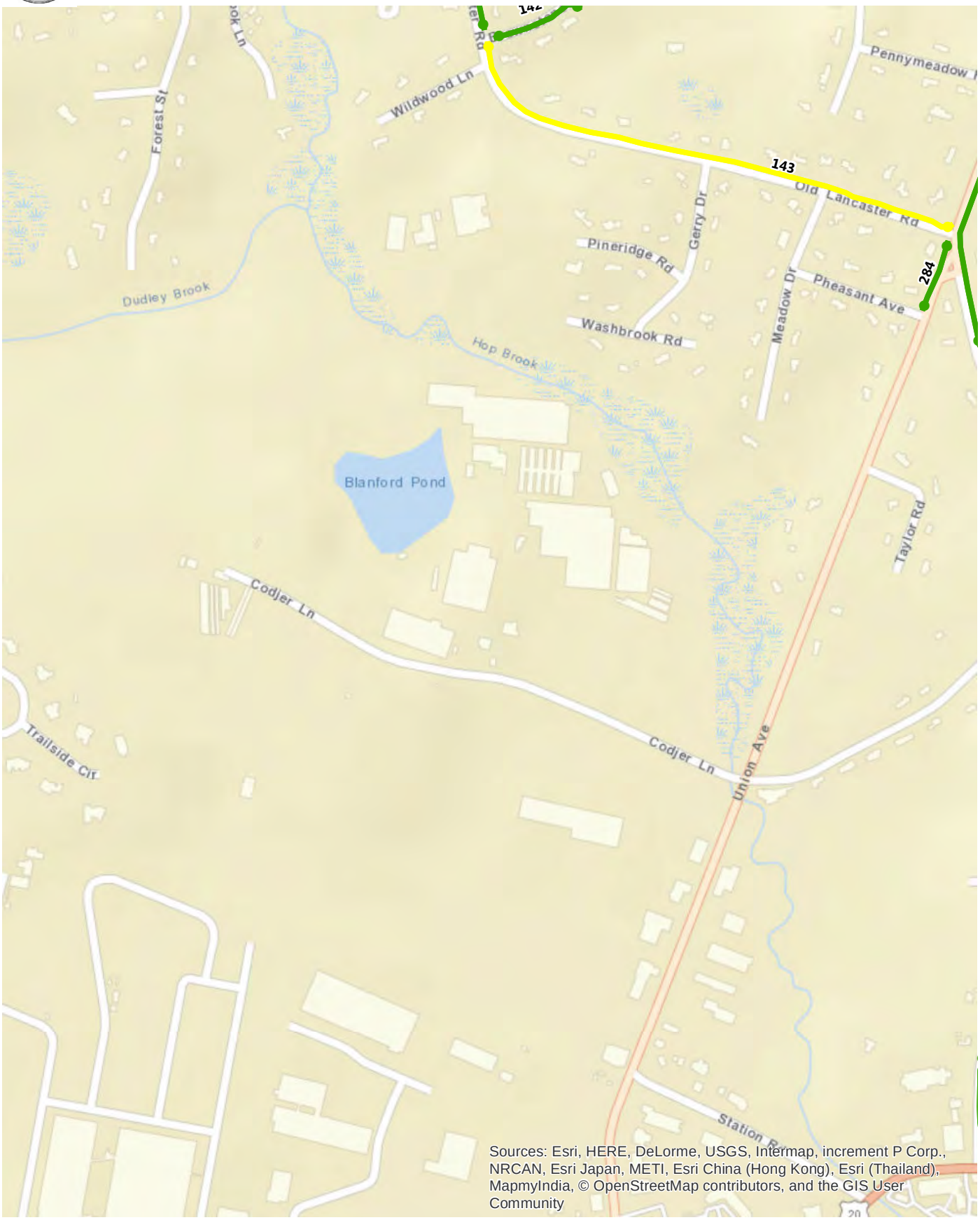
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

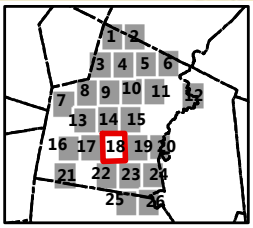
Sidewalk Condition Map



0 125 250 500 Feet

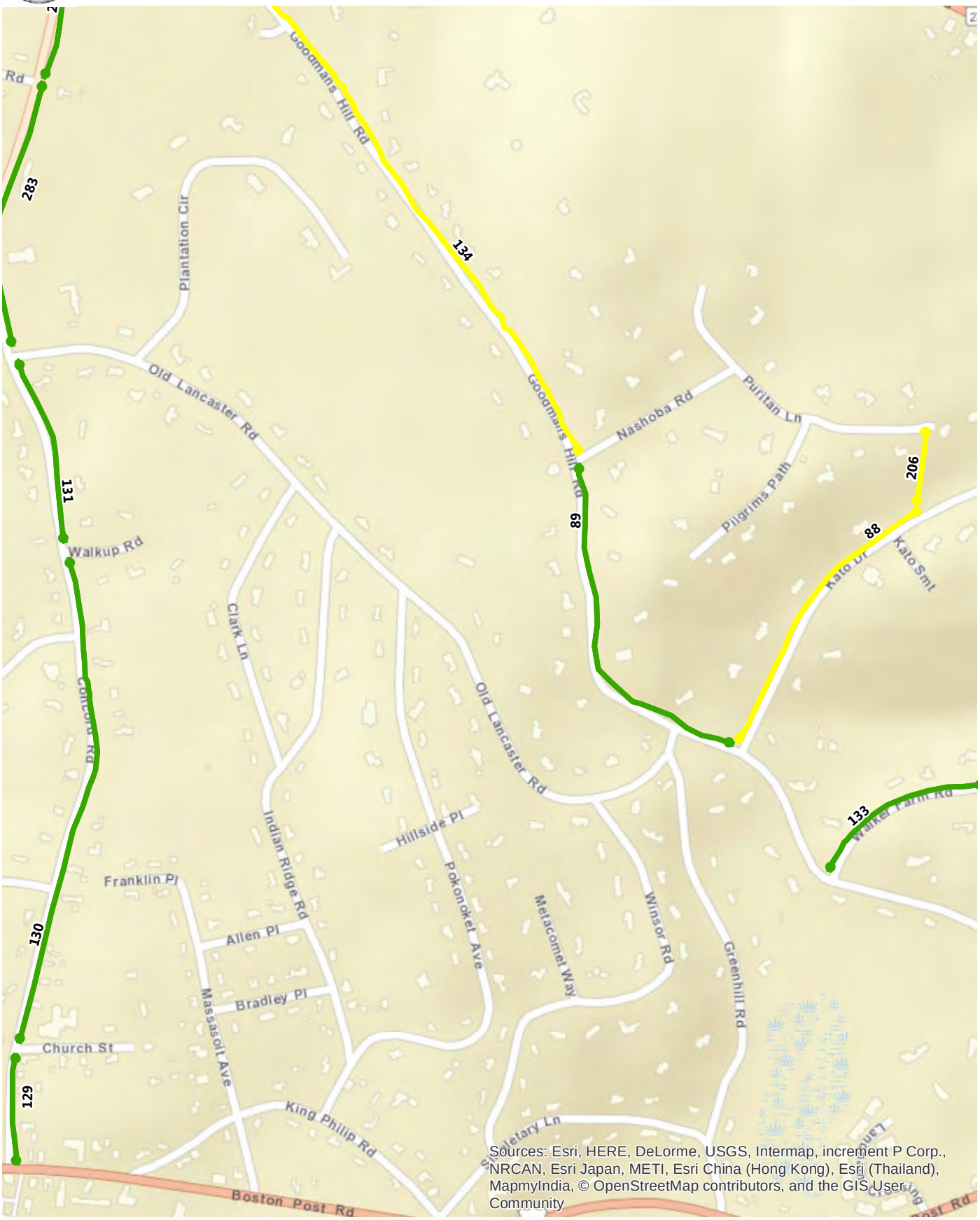
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

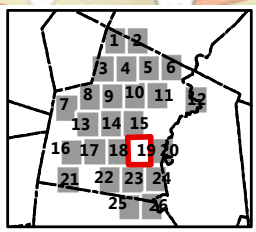
Sidewalk Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



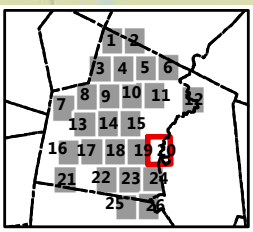
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

Sidewalk Condition Map

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0 125 250 500 Feet

Condition

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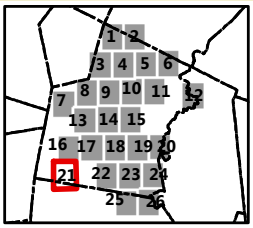
- Good
- Fair



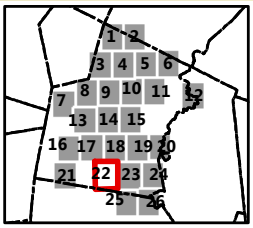
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

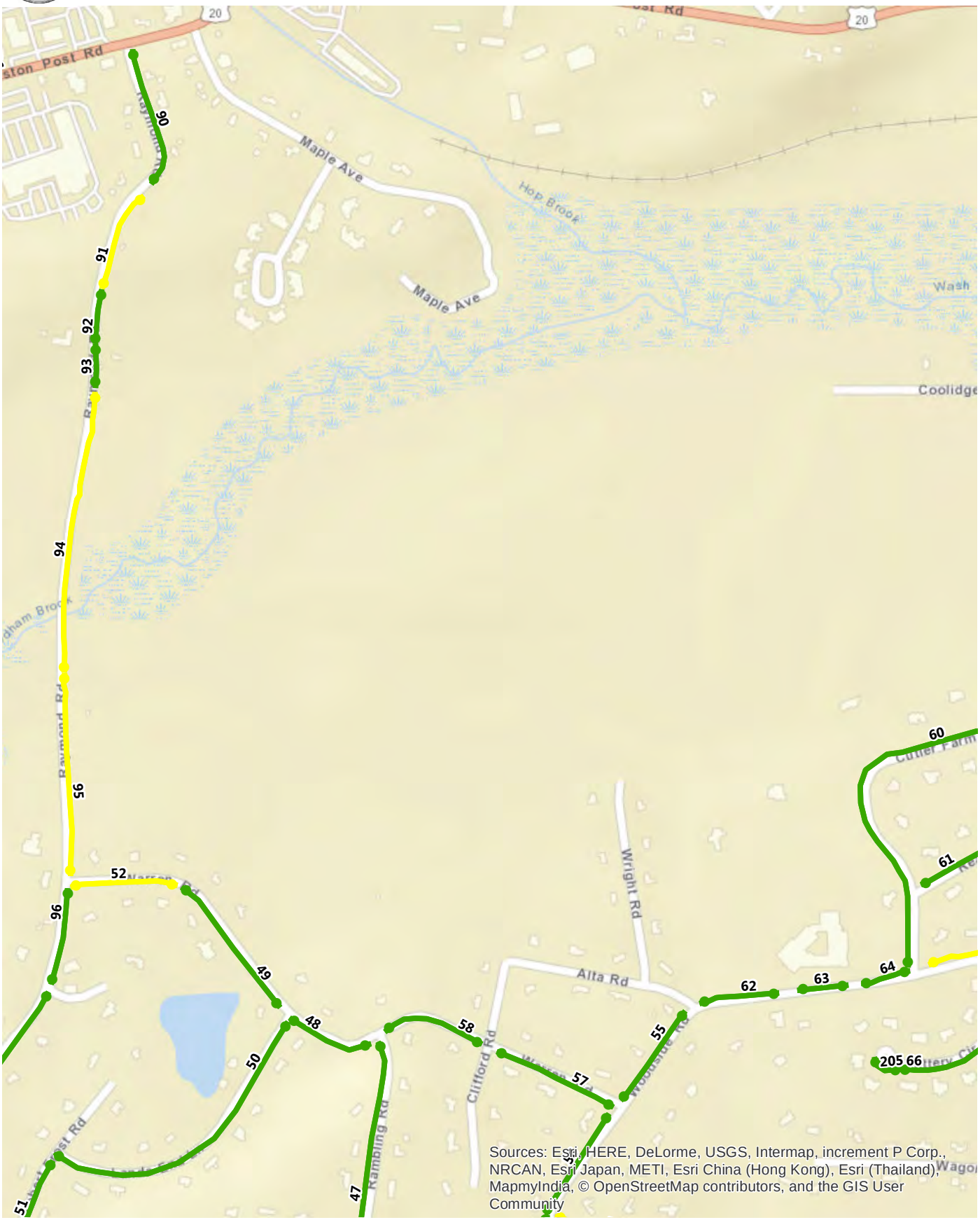
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

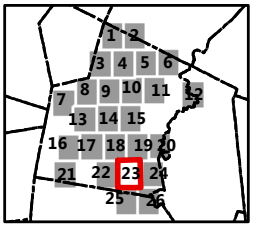
Sidewalk Condition Map



0 125 250 500 Feet

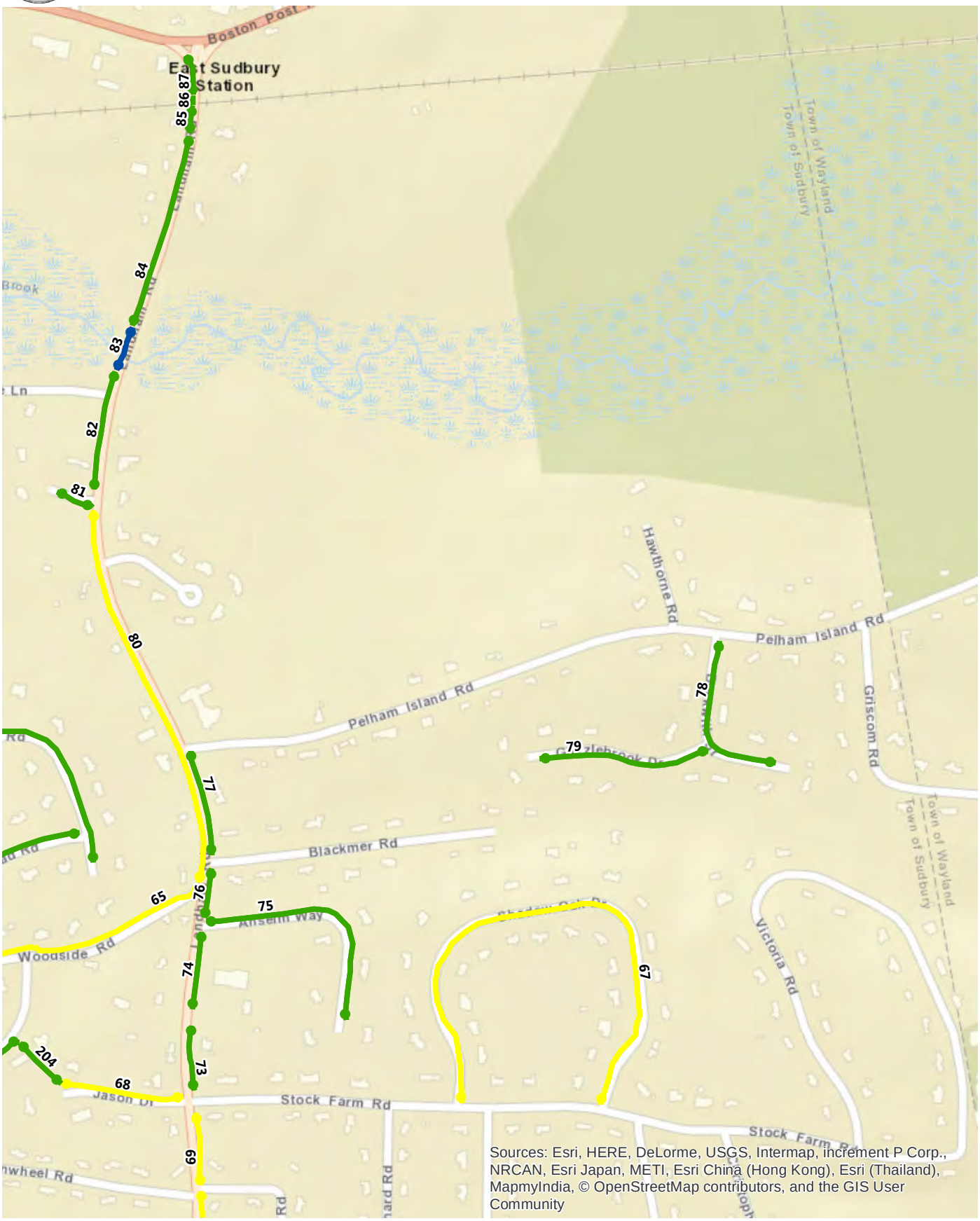
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

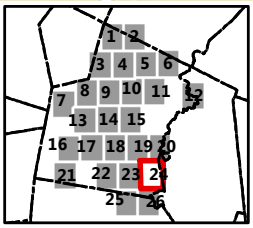
Sidewalk Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



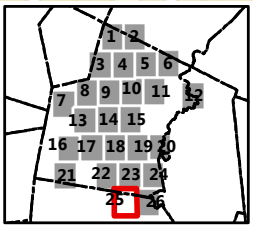
Sudbury, Massachusetts

Sidewalk Condition Map

0 125 250 500 Feet

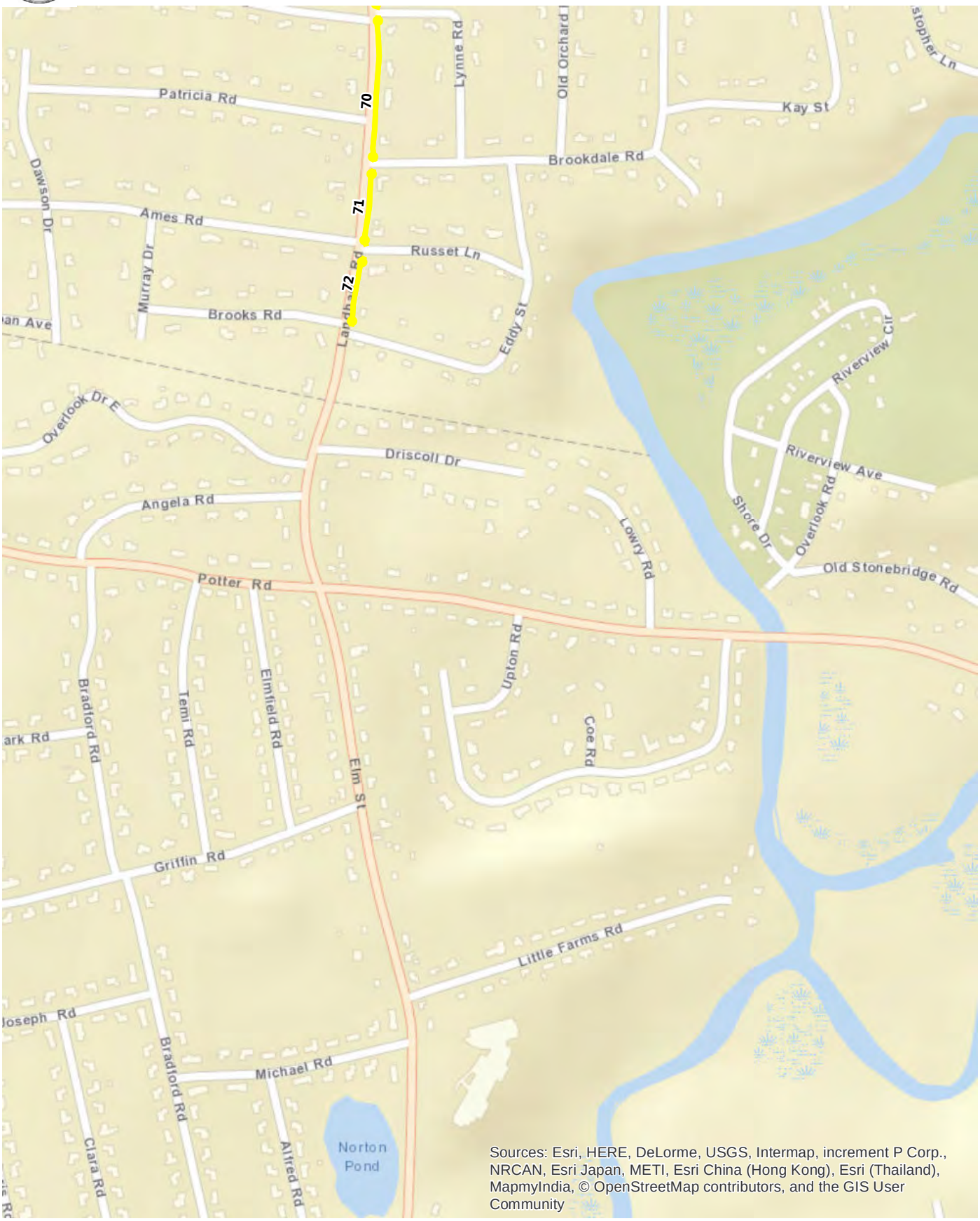
Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

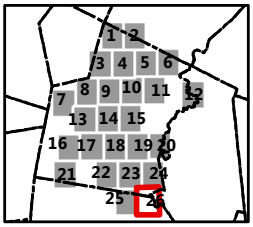
Sidewalk Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair



Sudbury, Massachusetts

Sidewalk Condition Map

Appendix I: Guardrail Inventory Report

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
BLANDFORD DRIVE			WILLIS ROAD			To	CUL DE SAC						
144	22	269	256	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
BRIANT DRIVE			WILLIS ROAD			To	CUL DE SAC						
82	730	842	108	Left	Other	Good	Wood	10	Other	Other	No	$0	0.00
83	735	824	93	Right	Other	Good	Wood	10	Other	Other	No	$0	0.00
CODJER LANE			CONCORD ROAD			To	DEAD END						
141	1,625	1,695	73	Left	Wood Timber	Good	Wood	7	Bridge Attachment	Other	No	$0	0.00
140	1,641	1,775	127	Right	Wood Timber	Good	Wood	7		Other	No	$0	0.00
CONCORD ROAD			BOSTON POST ROAD			To	CONCORD TOWN LINE						
174	2,933	3,028	97	Left	W-Beam Steel	Excellent	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
134	8,682	9,257	585	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	Yes	$0	0.00
133	9,202	9,465	271	Left	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
132	12,485	12,656	164	Right	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
131	13,060	13,183	124	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
130	13,302	13,392	90	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
161	13,723	15,288	1,565	Right	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00
162	14,067	15,288	1,235	Right	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00
160	16,878	17,396	514	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
159	16,958	17,436	484	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$17,925	53.20
17	22,012	22,119	107	Right	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$3,959	53.20
16	22,012	22,112	100	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$3,693	53.20
19	23,193	23,308	115	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
18	23,193	23,305	112	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
20	24,090	24,272	182	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
24	24,469	25,221	744	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
23	25,460	25,646	185	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Blunt End	No	$0	0.00
21	25,491	26,164	660	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	Yes	$0	0.00
22	25,673	25,756	84	Right	W-Beam Steel	Good	Galvanized Steel	6	Blunt End	Terminal End	No	$0	0.00
DAKIN ROAD			NORTH ROAD			To	CONCORD TOWN LINE						
14	569	658	89	Right	W-Beam Steel	Good	Galvanized Steel	6	Other	Terminal End	No	$0	0.00
15	569	653	84	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
DUTTON ROAD			HUDSON ROAD			To	HUDSON ROAD						
91	4,279	4,416	136	Right	Wood Timber	Good	Galvanized Steel	6	Other	Other	No	$0	0.00
90	4,303	4,398	100	Left	Wood Timber	Good	Galvanized Steel	6	Other	Other	No	$0	0.00
178	13,213	13,279	67	Left	W-Beam Steel	Good	Galvanized Steel	10	Terminal End	Terminal End	No	$0	0.00
175	13,235	13,304	68	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
176	13,297	13,514	226	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
177	13,329	13,366	37	Left	W-Beam Steel	Fair	Galvanized Steel	8	Terminal End	Terminal End	No	$1,358	53.20
FAIRBANK ROAD (Y INT)			HUDSON ROAD			To	FAIRBANK ROAD						
147	55	80	34	Right	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
FRENCH ROAD			PEAKHAM ROAD			To	DUTTON ROAD						
89	2,842	2,929	89	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
88	2,847	2,908	61	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
HAYNES ROAD			PANTRY ROAD			To	NORTH ROAD						
56	925	1,112	180	Right	W-Beam Steel	Good	Galvanized Steel	6	Roll-over	Terminal End	No	$0	0.00

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
HUDSON ROAD			CONCORD ROAD			To	STOW TOWN LINE						
145	8,018	8,184	166	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
146	9,692	9,751	67	Right	Wood Timber	Good	Wood	7	Other	Other	Yes	$0	0.00
148	10,434	10,844	408	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
149	14,357	14,534	179	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
151	15,755	15,869	113	Right	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
150	15,812	15,931	119	Left	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
LANDHAM ROAD			BOSTON POST ROAD			To	FRAMINGHAM TOWN LINE						
41	107	238	131	Right	W-Beam Steel	Good	Galvanized Steel	6	Bridge Attachment	Terminal End	No	$0	0.00
42	239	282	45	Left	Bridge Rail	Good	Galvanized Steel	7	Bridge Attachment	Bridge Attachment	No	$0	0.00
40	244	282	38	Right	Bridge Rail	Good	Galvanized Steel	7	Bridge Attachment	Bridge Attachment	No	$0	0.00
39	284	370	87	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Bridge Attachment	No	$0	0.00
38	288	400	112	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Bridge Attachment	Yes	$0	0.00
37	371	496	131	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
35	1,453	1,522	68	Right	Wood Timber	Excellent	Combination	6	Other	Other	No	$0	0.00
36	1,477	1,565	88	Left	Wood Timber	Excellent	Combination	6	Other	Other	No	$0	0.00
LANDHAM ROAD (NB LEG)			LANDHAM ROAD			To	BOSTON POST ROAD						
43	82	96	144	Right	W-Beam Steel	Good	Galvanized Steel	6	Bridge Attachment	Terminal End	No	$0	0.00
LINCOLN ROAD			CONCORD ROAD			To	WAYLAND TOWN LINE						
64	3,157	3,194	121	Left	Wood Timber	Fair	Wood	6	Bridge Attachment	Other	No	$15,170	53.20
65	3,175	3,198	75	Right	Wood Timber	Fair	Wood	3	Other	Bridge Attachment	No	$9,433	53.20
63	3,194	3,240	150	Left	Bridge Rail	Fair	Wood	10	Bridge Attachment	Bridge Attachment	No	$0	53.20

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
66	3,198	3,241	142	Right	Bridge Rail	Fair	Wood	10	Bridge Attachment	Bridge Attachment	No	$0	53.20
62	3,241	3,254	44	Left	Wood Timber	Good	Wood	3	Other	Bridge Attachment	No	$0	0.00
67	3,242	3,255	44	Right	Wood Timber	Fair	Wood	3	Bridge Attachment	Other	No	$5,494	53.20
LINCOLN ROAD (EB Y INT)			LINCOLN ROAD				To	WAYLAND TOWN LINE					
78	38	143	95	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
LINCOLN ROAD (WB Y INT)			CONCORD ROAD				To	WAYLAND TOWN LINE					
79	0	246	258	Right	W-Beam Steel	Good	Galvanized Steel	6	Roll-over	Terminal End	No	$0	0.00
MARLBORO ROAD			MAYNARD ROAD				To	HAYNES ROAD					
59	6,577	6,697	118	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$4,376	53.20
60	6,578	6,631	54	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
61	6,637	6,693	56	Right	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$2,080	53.20
MAYNARD ROAD			HUDSON ROAD				To	MAYNARD TOWN LINE					
163	5,314	5,339	25	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
164	5,341	5,402	61	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
165	5,933	5,968	35	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$1,299	60.00
166	5,946	6,004	58	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
167	6,426	6,624	194	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
168	7,066	7,068	399	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$14,767	60.00
170	9,442	9,708	268	Right	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Roll-over	Yes	$9,906	60.00
169	9,750	10,283	530	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
171	9,998	10,508	509	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Missing	No	$0	0.00
173	10,329	10,708	384	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$14,199	60.00

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
172	10,525	10,704	174	Right	W-Beam Steel	Good	Galvanized Steel	6	Missing	Terminal End	No	$0	0.00
MORSE ROAD			CONCORD ROAD			To	MARLBORO ROAD						
80	2,272	2,395	130	Right	W-Beam Steel	Poor	Galvanized Steel	12	Terminal End	Terminal End	No	$4,795	66.60
57	6,939	7,031	137	Right	W-Beam Steel	Fair	Wood	6	Terminal End	Terminal End	Yes	$5,084	46.60
MOSSMAN ROAD			MARLBORO ROAD			To	NORTH ROAD						
58	42	102	62	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
81	4,783	4,848	59	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$2,175	46.60
NEW BRIDGE ROAD			CONCORD ROAD			To	WATER ROW						
128	1,572	1,642	74	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
129	1,573	1,666	88	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
NOBSCOT ROAD			BOSTON POST ROAD			To	FRAMINGHAM TOWN LINE						
103	54	467	421	Left	W-Beam Steel	Good	Combination	6	Terminal End	Roll-over	No	$0	0.00
104	954	1,196	238	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
105	1,342	1,695	353	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
106	1,808	2,151	331	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
107	1,893	2,146	264	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
109	3,986	4,157	172	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
110	4,271	4,383	111	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
111	4,417	4,473	56	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
112	4,506	5,034	537	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
113	5,534	5,716	183	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
NORTH ROAD			MAYNARD TOWN LINE			To	CONCORD TOWN LINE						
26	4,103	4,317	215	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Roll-over	No	$0	0.00
27	4,350	5,139	789	Left	W-Beam Steel	Good	Galvanized Steel	6	Roll-over	Terminal End	No	$0	0.00
25	7,221	7,612	388	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
11	10,397	10,913	518	Right	W-Beam Steel	Fair	Galvanized Steel	6	Roll-over	Terminal End	Yes	$19,165	60.00
13	10,491	10,645	151	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$5,599	60.00
12	10,657	10,957	301	Left	W-Beam Steel	Good	Galvanized Steel	6	Roll-over	Terminal End	No	$0	0.00
10	12,777	13,902	1,129	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
6	14,604	15,052	451	Right	W-Beam Steel	Good	Galvanized Steel	6	Roll-over	Roll-over	No	$0	0.00
8	14,697	14,964	267	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
7	14,976	15,040	64	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	No	$2,358	60.00
NORTHWOOD DRIVE						To							
9	270	629	354	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
OLD FRAMINGHAM ROAD			NOBSCOT ROAD			To	FRAMINGHAM TOWN LINE						
108	1,724	2,002	277	Left	Wood Timber	Good	Wood	6	Other	Other	No	$0	0.00
OLD LANCASTER ROAD			HUDSON ROAD			To	GREEN HILL ROAD						
69	320	436	59	Right	Wood Timber	Good	Wood	6	Bridge Rail	Bridge Rail	No	$0	0.00
68	320	430	63	Right	Wood Timber	Good	Wood	6	Bridge Rail	Bridge Rail	No	$0	0.00
OLD SUDBURY ROAD			CONCORD ROAD			To	WAYLAND TOWN LINE						
157	1,582	1,781	197	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
158	1,647	1,819	172	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
153	3,146	3,975	832	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	Yes	$0	0.00

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
155	3,211	3,319	108	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
156	4,389	4,890	488	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$18,050	60.00
152	5,484	5,941	461	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
154	5,509	5,607	98	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
120	6,455	7,350	895	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
121	6,478	6,481	768	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
118	7,263	7,575	312	Left	W-Beam Steel	Good	Galvanized Steel	6	Other	Terminal End	No	$0	0.00
119	7,436	7,576	150	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Other	No	$0	0.00
PANTRY ROAD			NORTH ROAD				To	CONCORD ROAD					
54	2,159	2,237	79	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
53	2,169	2,282	112	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
55	2,241	2,347	107	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
52	2,290	2,352	61	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
PEAKHAM ROAD			BOSTON POST ROAD				To	HUDSON ROAD					
76	3,129	3,173	44	Left	Wood Timber	Good	Wood	6	Other	Other	No	$0	0.00
77	3,136	3,182	45	Right	Wood Timber	Good	Wood	6	Other	Other	No	$0	0.00
75	5,341	5,406	66	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
74	5,344	5,397	53	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	Yes	$0	0.00
70	11,517	11,560	44	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Blunt End	No	$0	0.00
73	11,522	11,629	106	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
72	11,563	11,576	13	Left	Other	Good	Other	0	Other	Other	No	$0	0.00
71	11,576	11,619	44	Left	W-Beam Steel	Good	Galvanized Steel	6	Blunt End	Terminal End	No	$0	0.00
PLYMPTON ROAD			CONCORD ROAD				To	WATER ROW					
126	1,032	1,111	83	Right	W-Beam Steel	Fair	Combination	6	Terminal End	Terminal End	No	$3,067	46.60

Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
127	1,034	1,116	79	Left	W-Beam Steel	Fair	Combination	6	Terminal End	Terminal End	No	$2,909	46.60
125	2,895	2,946	51	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
124	2,899	2,947	47	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
RAYMOND ROAD			BOSTON POST ROAD			To	FRAMINGHAM TOWN LINE						
44	725	742	25	Left	W-Beam Steel	Poor	Galvanized Steel	12	Terminal End	Missing	No	$918	73.20
45	1,497	1,685	190	Left	W-Beam Steel	Excellent	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
49	2,223	3,074	848	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
46	2,229	2,343	116	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
48	2,698	2,825	129	Right	Wood Timber	Fair	Wood	8	Other	Other	No	$16,095	53.20
47	2,699	2,815	114	Left	Wood Timber	Poor	Wood	8	Other	Other	No	$14,268	73.20
50	3,038	3,130	92	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
51	4,547	4,599	56	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
UNION AVENUE			BOSTON POST ROAD			To	CONCORD ROAD						
142	2,165	2,166	297	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Roll-over	No	$0	0.00
143	2,175	2,411	237	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Roll-over	No	$0	0.00
139	2,466	2,687	295	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
138	2,468	2,684	239	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
137	4,107	4,225	118	Left	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
135	4,436	4,647	210	Left	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
136	4,439	4,649	211	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
VIRGINIA RIDGE ROAD			POWDER MILL ROAD			To	CUL DE SAC LOOP						
33	15	243	228	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
32	15	265	256	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
31	865	1,055	195	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00

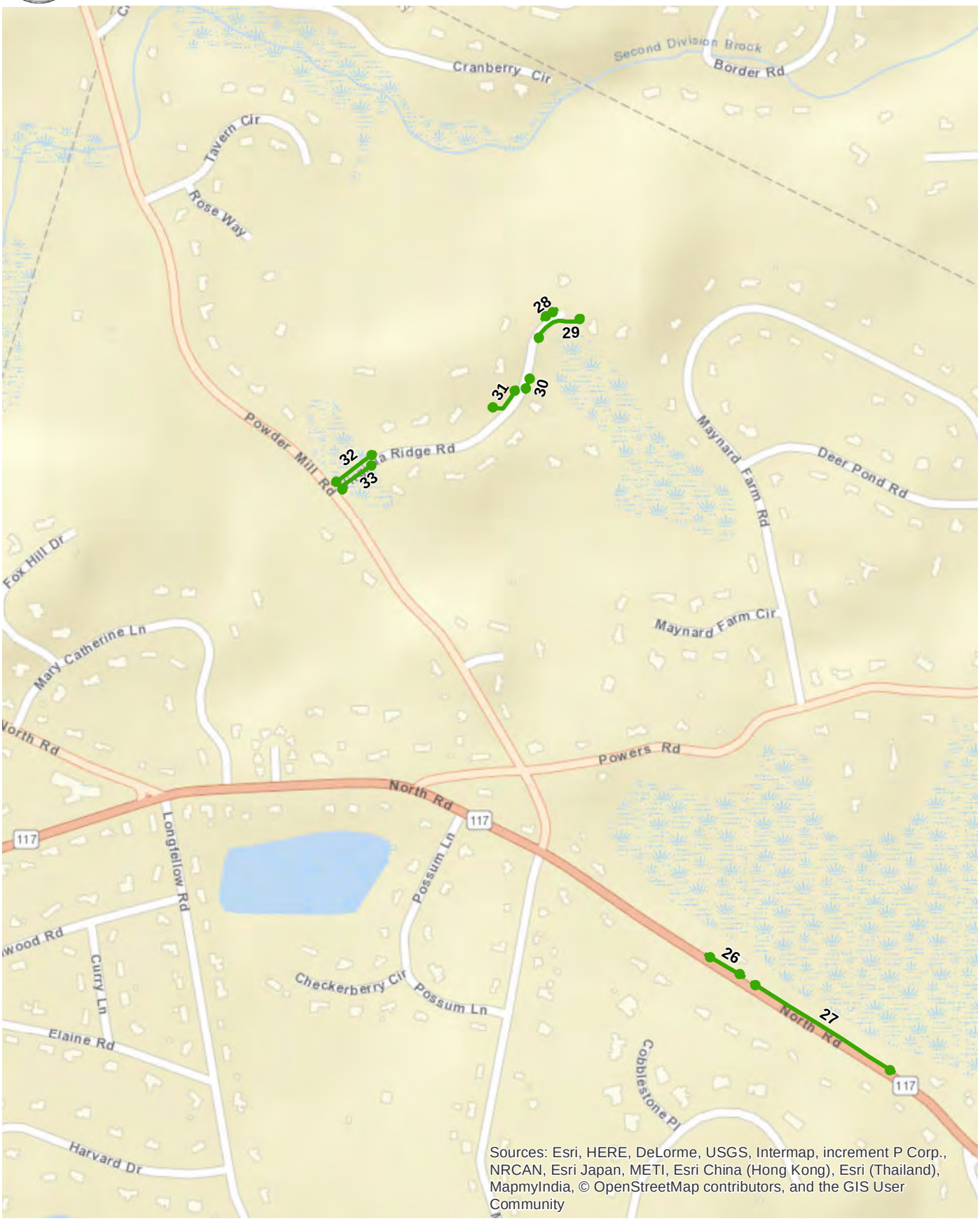
Guardrail Inventory Report

ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
30	1,036	1,126	94	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
29	1,276	1,623	276	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
28	1,373	1,461	92	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
WATER ROW			OLD SUDBURY ROAD			To	LINCOLN ROAD						
117	301	472	171	Left	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00
122	302	385	84	Right	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00
116	605	739	140	Left	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00
123	611	735	117	Right	Wood Timber	Good	Other	10	Other	Other	No	$0	0.00
115	4,189	4,260	70	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
114	7,677	7,965	309	Right	W-Beam Steel	Good	Galvanized Steel	6	Terminal End	Terminal End	No	$0	0.00
WAYSIDE INN ROAD			MARLBOROUGH TOWN LINE			To	MARLBORO TOWN LINE						
93	1,342	1,391	49	Right	Wood Timber	Good	Wood	6	Other	Other	No	$0	0.00
92	1,345	1,388	44	Left	Wood Timber	Good	Wood	6	Other	Other	No	$0	0.00
94	2,747	2,806	60	Left	Wood Timber	Good	Wood	7	Other	Other	No	$0	0.00
95	3,367	3,413	46	Left	Wood Timber	Good	Wood	6	Other	Bridge Attachment	No	$0	0.00
98	3,368	3,420	52	Right	Wood Timber	Good	Wood	8	Other	Bridge Attachment	No	$0	0.00
96	3,415	3,451	36	Left	Other	Good	Other	0	Bridge Attachment	Bridge Attachment	No	$0	0.00
99	3,422	3,451	29	Right	Other	Good	Other	0	Bridge Attachment	Bridge Attachment	No	$0	0.00
97	3,451	3,490	40	Left	Wood Timber	Good	Wood	6	Bridge Attachment	Other	No	$0	0.00
100	3,453	3,498	45	Right	Wood Timber	Good	Wood	6	Bridge Attachment	Other	No	$0	0.00
102	4,579	4,650	71	Right	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
101	4,580	4,648	67	Left	Wood Timber	Good	Wood	8	Other	Other	No	$0	0.00
WILLIS ROAD			MAYNARD ROAD			To	MOSSMAN ROAD						
84	3,522	3,737	216	Right	Wood Timber	Good	Wood	10	Other	Other	No	$0	0.00

Guardrail Inventory Report

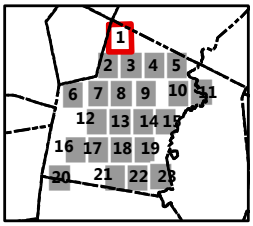
ID	From Station	To Station	Length (ft)	Side	Type	Condition	Post Type	Post Space	End Treatment 1	End Treatment 2	Accident Damage	Estimated Cost	Repair Priority
WOODSIDE ROAD			HOPESTILL BROWN ROAD (S)			To	CUTLER FARM ROAD						
34	2,543	2,592	65	Left	W-Beam Steel	Fair	Galvanized Steel	6	Terminal End	Terminal End	Yes	$2,421	53.20

Appendix J: Guardrail Inventory Maps



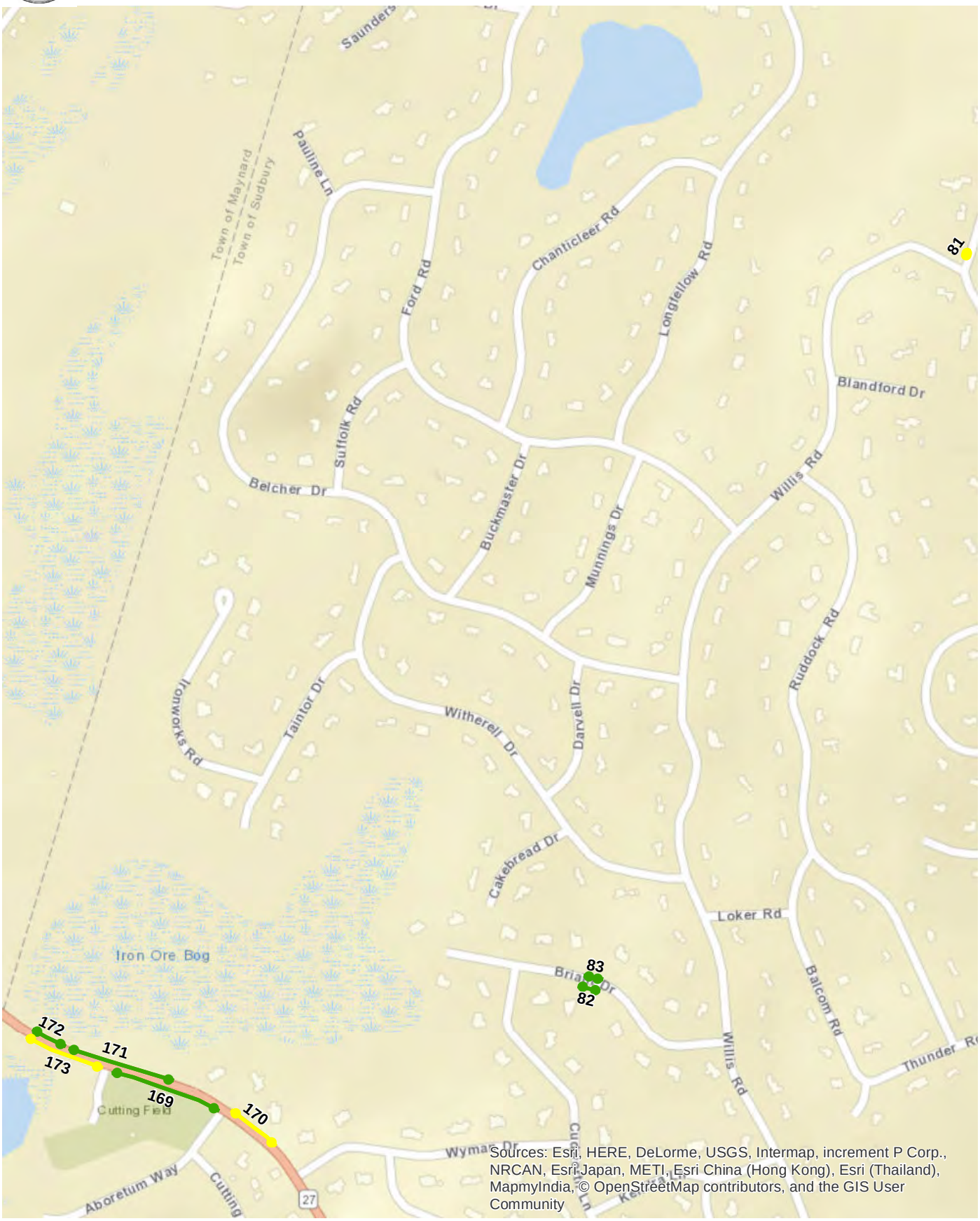
0 125 250 500 Feet

Condition
Excellent
Good
Fair
Poor



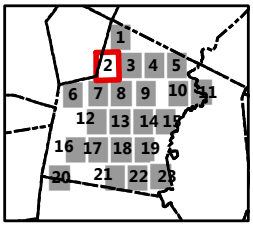
Sudbury, Massachusetts

Guardrail Condition Map



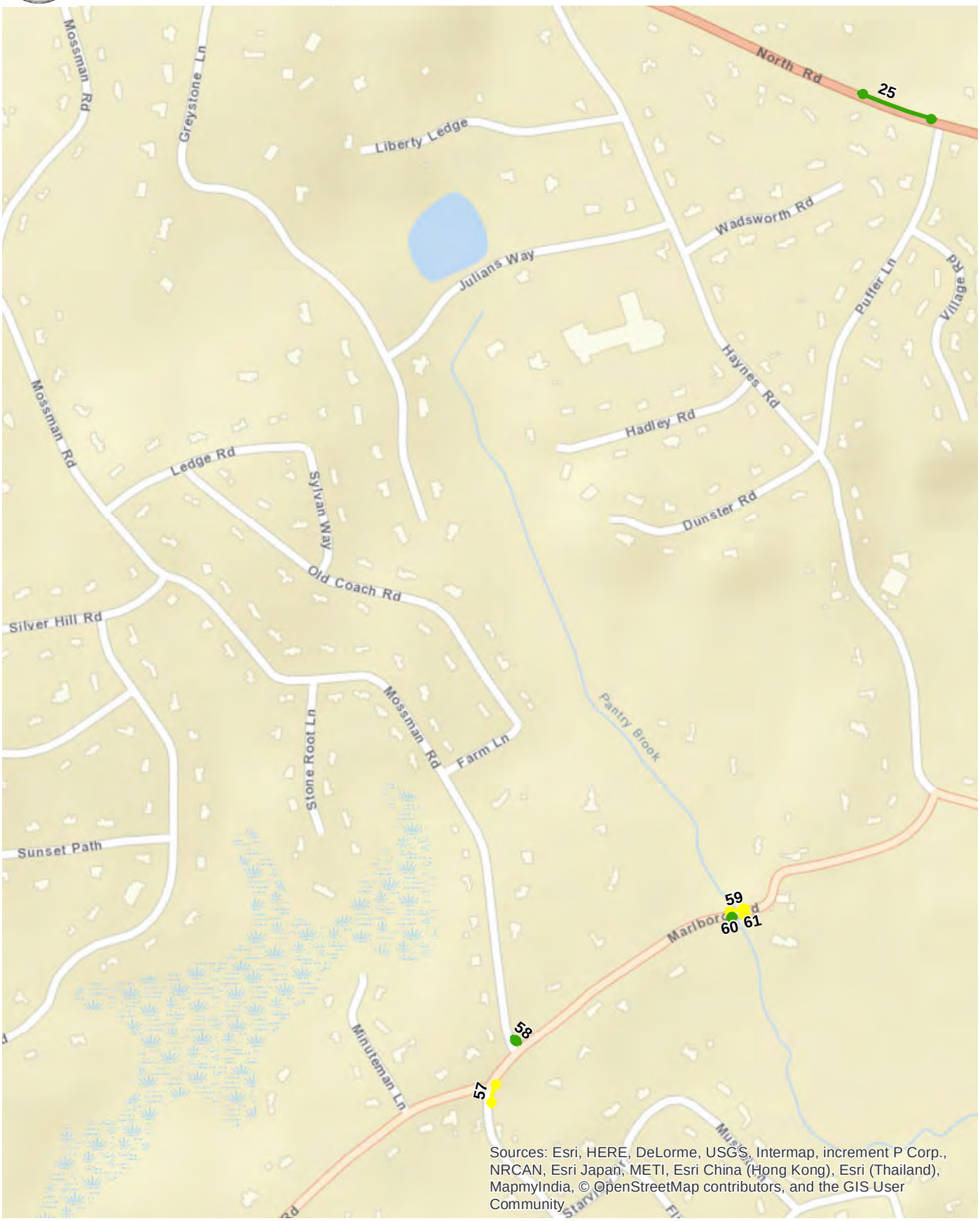
0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor



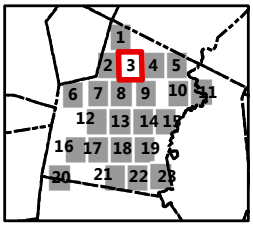
Sudbury, Massachusetts

Guardrail Condition Map



0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor

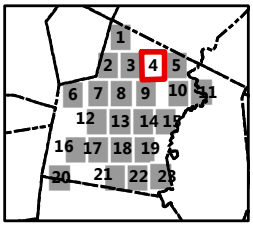


Sudbury, Massachusetts

Guardrail Condition Map

0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor



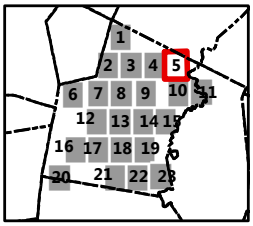
Sudbury, Massachusetts

Guardrail Condition Map

Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor

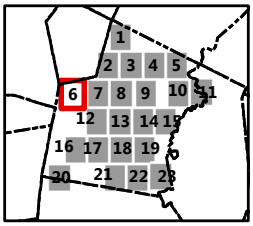


Sudbury, Massachusetts

Guardrail Condition Map

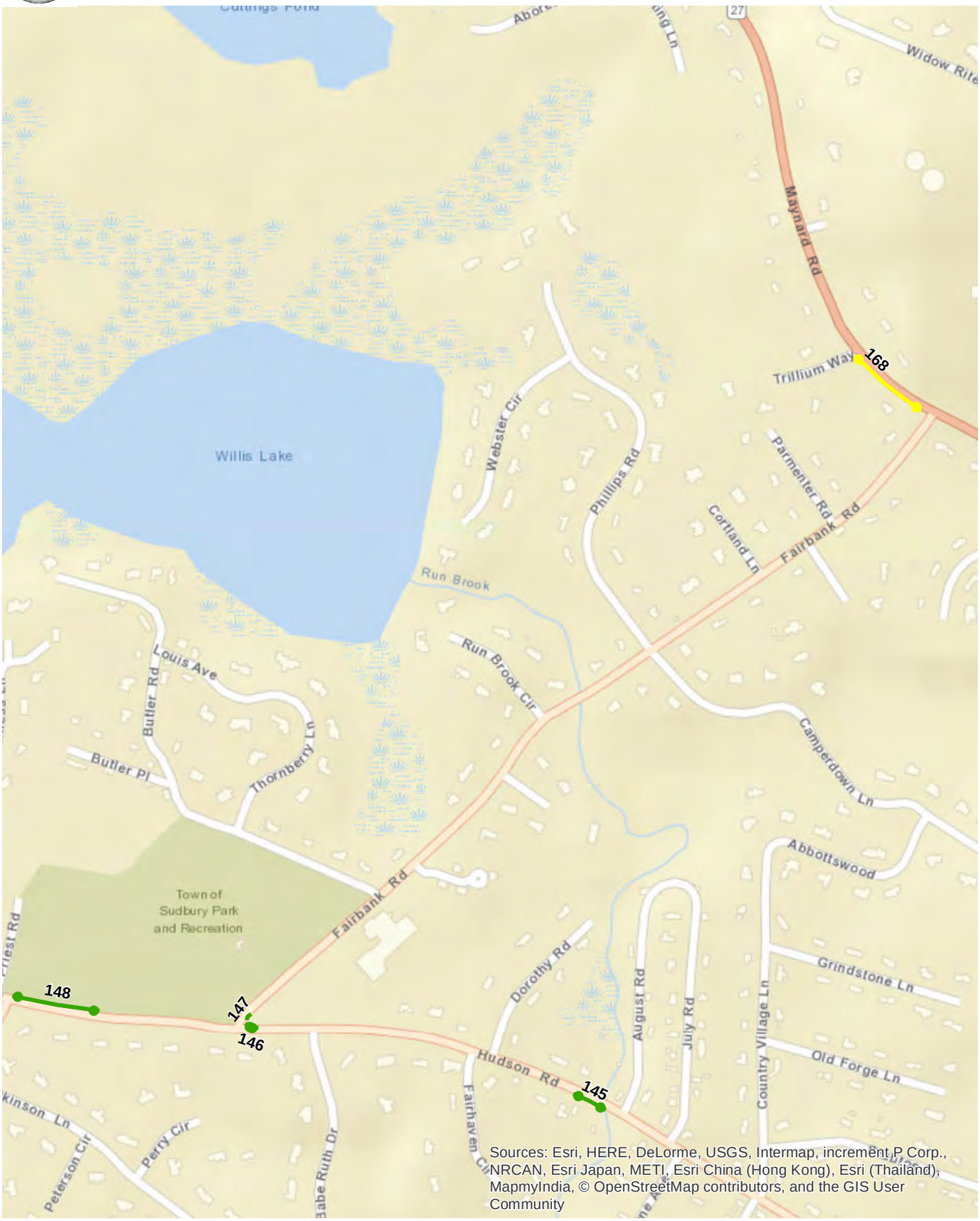
0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor



Sudbury, Massachusetts

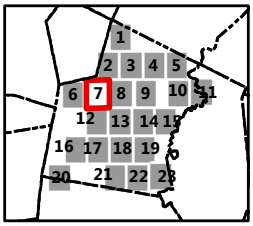
Guardrail Condition Map



Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

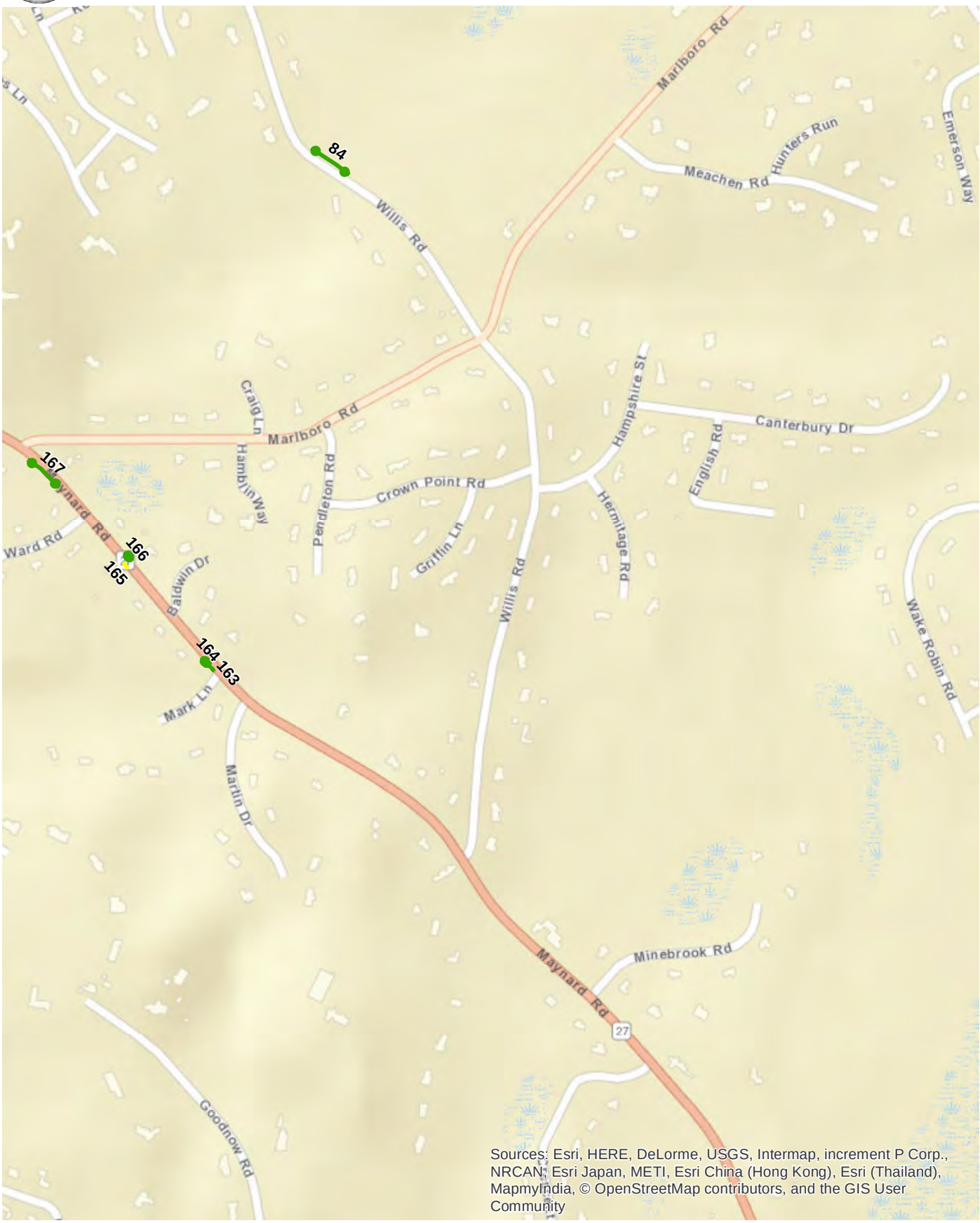
0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor



Sudbury, Massachusetts

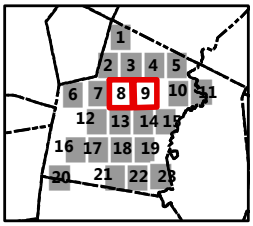
Guardrail Condition Map



0 125 250 500 Feet

Condition

- Excellent
- Good
- Fair
- Poor



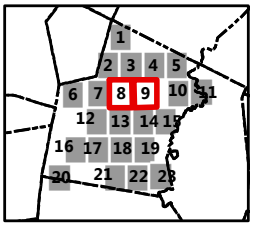
Sudbury, Massachusetts

Guardrail Condition Map

Sources: Esri, HERE, DeLorme, USGS, Intermap, increment P Corp., NRCAN, Esri Japan, METI, Esri China (Hong Kong), Esri (Thailand), MapmyIndia, © OpenStreetMap contributors, and the GIS User Community

0 125 250 500 Feet

Condition
- Excellent
- Good
- Fair
- Poor



Sudbury, Massachusetts

Guardrail Condition Map

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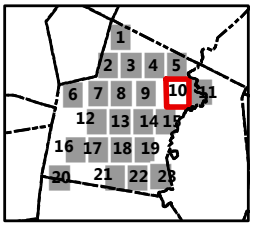

Sudbury, Massachusetts

Guardrail Condition Map

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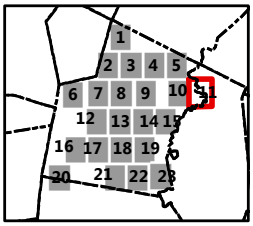
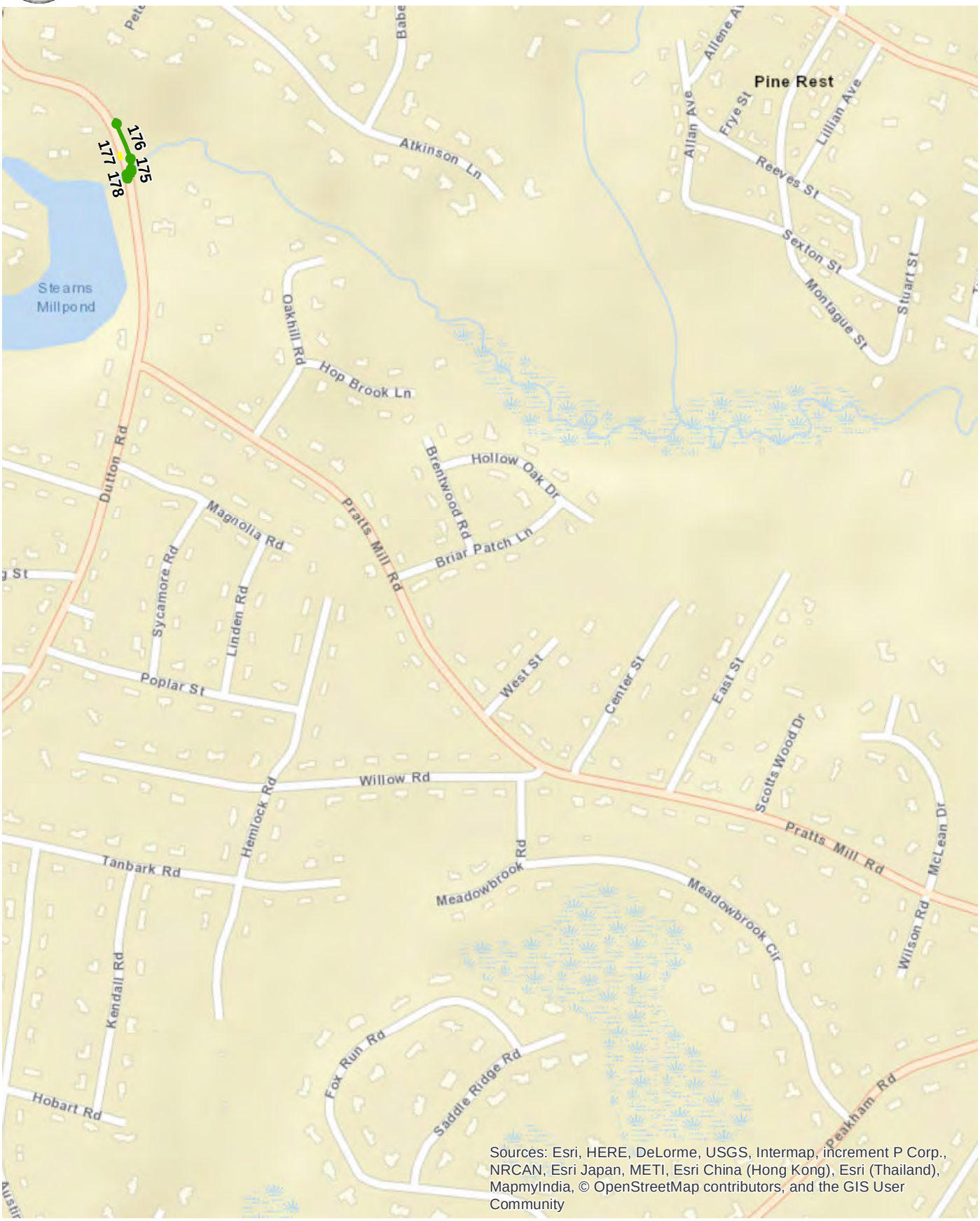


Sudbury, Massachusetts

Guardrail Condition Map



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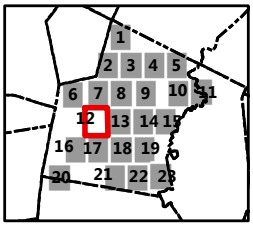
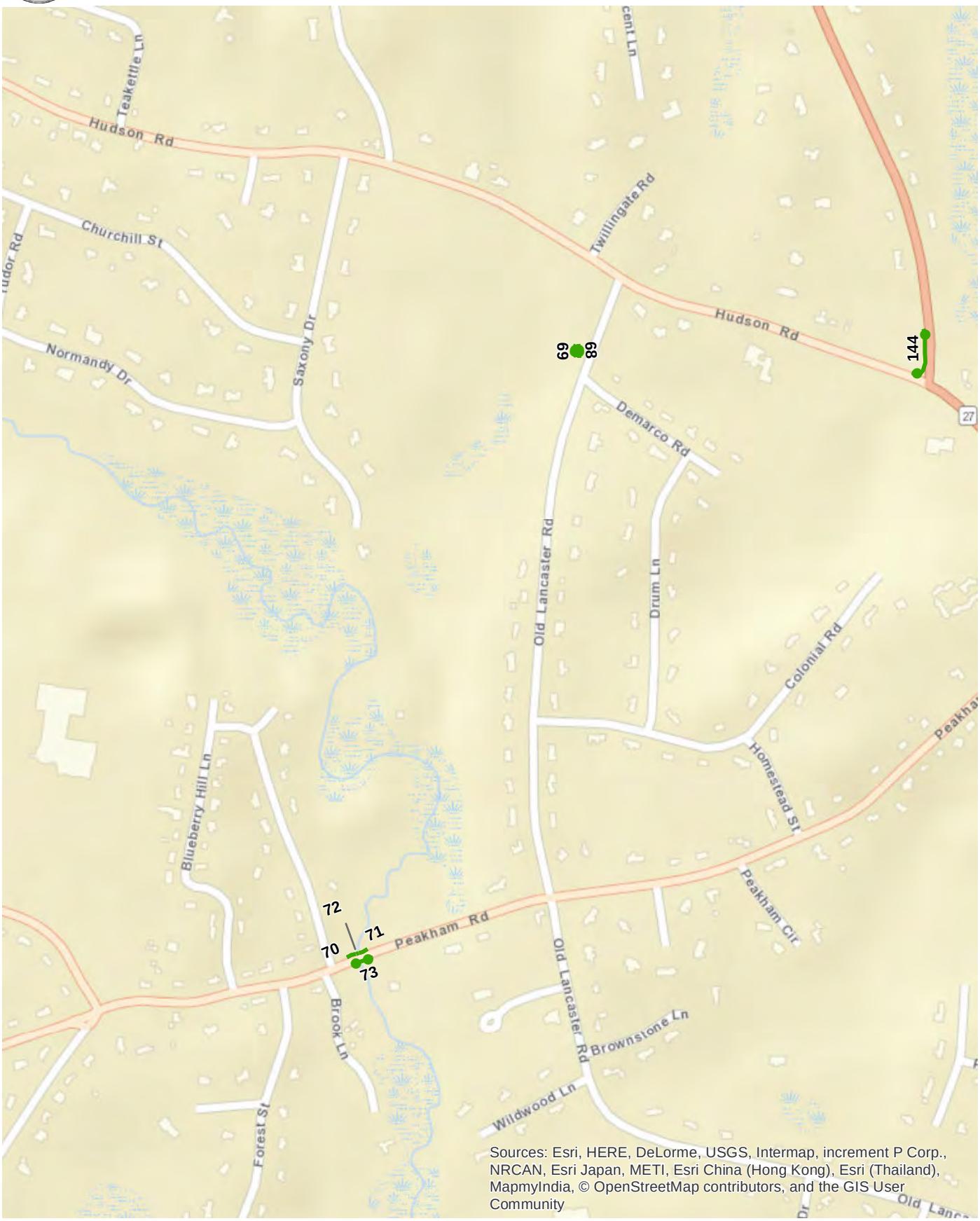


Sudbury, Massachusetts

Guardrail Condition Map



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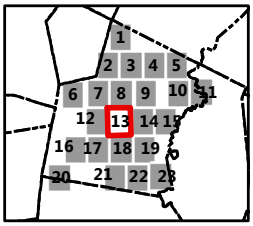
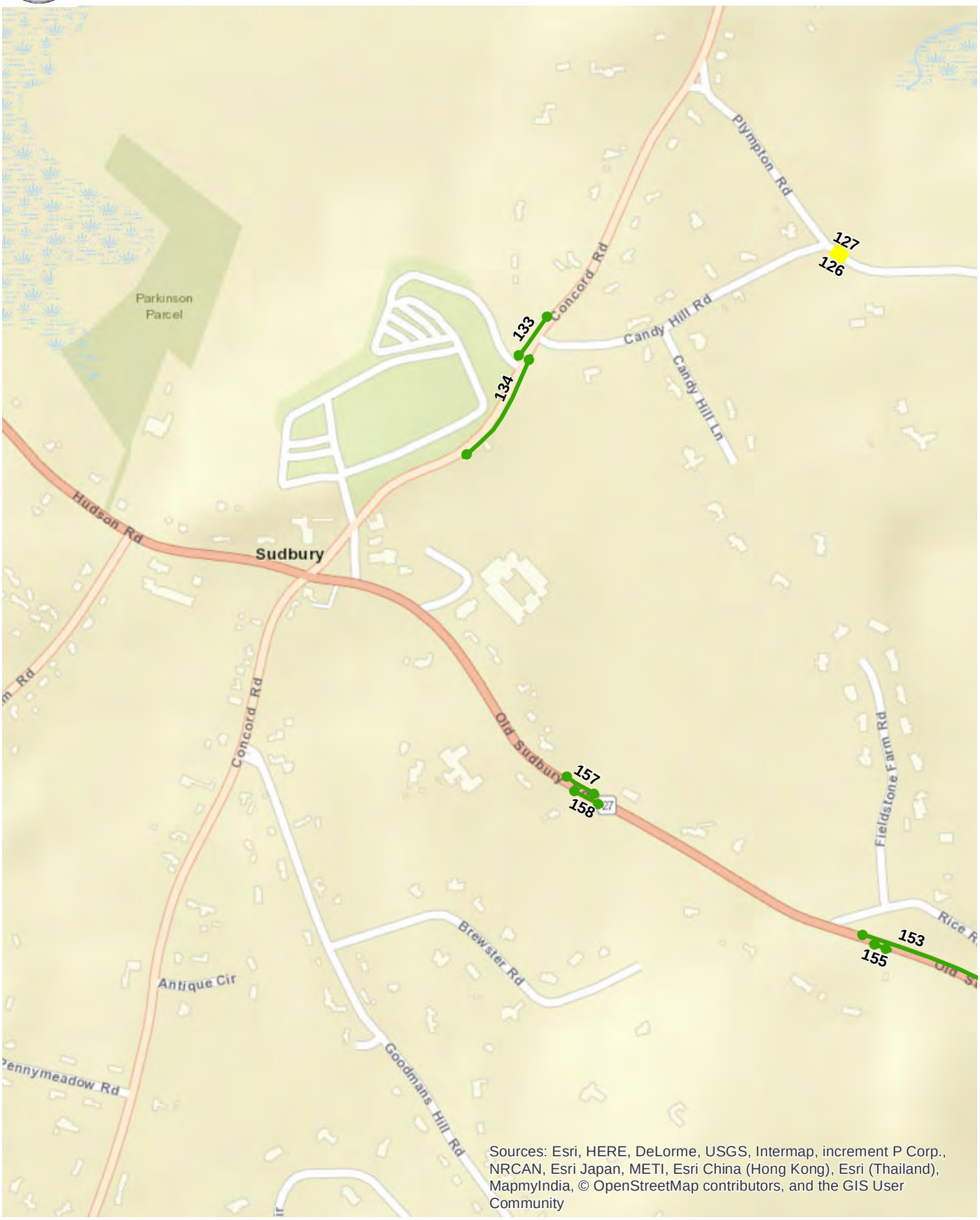


Sudbury, Massachusetts

Guardrail Condition Map



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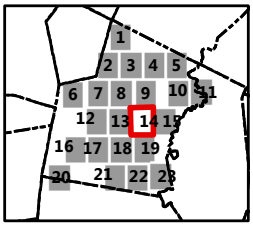

Sudbury, Massachusetts

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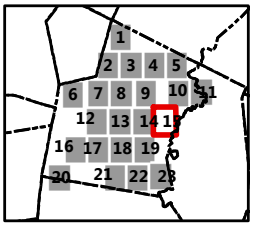
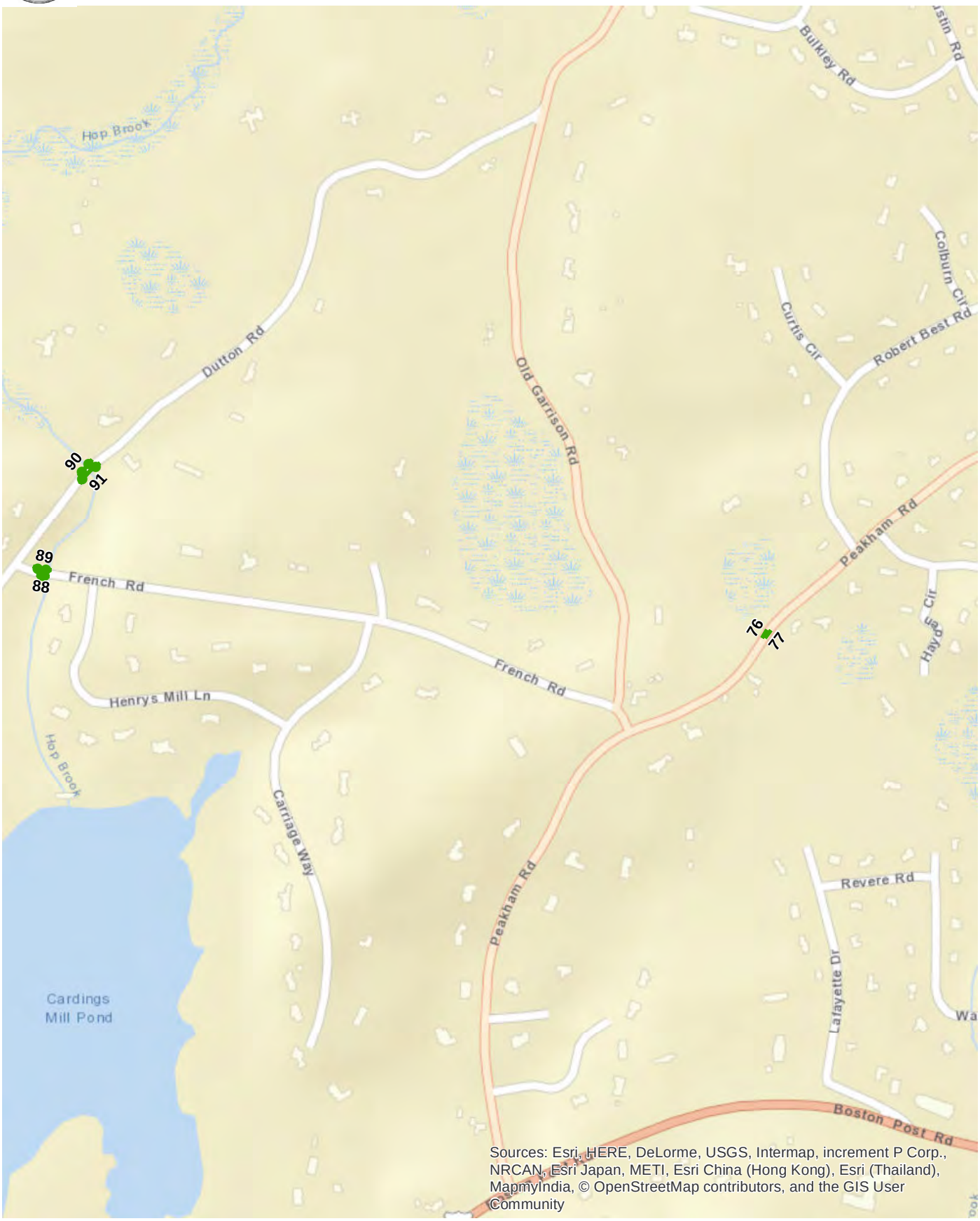


Sudbury, Massachusetts

Guardrail Condition Map



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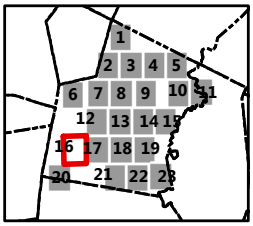
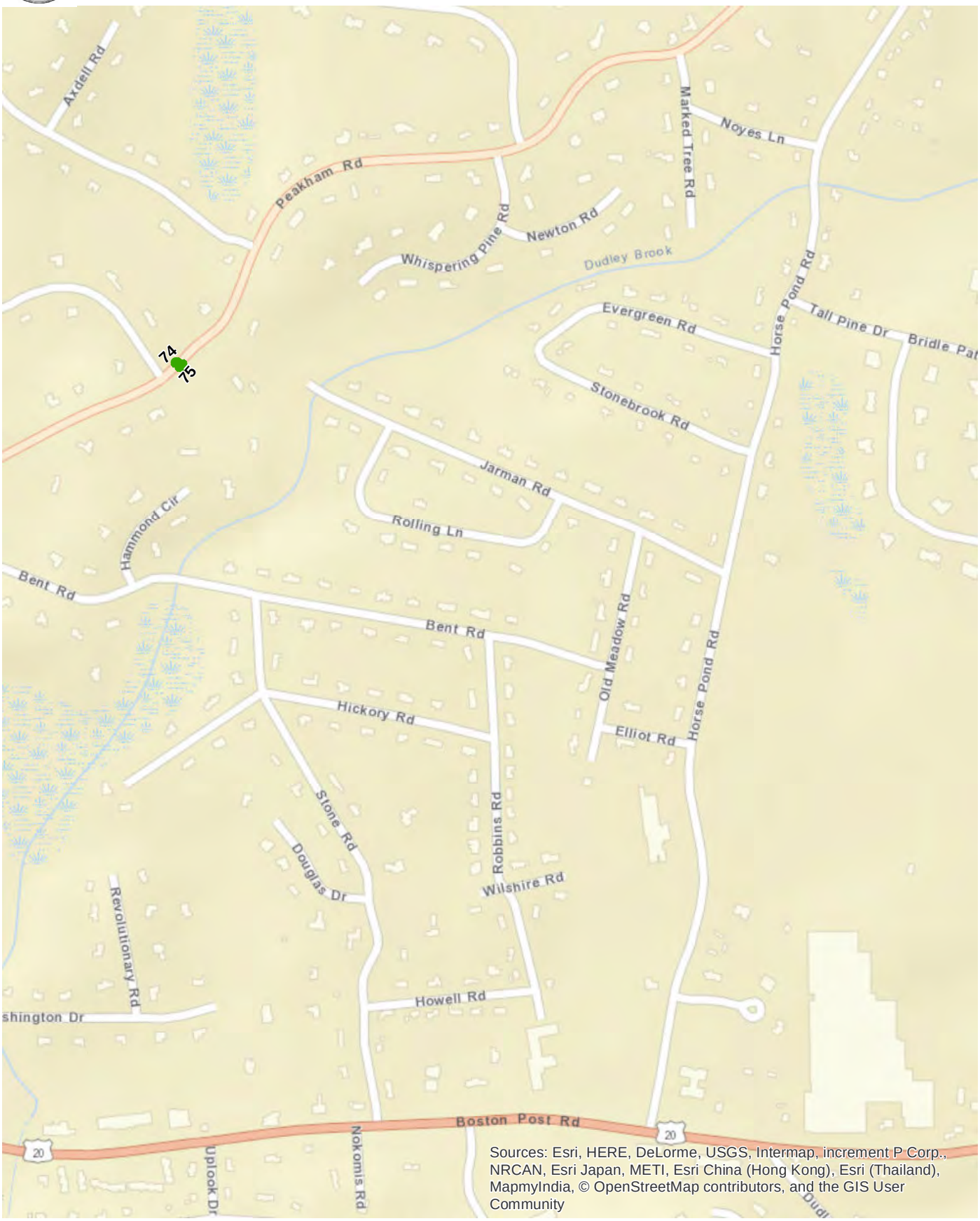


Sudbury, Massachusetts

Guardrail Condition Map



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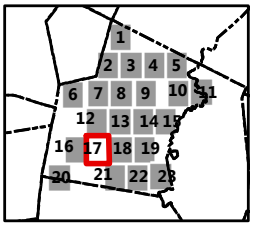
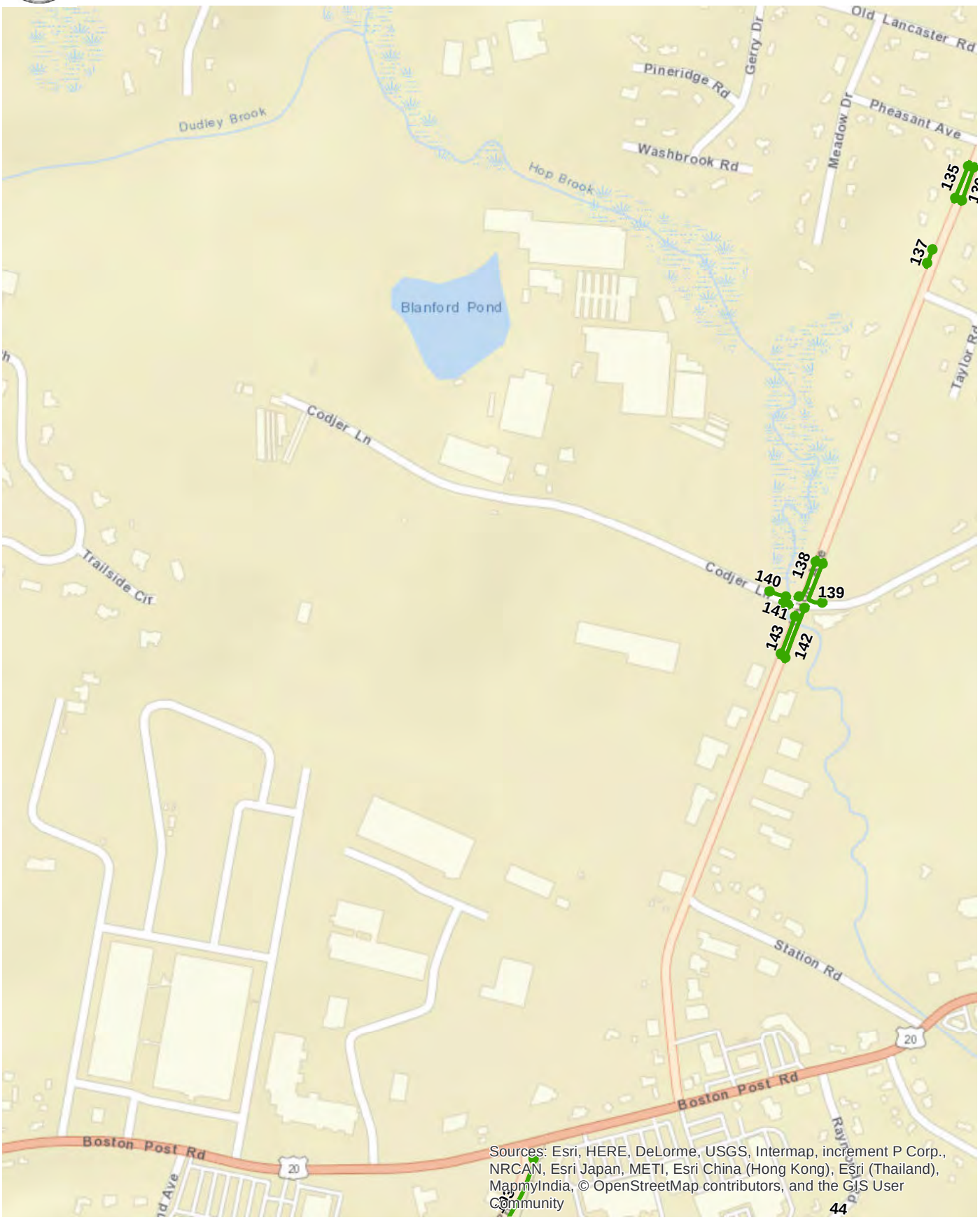


Sudbury, Massachusetts

Guardrail Condition Map



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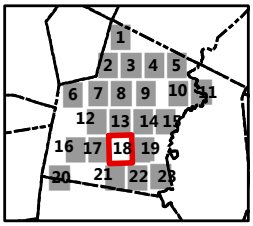
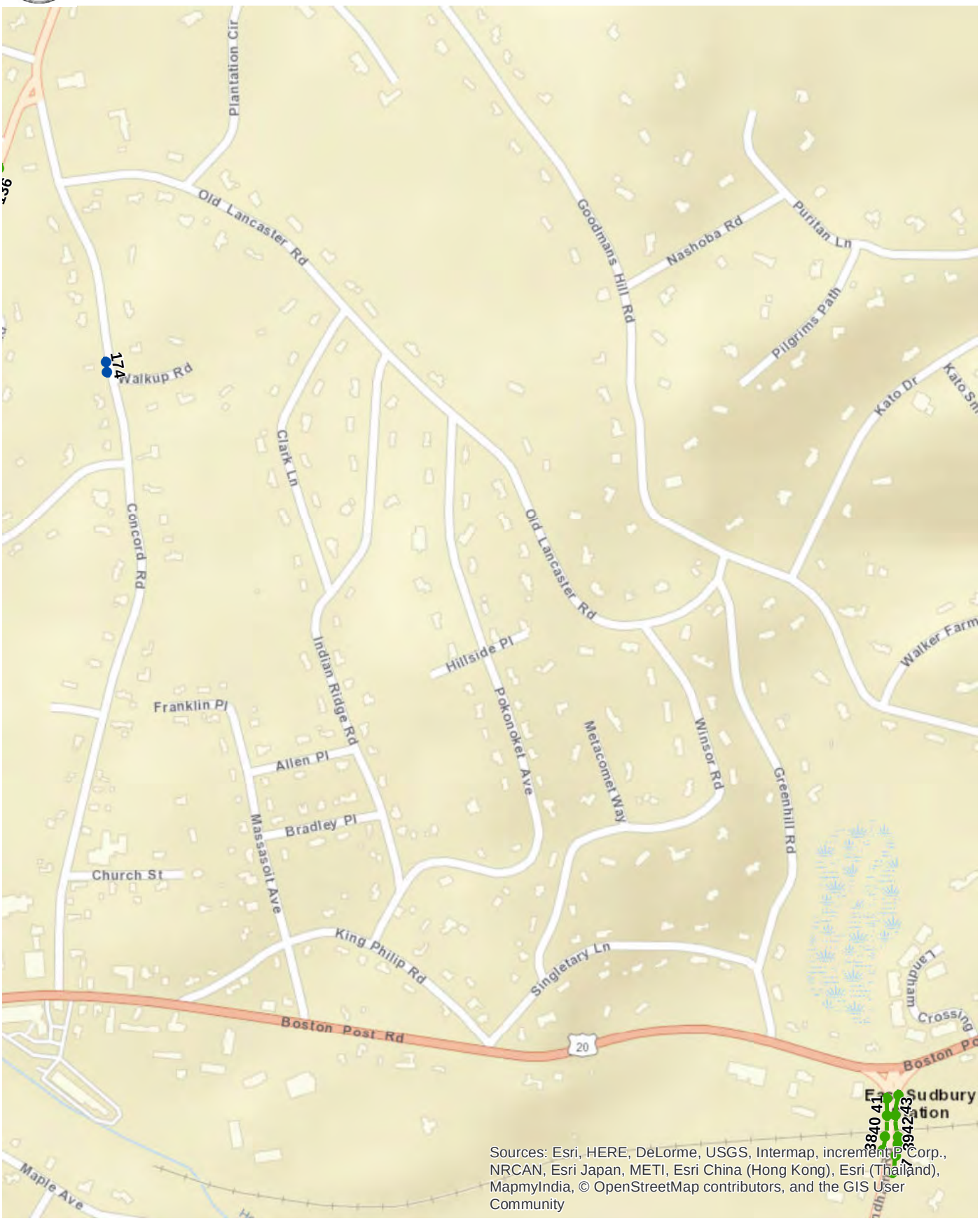


Sudbury, Massachusetts

Guardrail Condition Map



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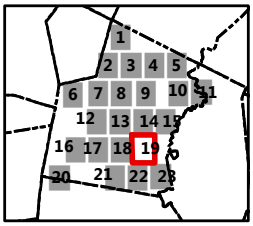
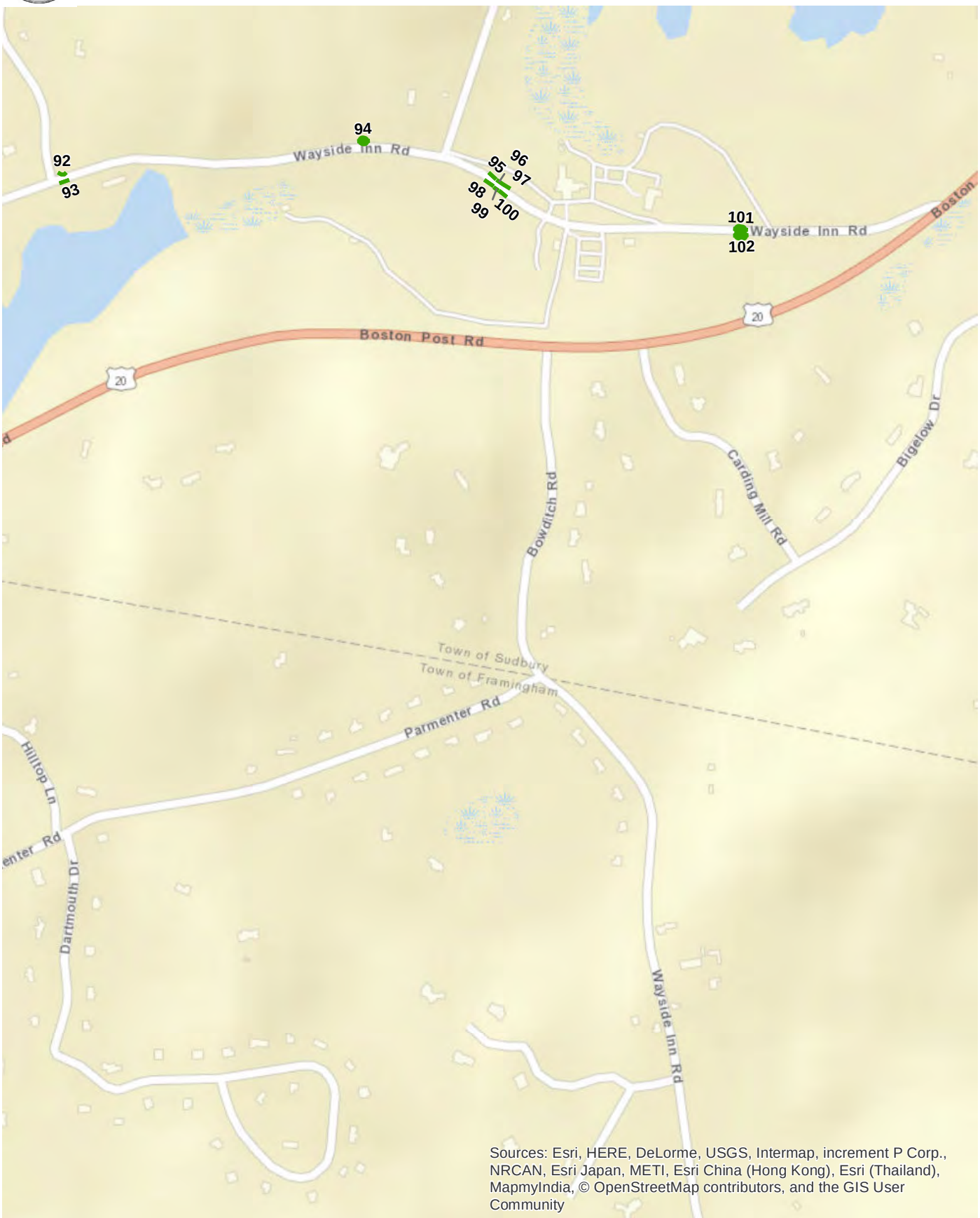


Sudbury, Massachusetts

Guardrail Condition Map



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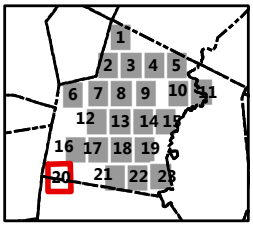

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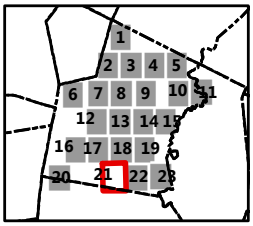
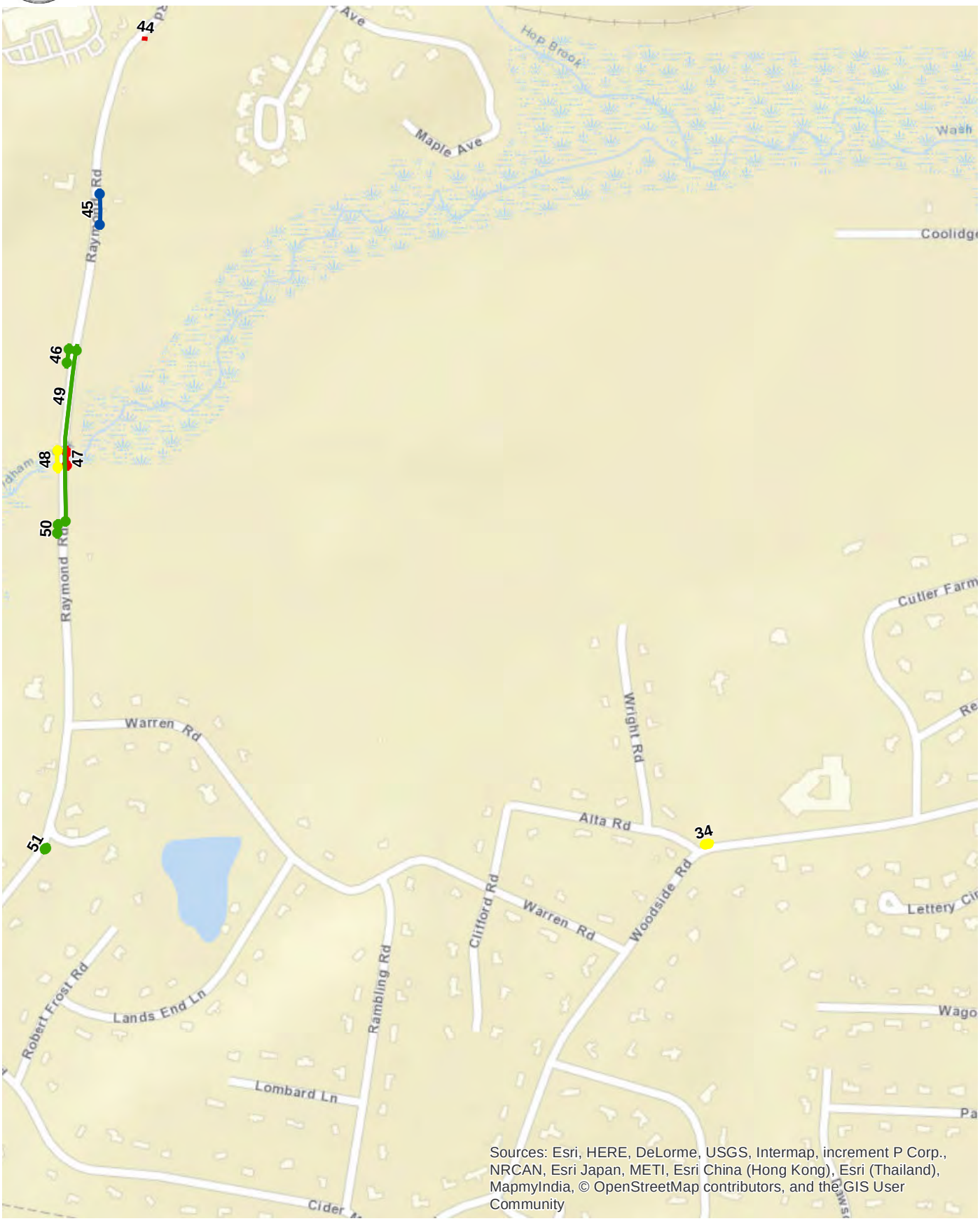


Sudbury, Massachusetts

Guardrail Condition Map



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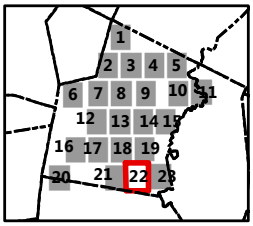
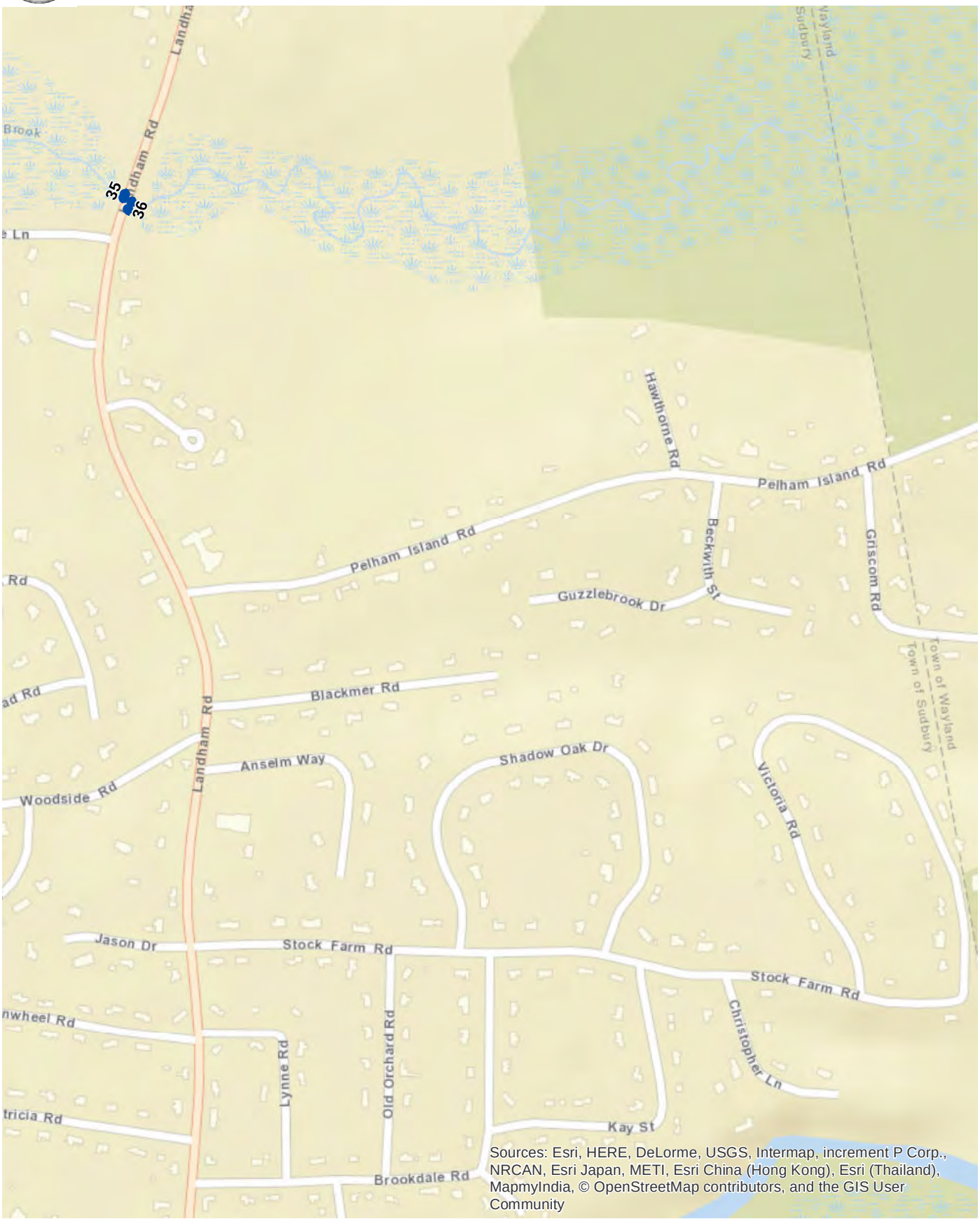


Sudbury, Massachusetts

Guardrail Condition Map



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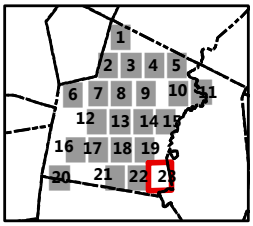

Sudbury, Massachusetts

Guardrail Condition Map